LEMOORE PLANNING COMMISSION Regular Meeting AGENDA Lemoore Council Chamber 429 'C' Street

August 10, 2020 7:00 p.m.

- 1. Pledge of Allegiance
- 2. Call to Order and Roll Call
- 3. Public Comment

Public Comment will be in accordance with the attached COVID 19 policy. This time is reserved for members of the audience to address the Planning Commission on items of interest that are not on the Agenda and are within the subject matter jurisdiction of the Commission. It is recommended that speakers limit their comments to 3 minutes each and it is requested that no comments be made during this period on items on the Agenda. The Commission is prohibited by law from taking any action on matters discussed that are not on the Agenda. Prior to addressing the Commission, any handouts for Commissioners will be provided to the Planning Commission Secretary for distribution to the Commissioners and appropriate staff.

- 4. Approval Minutes Regular Meeting, July 13, 2020
- 5. Report and Recommendation A Request by Pacific West Communities to Extend Approval of Conditional Use Permit No. 2018-01 and Major Site Plan Review No. 2018-04 for One Year. This project proposes to construct a 28-unit Senior Affordable Apartment complex on the southeast corner of Cinnamon Drive and Follett Street (APN 021-500-007). The project is the second phase of the existing Cinnamon Villas apartments.
- 6. Report and Recommendation A Request by Pharris Lemoore, LLC to Extend Approval of Vesting Tentative Subdivision Map 2005-02 (Tract 845) for One Year. This project proposes to divide approximately 13.32 acres into 51-unit single-family lots on the northeast corner of Bush Street and Marsh Drive (APN 023-510-055).
- Report and Recommendation A Request by Daley Homes to Extend Approval of Tentative Subdivision Map No. 2018-01 (Tract 793) and Major Site Plan Review No. 2018-03 for One Year. This project proposes to divide 17.87 acres into 30 single-family lots, 12 multi-family lots and a remainder located at the northeast corner of Highway 198 and Highway 41 (APN 023-320-005).
- 8. Director's Report Judy Holwell
- 9. Commission's Reports and Requests for Information
- 10. Adjournment

Upcoming Meetings

Regular Meeting of the Planning Commission, September 14, 2020

Agendas for all Planning Commission meetings are posted at least 72 hours prior to the meeting at the Council Chamber, 429 C Street and the Cinnamon Municipal Complex, 711 W. Cinnamon Drive. Any writings or documents provided to a majority of the Planning Commission regarding any item on this agenda will be made available for public inspection at the Community Development Department, located at 711 W. Cinnamon Drive, during normal business hours. The City of Lemoore complies with the Americans with Disabilities Act (ADA of 1990). The Council Chamber is accessible to the physically disabled. Should you need special assistance, please call (559) 924-6744, at least four (4) business days prior to the meeting.

CERTIFICATION OF POSTING

I, Kristie Baley, Planning Commission Secretary for the City of Lemoore, do hereby declare that I posted the above Planning Commission Agenda for the Regular Meeting of Monday, August 10, 2020 at 7:00 p.m. at Council Chamber, 429 C Street and Cinnamon Municipal Complex, 711 W. Cinnamon Drive, Lemoore CA in accordance with applicable legal requirements.

Posted this 7th day of August 2020.

//s//
Kristie Baley, Commission Secretary

CITY OF LEMOORE ALL PLANNING COMMISSION REGULAR AND SPECIAL MEETINGS

Attendance and Public Comment Changes Due to COVID-19

Given the current Shelter-in-Place Order covering the State of California and the Social Distance Guidelines issued by Federal, State, and Local Authorities, the City is implementing the following changes for attendance and public comment at all Planning Commission meetings until notified otherwise.

All upcoming regular and special Planning Commission meetings will *only be accessible online*. The meeting may be viewed through the following options:

• Youtube: <u>www.Youtube.com/c/cityoflemoore</u>

The City will also provide links to streaming options on the City's website and on its Facebook page. Unfortunately, physical attendance by the public cannot be accommodated given the current circumstances and the need to ensure the health and safety of the Planning Commission, City staff, and the public as a whole.

If you wish to make a general public comment or public comment on a particular item on the agenda, you must submit your public comments by e-mail to: planning@lemoore.com. In the subject line of the e-mail, please state your name and the item you are commenting on. If you wish to submit a public comment on more than one agenda item, please send a separate e-email for each item you are commenting on. Please be aware that written public comments, including your name, may become public information. Additional requirements for submitting public comments by e-mail are provided below.

General Public Comments & Comments on Planning Commission Business Items

For general public comments and comments regarding specific Planning Commission Business Items, all public comments must be received by e-mail no later than 5:00 p.m. the day of the meeting. Comments received by this time will be read aloud by a staff member during the applicable agenda item, provided that such comments may be read within the normal three (3) minutes allotted to each speaker. Any portion of your comment extending past three (3) minutes may not be read aloud due to time restrictions. If a general public comment or comment on a business item is received after 5:00 p.m., efforts will be made to read your comment into the record. However, staff cannot guarantee that written comments received after 5:00 p.m. will be read. All written comments that are not read into the record will be made part of the meeting minutes, provided that such comments are received prior to the end of the Planning Commission meeting.

Public Hearings

For public comment on a public hearing, all public comments must be received by the close of the public hearing period. All comments received by the close of the public hearing period will be

read aloud by a staff member during the applicable agenda item, provided that such comments may be read within the normal three (3) minutes allotted to each speaker. Any portion of your comment extending past three (3) minutes may not be read aloud due to time restrictions. If a comment on a public hearing item is received after the close of the public hearing, such comment will be made part of the meeting minutes, provided that such comment is received prior to the end of the meeting.

PLEASE BE AWARE THAT ANY PUBLIC COMMENTS RECEIVED THAT DO NOT SPECIFY A PARTICULAR AGENDA ITEM WILL BE READ ALOUD DURING THE GENERAL PUBLIC COMMENT PORTION OF THE AGENDA.

The City thanks you for your cooperation in advance. Our community's health and safety is our highest priority.

Minutes of the LEMOORE PLANNING COMMISSION Regular Meeting July 13, 2020

ITEM NO. 1 Pledge of Allegiance

ITEM NO. 2 Call to Order and Roll Call

The meeting was called to order at 7:00 PM.

Chair: Etchegoin Vice Chair: Koelewyn

Commissioners: Clement, Dey, Franklin, Meade

City Staff and Contract Employees Present: Community Development Director Holwell, City Planner Brandt (QK), City Attorney Carlson (Lozano Smith) via conference call, Commission Secretary Baley

ITEM NO. 3 Public Comment

There was no comment.

ITEM NO. 4 Approval – Minutes – Special Meeting, June 22, 2020

Motion by Commissioner Dey, seconded by Commissioner Clement, to approve the Minutes of the Planning Commission Special Meeting of June 22, 2020.

Ayes: Dey, Clement, Franklin, Meade, Etchegoin

Abstain: Koelewyn

ITEM NO. 5 – Public Hearing to accept public comment for Tentative Parcel Map No. 2020-01: a request by CV Housing, LLC (Brett Fugman) to divide 16.14 acres into four commercial parcels and a remainder parcel. The site is located on the southeast corner of State Route 41 and Hanford-Armona Road (APN 021-660-031). It has been determined that the project is Categorically Exempt from the requirements of CEQA per Section 15315 (Class 15 – Minor Land Divisions). In addition, A Mitigated Negative Declaration was prepared and adopted previously that included minor divisions of land in the description of the project.

City Planner Brandt presented the staff report and answered questions.

Community Development Director Holwell answered questions asked by Commissioners.

Chair Etchegoin opened the public hearing at 7:31 p.m.

There was no comment.

Chair Etchegoin closed the public hearing at 7:32 p.m.

Motion by Commissioner Koelewyn, seconded by Commissioner Meade to approve Resolution No. 2020-08, approving Tentative Parcel Map No. 2020-01, with the conditions listed in the staff report, mark ups on the parcel map and comments dated June 10, 2020.

Ayes: Koelewyn, Meade, Clement, Dey, Franklin, Etchegoin

ITEM NO. 6 Director's Report

Community Development Director Holwell provided information regarding:

A request for a one year extension submitted by Pacific Partners for the 28 unit senior affordable housing development, Cinnamon Villas II to be located on the south east corner of Cinnamon Drive and Follett Street is expected to be brought to the Planning Commission during the next meeting. The developer has delayed pulling building permits while securing funding and the site plan approval was scheduled to expire July 9, 2020.

Staff is in the process of reviewing the first portion (initial study) of the EIR for the Lacey Ranch project to be located northeast of Glendale and 18th Avenue.

Several developers have inquired about undeveloped subdivision tracts in Lemoore and staff are hopeful that any approved tract maps that have remained undeveloped might finally move forward.

The second reading of the Lennar Homes Tract 848 project was approved by the City Council on July 7th. Staff is unsure when they will move forward with the project at this time.

Commissioner Dey asked Holwell if the project approval date met the CEQA method of analysis deadline.

Holwell answered 'yes' and explained the process in which traffic is looked at for CEQA analysis. She notified Commissioners that the traffic study for the Lennar Homes, Tract 848 project was completed using the Level of Service (LOS) method, because the CEQA document was approved prior to July 1, 2020. Holwell went on to explain that as of July 1, 2020, the State of California requires traffic studies to be completed using the Vehicle Miles Traveled (VMT) method.

Holwell notified Commissioners that City offices will be closed to the public for an undetermined length of time, due to Governor Newsom's recent social distancing mandates.

ITEM NO. 7 – Commission's Reports and Requests for Information

Commissioner Meade expressed that he was pleased with the City Council's vote to approve the Lennar Homes subdivision. Tract 848.

Commissioner Clement asked Holwell to confirm the date of the next meeting.

Holwell notified Commissioners that the next regular meeting is scheduled for August 10, 2020.

Commissioner Meade commented that he had listened to Governor Newsom's report regarding COVID-19 earlier in the day and expressed his dismay that there are still members of the public not following simple guidelines and the economy is suffering because of it.

ITEM No. 8 – Adjournment	
The meeting adjourned at 7:44 P.M.	
Approved the 10 th day of August 2020.	
	APPROVED:
	Ray Etchegoin, Chairperson
ATTEST:	
Kristie Baley, Commission Secretary	



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Staff Report

To: Lemoore Planning Commission Item No. 5

From: Steve Brandt, City Planner

Date: August 3, 2020 Meeting Date: August 10, 2020

Subject: A request by Lemoore Pacific Associates II on behalf of the Housing Authority

of Kings County to extend the approval of Major Site Plan Review 2018-04 and Conditional Use Permit No. 2018-01 for one year (APN 021-500-007).

Proposed Motion:

Approve a one-year extension of time for Major Site Plan Review No. 2018-04 and Conditional Use Permit No. 2018-01, in accordance with Resolution No. 2020-09.

Discussion:

On July 9, 2020, Lemoore Pacific Associates II, on behalf of the Housing Authority of Kings County, submitted a written request to extend the approval of Major Site Plan Review (SPR) No. 2018-04 and Conditional Use Permit (CUP) No. 2018-01 for one year. The SPR and CUP were originally approved by the Planning Commission on July 9, 2018. The original request of the SPR and CUP was to construct Phase II, the last phase of the Cinnamon Villas Project on the proposed 1.86-acre lot, east of the existing Phase I site.

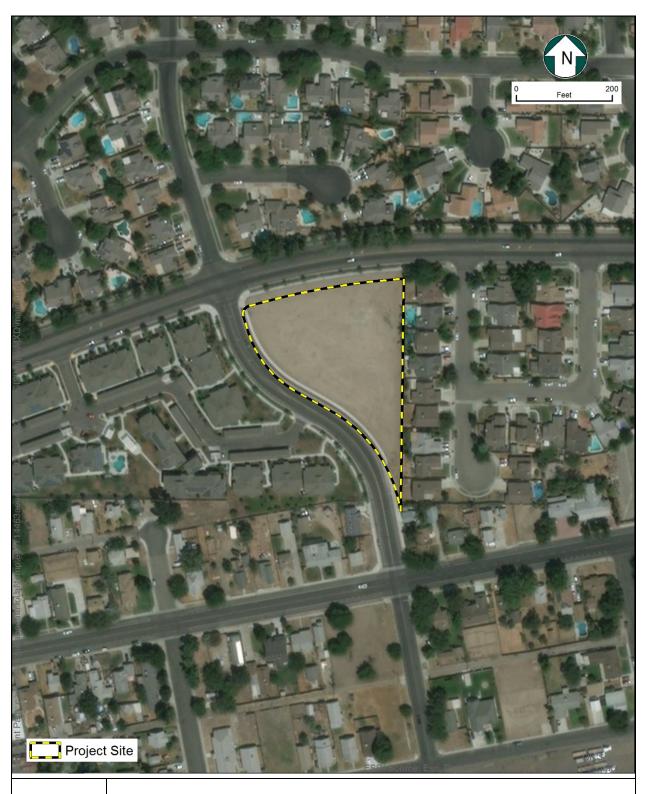
Phase II of the project will contain 28 senior affordable living apartments, a laundry room building, an outdoor BBQ area, a fenced dog park area, and covered parking. The site will consist of 5 separate building structures: three Type C buildings that provide eight 1-bedroom units, one Type B building that provides four 2-bedroom units, and the laundry building. Parking for the villas will include 31 parking spaces. All but three of the spaces will be covered. Three of the spaces will be van accessible parking spaces.

The applicant has stated that due to complications with recent workplace parameters regarding Covid-19, the plan checking timeline took longer than expected. Additionally, the State made an error in posting the NEPA review and the time period for comments was going to be very close to the projects expiration date. To be on the safe side, the application filed for the time extension.

The applicant is near to being able to pull building permits as of the date when the extension was submitted. In accordance with the Lemoore Municipal Code, the expiration date of the planning entitlements is July 9, 2020, two years after the original approval. Pursuant to Municipal Code Section 9-2A-7, the applicant or property owner may file a request for extension prior to the expiration of the permit, along with appropriate fees and application submittal materials. The filing of an application for an extension shall stay the expiration, but no subsequent permits (e.g., building permit) shall be issued during the stay period. Staff supports the one-year extension to give the developer more time to secure State funding and pull permits. If approved, the expiration date would be extended to July 9, 2021. There would be one more year of discretionary extensions remaining that the City could grant, as this proposal constitutes the first extension request by the applicant.

Attachments:

- Vicinity Map
- Draft Resolution for Approval
- Resolution No. 2018-04 Approving Major Site Plan Review No. 2018-04 and Conditional Use Permit No. 2018-01
- Approved Site Plan





Site Location

RESOLUTION NO. 2020-09

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF LEMOORE APPROVING A ONE YEAR TIME EXTENSION OF MAJOR SITE PLAN REIVEW NO. 2018-04 AND CONDITIONAL USE PERMIT NO. 2018-01 LOCATED AT THE SOUTHEAST CORNER OF CINNAMON DRIVE AND FOLLETT STREET IN THE CITY OF LEMOORE

At a Regular Meeting of the Planning Commission held on August 10, 2020, at 7:00 p.m. on seconded by Commissioner	
Resolution be adopted:	, and carried that the following
WHEREAS, Lemoore Pacific Associates II, County, has requested a one year time extension Conditional Use Permit 2018-01, located on 235 W.	
WHEREAS, Major Site Plan Review No. 20 01 were previously approved by the City of Lemoore	18-04 and Conditional Use Permit No. 2018- e on July 9, 2018; and
WHEREAS, the current extension date end extension application prior to the expiration date subsequent permits shall be issued during the st extension, the new expiration date will be July 9, 20	ay period. Upon approval of the timeframe
NOW THEREFORE, BE IT RESOLVED the Lemoore previously found that the Project is conside Small Structures exemption of the California Environment the approval of the Major Site Plan Review and	nmental Quality Act Guidelines in conjunction
BE IT FURTHER RESOLVED that the Planereby grants a one year extension to Major Site Planeremit No. 2018-01, which extension shall expire subject to compliance with the conditions of the o 2018-04.	on July 9, 2021. This extension is granted
Passed and adopted at a Regular Meeting of the F held on August 10, 2020, by the following votes:	Planning Commission of the City of Lemoore
AYES: NOES: ABSTAINING: ABSENT:	
	APPROVED:
	Pay Etchogoin Chairnerson
	Ray Etchegoin, Chairperson

ATTEST:			
Kristie Balev	, Commissi	ion Secretary	

RESOLUTION NO. 2018-04

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF LEMOORE APPROVING MAJOR SITE PLAN REVIEW NO. 2018-04 AND CONDITIONAL USE PERMIT NO. 2018-01 TO ALLOW A 28-UNIT SENIOR AFFORDABLE APARTMENT COMPLEX LOCATED ON THE SOUTHEAST CORNER OF CINNAMON DRIVE AND FOLLETT STREET

At a Regular Meeting of the Planning Commission of the City of Lemoore duly called and held on July 9, 2018, at 7:00 p.m. on said day, it was moved by Commission Member <u>Etchegoin</u>, seconded by Commission Member <u>Marvin</u> and carried that the following Resolution be adopted:

WHEREAS, Pacific West Communities, Inc., represented by Tim Sciacqua, has requested a conditional use permit to allow a 28-unit senior affordable apartment complex located on the southeast corner of Cinnamon Drive and Follett Street; and

WHEREAS, the site is currently vacant; and

WHEREAS, the zoning on the parcel is RLMD (Low/Medium Density Residential); and

WHEREAS, the project is categorically exempt from California Environmental Quality Act (CEQA) requirements per the new small structures exemption in Section 15303 of the CEQA Guidelines.

WHEREAS, the Lemoore Planning Commission held a duly noticed public hearing at their July 9, 2018, meeting.

NOW THEREFORE, BE IT RESOLVED that the Planning Commission of the City of Lemoore hereby makes the following findings regarding the proposed conditional use permit:

- 1. The proposed use is consistent with the general plan, any applicable specific plans, and all applicable provisions of this title. The project is within the allowed density.
- 2. The establishment, maintenance, or operation of the use applied for will not, under the circumstances of the particular case (location, size, design, and operating characteristics), be detrimental to the health, safety, peace, morals, comfort, or general welfare of persons residing or working in the neighborhood of such use or to the general welfare of the city. The use is compatible with the zone and existing uses in the vicinity.
- 3. The site of the proposed use is physically suitable for the type, density, and intensity of the use and related structures being proposed. Given the proposed plans, access to and from the site is suitable for the intensity of traffic. Follett Street and Cinnamon Drive are classified as collector streets under the City of Lemoore's General Plan and will provide connection between the proposed site for higher density apartments, downtown, and other parts of the city.
- 4. The project will not be contrary to the specific intent clauses, development regulations, or performance standards established for the zoning district in which it is located. The proposed use and related structures are compatible with other land uses, transportation, and service facilities in the vicinity. Due to the existing Phase I of the Cinnamon Villas, residents of the second phase will have access to the adjacent phase, with aligned accesses to both housing complexes. Residents will be eligible

for use of the amenities provided by the existing first phase of the existing Cinnamon Villas.

BE IT FURTHER RESOLVED that the Planning Commission of the City of Lemoore approves Major Site Plan Review No. 2018-05 and Conditional Use Permit No. 2018-01, subject to the following conditions:

- 1. The project shall be developed as per proposed Site Plan. Any deviation from the approved plans shall require an amendment to the prior approvals or approval of a new permit as determined by the City.
- 2. The applicant shall submit a Site Photometric (lighting) Plan at time of building permit submittal consistent with 9-5B-4 of the City Municipal Code. The plan shall demonstrate how project lighting shall be designed, located, installed, and maintained in order to prevent glare, light trespass, and light pollution.
- 3. The applicant shall submit a master landscape plan at time of building permit submittal. The landscape plan shall be compliant with MWELO, including but not limited to the following conditions:
 - a. Plan shall include square footages of landscaped area shown, water use calculations, and the material to be utilized in the dog park.
 - b. Turf shall be limited to no more than 25% of total landscape area. Fescue is not a permitted ground cover, per the City Ordinance.
 - c. Water use classifications shall be based on WUCOLS IV
- 4. An encroachment permit shall be obtained prior to commencement of work in a public right of way.
- 5. Delivery times shall be limited to between 7:00am and 7:00pm.
- 6. No on-street parking shall be allowed on Follett Street.
- 7. Trash enclosures shall be designed to accommodate refuse and recycling bins. The trash enclosure shall also include a roof structure and enclosed by a finished block wall.
- 8. The existing 8" wide brick stamping behind the curb shall remain consistent with Phase I stamping.
- 9. The project shall connect to existing sewer and water lines. Sewer and water services shall be installed per plans approved by the City Engineer.
- 10. Apartment unit identification shall be appropriately sized and located on buildings as directed by police and fire departments. Fire hydrant and fire department connection locations shall be located according to Fire Department recommendation.
- 11. Trees shall be planted in the parkway between the curb and sidewalk with a spacing of approximately 40' on center using trees from the City Street Tree list. Groundcover shall also be planted in the parkway.

- 12. The proposed grasses shall be drought tolerant grass.
- 13. Where the final grade difference between the project site and adjacent properties to the east is more than one foot, a block retaining wall shall be installed at least to the height of the grade of the higher property.
- 14. New 6' vinyl fencing, similar in style to the Phase I fencing shall be installed along the easterly project boundary. It may be installed on top of any required retaining wall.
- 15. A separate sign application and administrative approval process will be required before ordering or installing any signage or lease up signage and flags so that it is in conformity with the city's sign ordinance.
- 16. The developer shall comply with the San Joaquin Valley Air Pollution Control District (SJVAPCD) standards, and the City of Lemoore dust control requirements. The project will be required to meet Rule 9510 (indirect source review) and submit a construction notification to the district for the project and that compliance with the Districts Regulation VIII (fugitive dust) Rules will be monitored.
- 17. Outdoor lighting shall utilize energy efficient fixtures and lamps, such as LED, high pressure sodium, metal halide, low pressure sodium, hard wired compact fluorescent, or other lighting technology that is of equal or greater efficiency. All new outdoor lighting fixtures shall be energy efficient with a rated average bulb life of not less than ten thousand (10,000) hours.
- 18. Architectural features may be illuminated by up lighting, provided that the lamps are low intensity to produce a subtle lighting effect and no glare or light trespass is produced. Wherever feasible, solar powered fixtures should be used.
- 19. If not exercised within two years from the date of approval, this conditional use permit shall expire and become void, except where an extension of time has been approved in compliance with the Zoning Ordinance.

Passed and adopted at a Regular Meeting of the Planning Commission of the City of Lemoore held on July 9, 2018, by the following votes:

AYES: Etchegoin, Marvin, Boerkamp, Franklin, Koelewyn, Meade, Clement

NOES:

ABSTAINING:

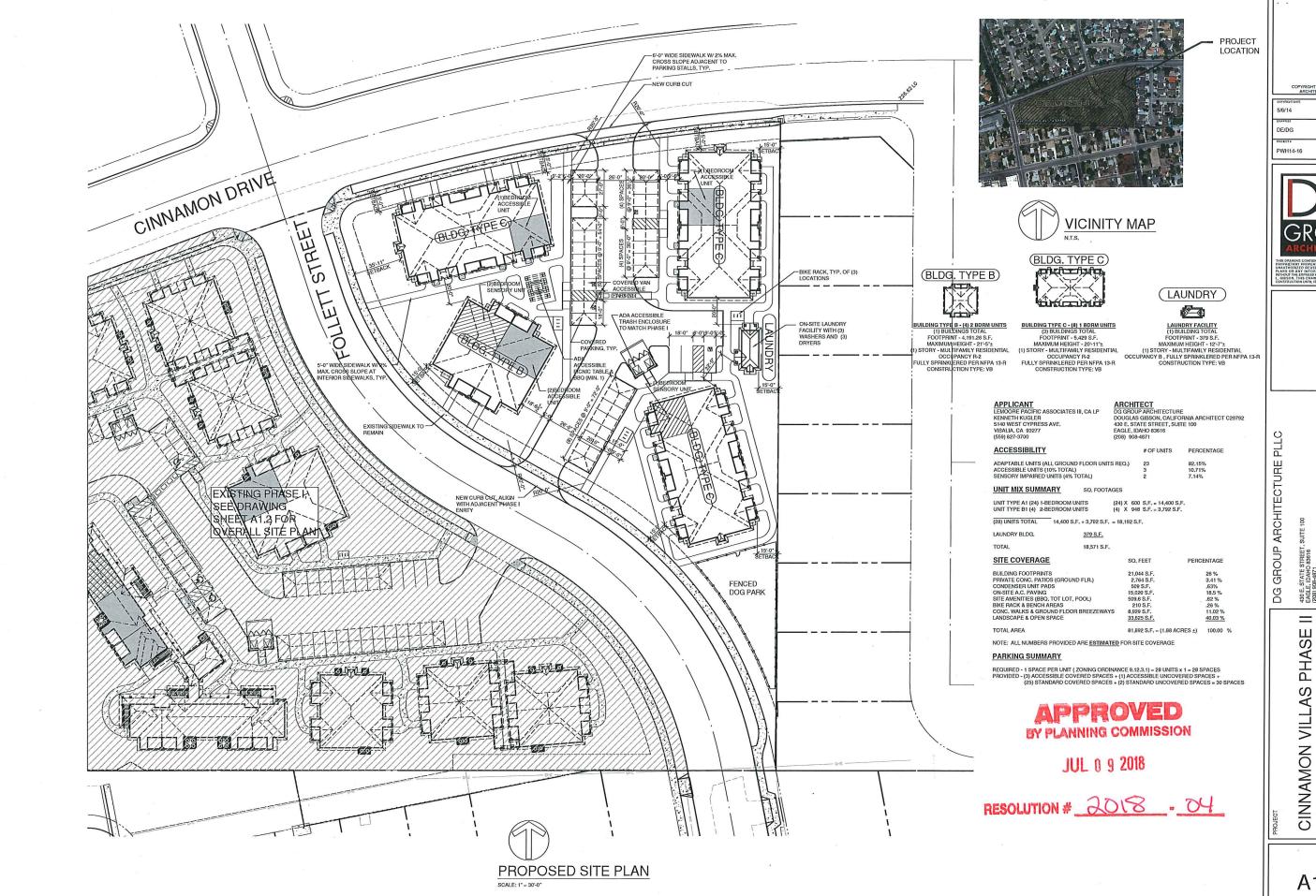
ABSENT:

APPROVED:

Bob Clement, Chairperson

ATTEST:

Kristie Baley, Commission Secretary

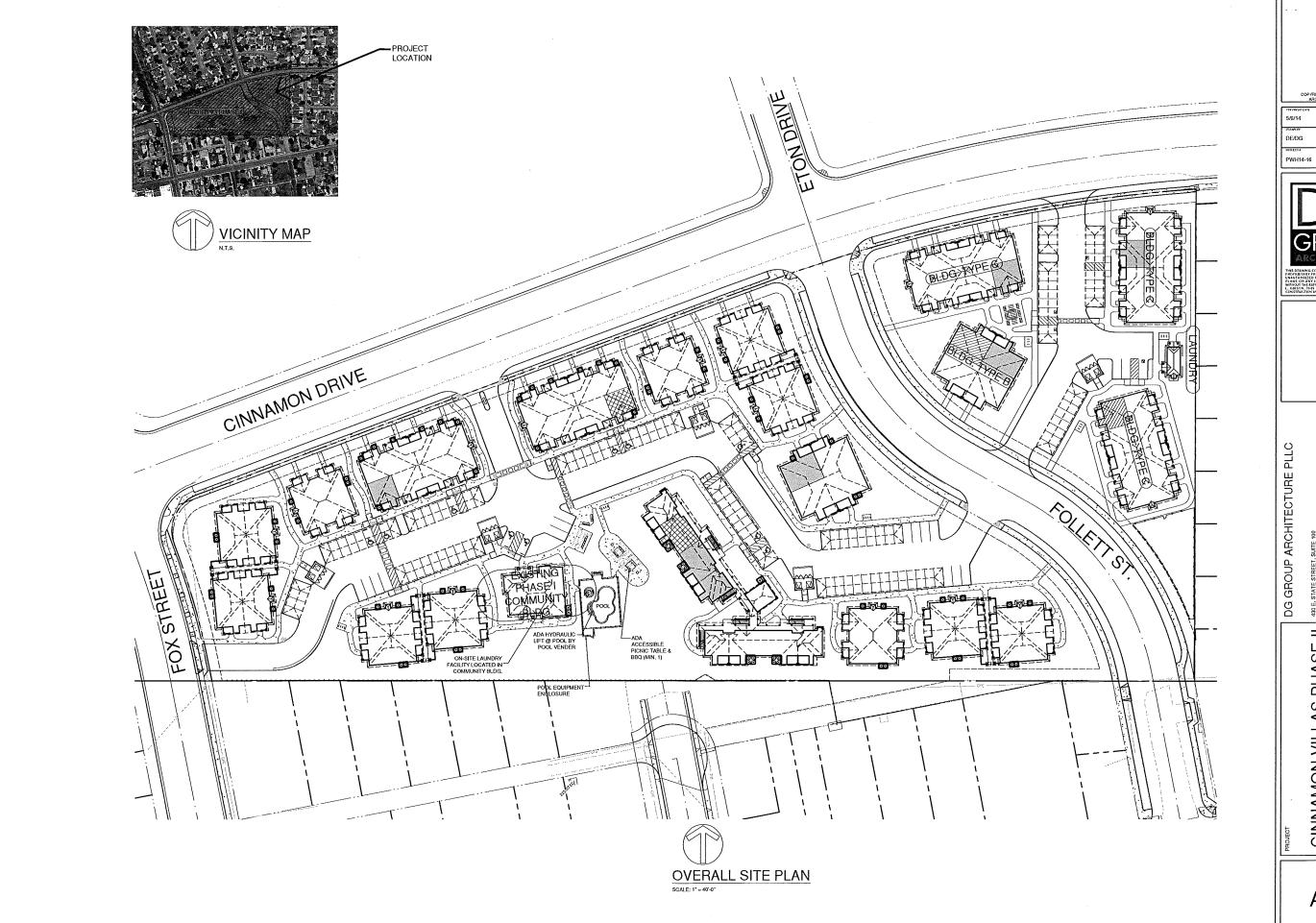


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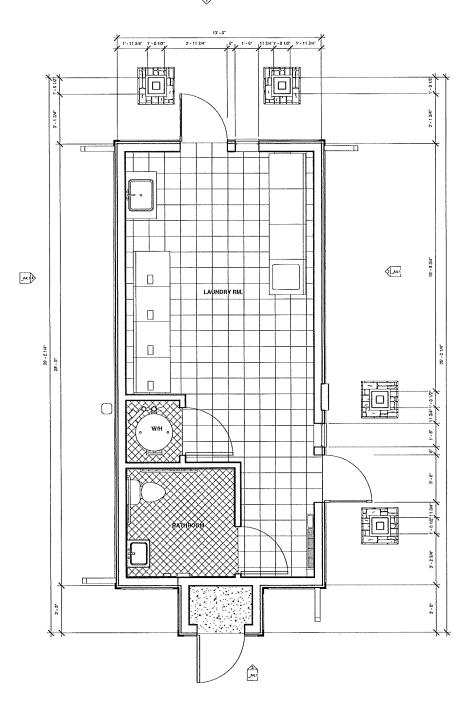


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CINNAMON VILLAS PHASE

A1.2

_A4.]



1 LAUNDRY ROOM - FLOOR PLAN (379 S.F.)
3/8" = 1'-0"

LAUNDRY FACILITY WILL INCLUDE A MINIMUM OF (3) WASHERS AND (3) DRYERS

05/06/14

DE/AW/DG

PWH14-16



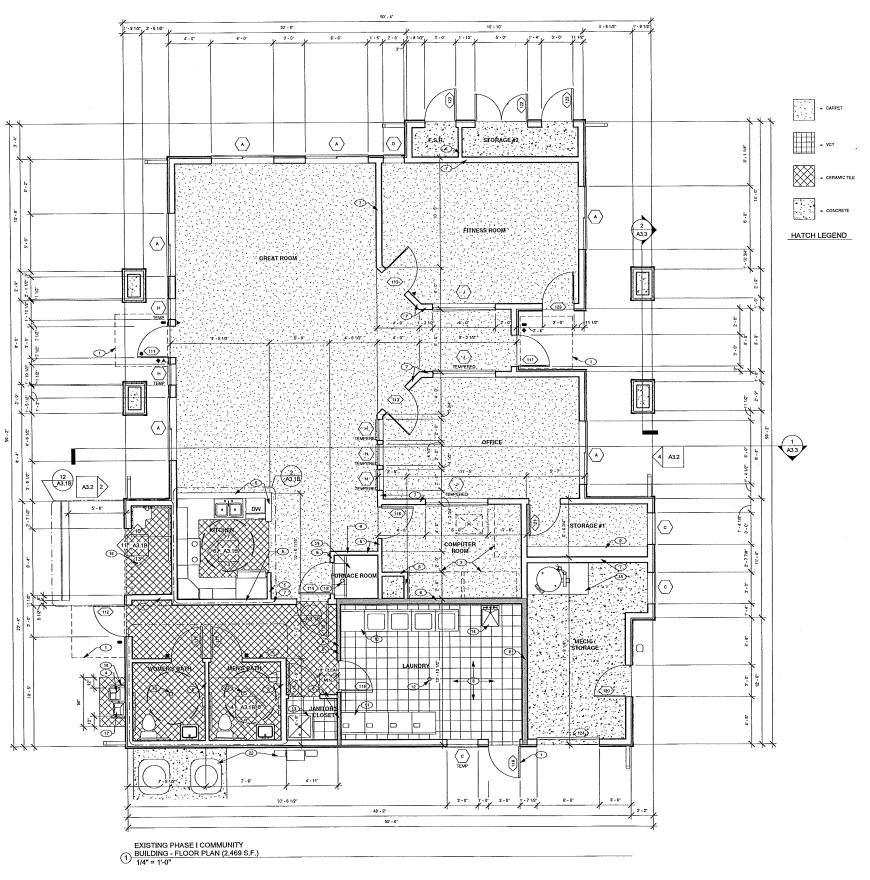
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430 E, STATE STREET, SUITE 10 EAGLE, IDAHO 83616 (208) 461-0022 fax (208) 461-3267

CINNAMON VILLAS PHASE II

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SCHEMATIC SET / NOT FOR CONSTRUCTION



- WIRE SHELVES,
- OWNER SUPPLIED COMPUTER AND WORK STATION, G.C. TO VERIF WITH OWNER.
- PARTIAL HEIGHT 2: WALL, SEE BUILDING SECTIONS FOR HEIGH
- 2x4 INTERIOR WALL W/ 5/6" GVP, EACH SIDE, FINISH PER SPECIFICAT FINISH SCHEDULE, SEE SHEET AR.4,

- FLOOR DRAIN, SLOPE FLOOR TO FLOOR DRAIN, SEE FLUMBING DRAW LOCATIONS, G.C. TO COORDINATE LOCATIONS WITH ALL DISCIPLINES
- JANITORS SINK, SEE PLUVEING DRAWINGS FOR LOCATIONS. UTILITY SINK, SEE PLUMBING DRAWINGS FOR LOCATIONS.
- PLATFORM FOR MECHANICAL/PLUMBING EQUIPMENT, G.C. TO BUILD ON-SITE PLATER AND PLUMB, CODES AND/OR REQUIREMENTS,

- NON-ALCOVE, CANE DETECTABLE SURFACE TO BE PROVIDED APP BEYOND OUTSIDE DINTS OF DRINNING FOUNTAIN FIXTURE, QUARI DALTILE "GOLDEN BROWN SURETREAD COTS, OR EQUAL
- PROVIDE BLOCKING IN WALL FOR FUTURE EQUIPTMENT, G.C. TO VERITY L
- M.E.P. EQUIPMENT, REFER TO M.E.P. SHEETS FOR WORE INF

- PROVIDE FULL BATT INSULATION ADJACENT SPACES.
- WHERE GYPSUM BOARD FINISH IS INDICATED IT SH SPECIFICATIONS A A US GYPSUM LEVEL 4, U.O.N.

- HARD FLOOR SURFACES IN RESTROOMS TO BE A HARD, NONABSCREENT SURFACE PER 2010 CBC SECTION 1210.1.
- AT TOLET REINFORCEMENT FOR GRAB BARS SHALL BE LOCATED AT THE HEIGHT OF 32 TO 36F, REINFORCEMENT SHALL BE CONBOTH SIDES OF TOLET, OR DO SIDE AND IN BACK OF TOLET, BACK REINFORCEMENT IS A MIX OF 47 IN LEIGHT SIDE! REINFORCEMENT IS A LIGICED WITH THE FRONT OF THE TAWK AND EXTENDED A MIX OF 26 IN FRONT OF TOLET FOXULAR DISC.
- GRAB BAR REINFORCEMENTS SHALL BE CAPABLE OF SUPPORTING A POINT LO OF 2504 MIN, REINFORCEMENTS SHALL BE A MIN OF 5" HIGH.

- ELECTRICAL DUTLETS TO BE LOCATED 15" MIN, FROM FINISH FLOOR OR CARPET SURFACE TO BOTTOM OF OUTLET BOX.
- CONFIRM LIGHTING LAYOUT AND ENERGY CODE COMPLIANCE FOR LIGHTING DESIGN WITH RESPECTIVE ELECTRICAL DRAWINGS SERIES.

- ALL INTERIOR DOORS SHALL HAVE A LEVEL AREA ON THE SIDE TO WHO DOOR SWINGS, EXTENDING 18" PAST THE STRIKE EDGE OF THE DOOR.
- INTERIOR PASSAGE DOORS SHULL HAVE HARDWARE CENTERED BETWEEN SY 44 ABOVE THE FLOOR AND SHULL BE OPERABLE WITH A SHOLL EFFORT BY A LEVER TYPE HARDWARE, HALLWAYS, WHERE OCCUR, SHALL BE A MIN, OF 55° IN WIDTH.
- EXTERIOR DOOR THRESHOLD AD A COMPLIANT,
- SEE DOOR, WINDOW, AND RINSH SCHEDULES FOR IT HARDWARE, WINDOWS, AND INTERIOR INFORMATION
- ALL EXIT DOORS TO BE LABELED CLEARLY, "THIS DOOR MUST REWAIN LINLOCK DURBYG BUSINESS HOURS, THIS DOOR TO REWAIN UNLOCKED WHENEVER THE BUILDING IS OCCUPIED."
- SEAL ALL PENETRATIONS IN BUILDING ENVELOPE PER CALIFORMA TITLE 24 REQUIREMENTS.

SIGN NOTES

- PROVIDE (1) 1/4" THICK 1/2" MENS & WOMEN'S GEOMETRIC SIGNS I A.F.F. ON RESTROOM ENTRANCE DOOR AND (2) PAUSE LETTER / BE SIGNS ON LATCH OUTSIDE OF RESTROOM ENTRANCE DOORS, M SIZ A.F.F. ON THE WALL, ALLOWANS APPROACH TO WITHIN 3".
- INTERNATIONAL SYMBOL OF ACCESSIBILITY TO BE MOUNTED AT ENTED DOOR TO PUBLIC FACILITIES AND SIMILAR USES.
- INTERNATIONAL SYMBOL OF ACCESS FOR HEARING IMPARED TO BE MOUNTED AT ENTRY DOOR.
- NOTE: SEE SHEET AS A FOR ADDITIONAL SIGNAGE NOTES AND DETAILS

2 CORNICE BOARD DETAIL
3/4" = 1'-0"

WALL LEGEND

05/06/14

DE/AW/DG

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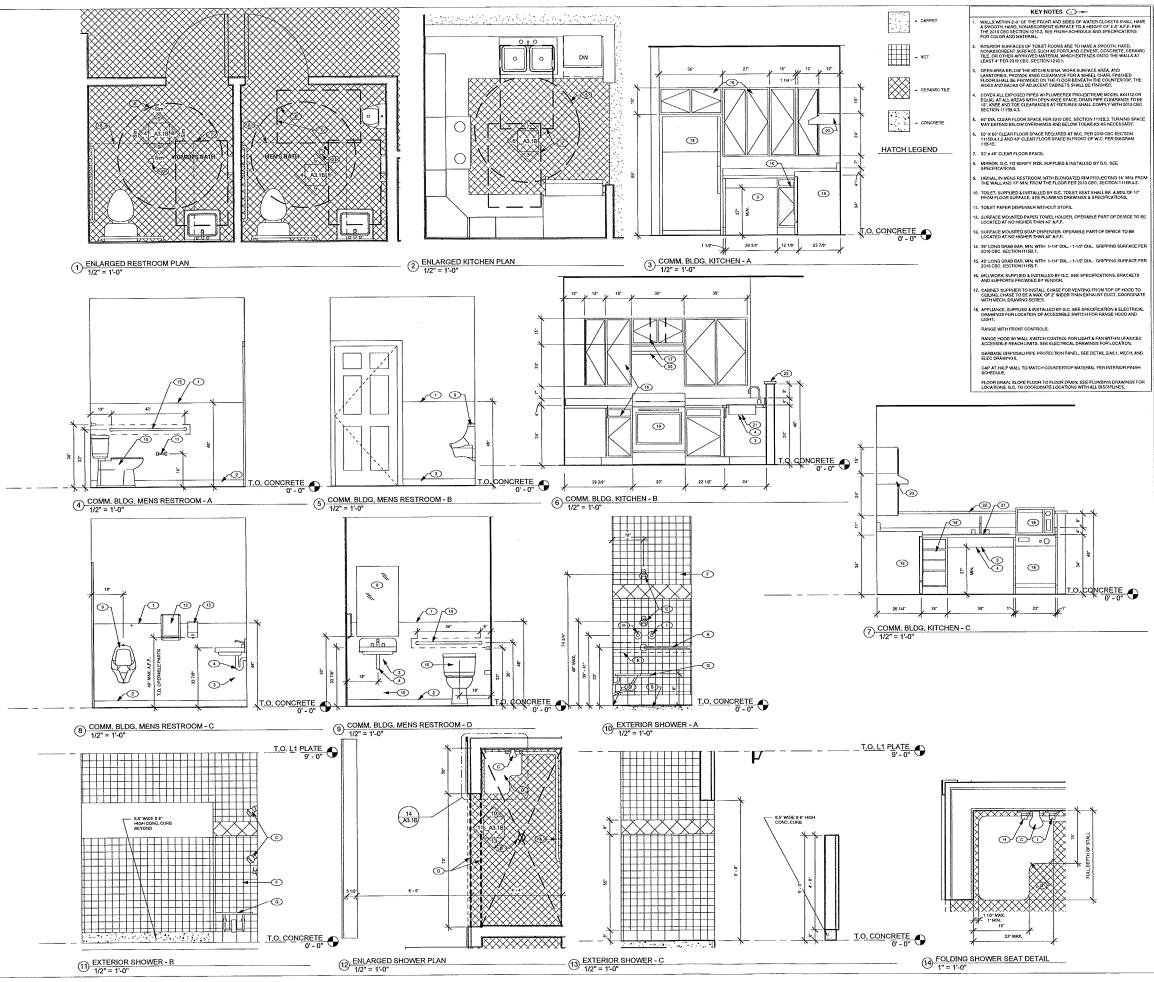
ALASKA - ARIZONA - CALÍFORNIA - COLORADO - HAWAII - IDAHO LOUISIANA - MONTANA - NEVADA - NORTH DAKOTA - OREGON -SOUTH DAKOTA - U.S.VI. - UTAH - WASHINGTON - WYOMING

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CINNAMON VILLAS PHASE

FOR CONSTRUCTION

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GENERAL NOTES

HARD FLOOR SURFACES IN RESTRIOUNS TO BE A HARD, NONABSORBENT SURFACE PER 2010 CBC SECTION 1210.1,

FLUSH VALVE SHALL BE LIDCATED ON WIDE SIDE OF TORLET AND MAX, 44" A.F.F.
VALVE SHALL BE OFFRABLE WITH OKE HAND MAX SHOULD NOT REQUIRE A TIGH GRASP, PINCHANG OR TWISTING OF WRIST, A WAX, 59 FORCE SHALL BE REQUIRE TO OFFRA

THE TOO OF BOY OF THE PEPALTRY WANDS OF SWITCHES USED TO COLUMN LIGHTS. THE REVOCATION AND OTHER EXPRONENTIAL CONTROL SALE LIGHTS. THE REVOCATION AS AND OTHER EXPRONENTIAL CONTROL SALE WAS THE REPORT OF THE REAL PROPERTY OF THE REAL PROPERT

LECTRICAL OUTLETS TO BE LOCATED 15" MINL FROM FINISH FLOCK! URFACE TO BOTTOM OF OUTLET BOX.

SEAL ALL PENETRATIONS IN BUILDING ENVELOPE PER CAUFORINA TITLE 24 REQUIREMENTS.

SHOWER KEY NOTES .

CORNERSTONE MODEL & 43316C ROTATING STYLE METAL LEVER HANDLE TEMPERATURE CONTROL VALVE.

PHASE CINNAMON VILLAS

_A3.1B

DE/AW/DG PWH14-16

05/06/14



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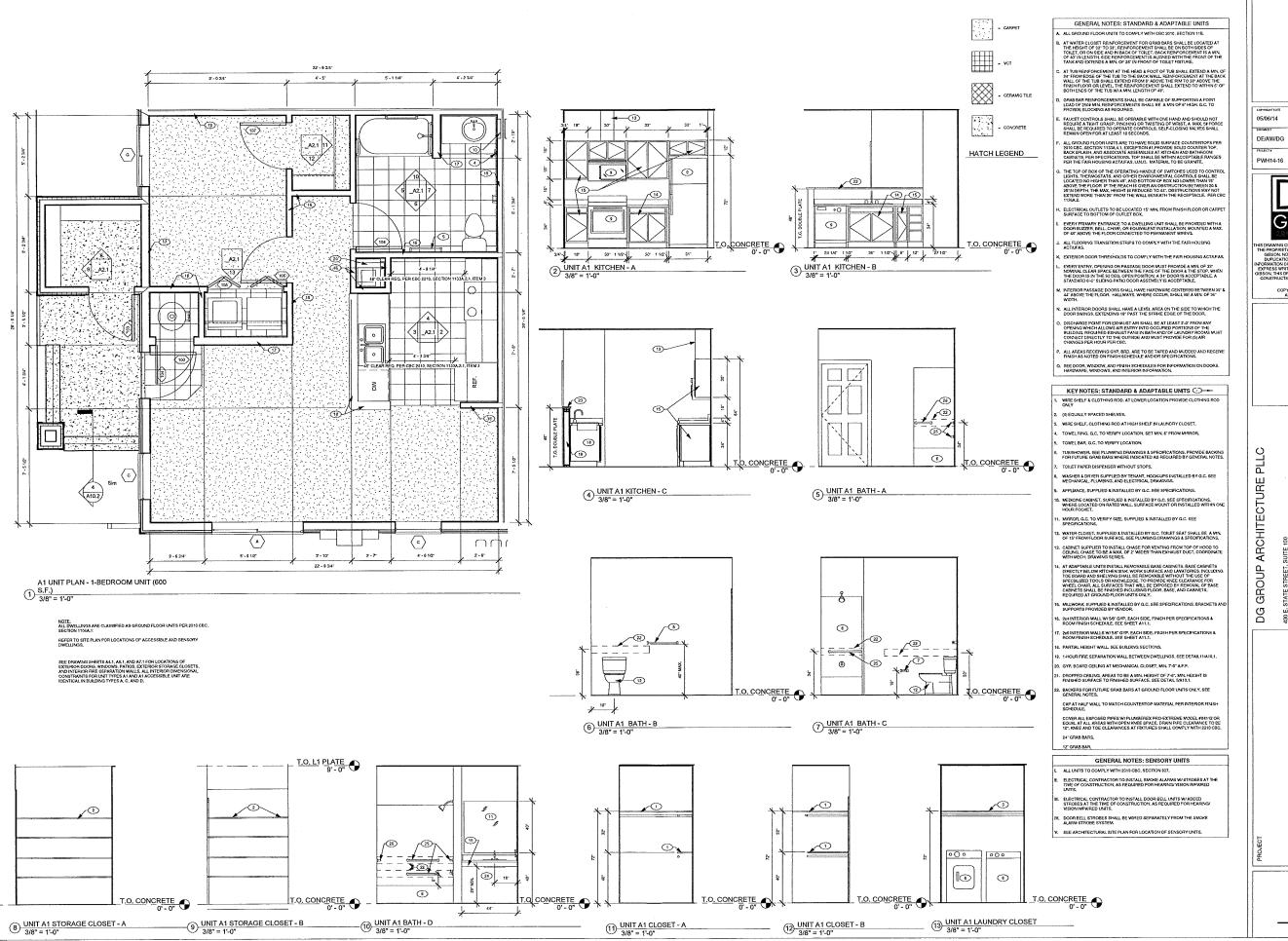
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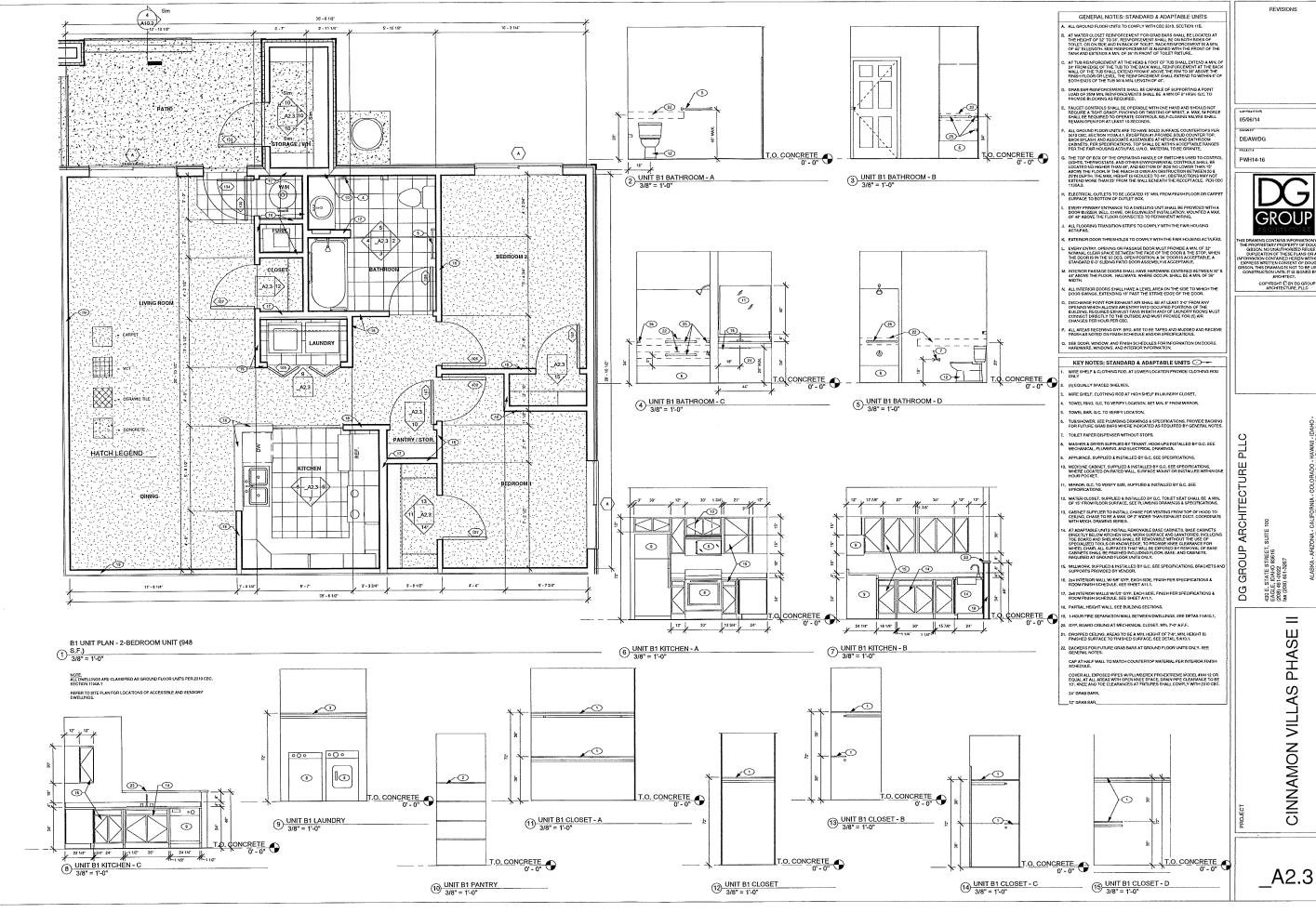
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CINNAMON VILLAS PHASE

_A2.1



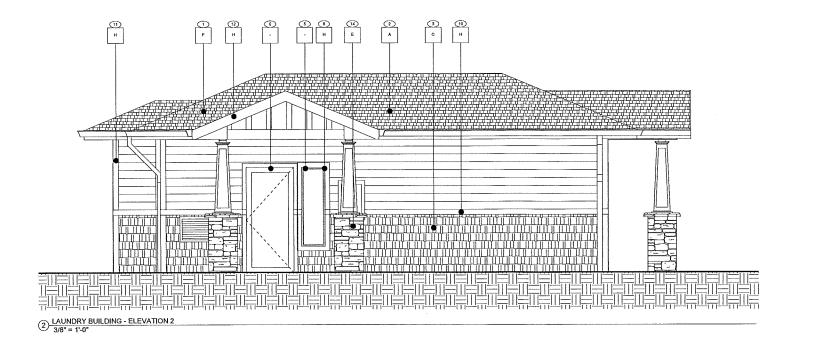
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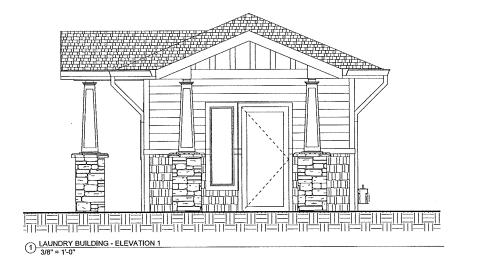
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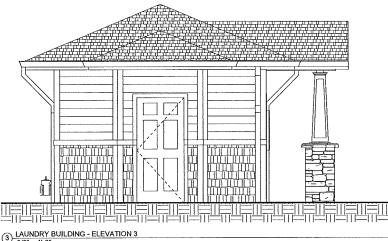
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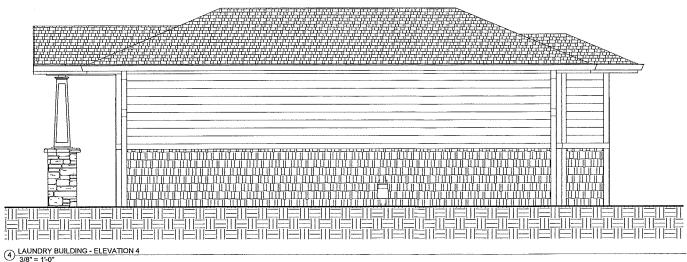
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GENERAL NOTES

ELEVATION (0-0") IS FOR REFERENCE CALY, SEE CIVIL DRAWINGS FOR ACTUAL FLOOR ELEVATIONS.

KEY NOTES 🕒 -

30 YEAR TYPE A COMPOSITE SHINGLE, TYP.

PRODICY HORIZONTAL VINYL SIDING OR APPROVED EQUAL

WHITE VINYL WINDOWS, SEE FLOOR PLANS, WINDOW SCHEDULE, AND SPECIFICATIONS.

EXTERIOR DOOR, SEE FLOOR PLANS, DOOR SCHEDULE, AND SPECIFICATION PAINT AS NOTED.

PRE-FINISHED METAL GUTTER, SEE ROOF F NOTED, DOWNSPOUT TO MATCH,

0. 2x HORZONTAL TRIM BY FRAVER W/ METAL WRAP, ALIGN TRIM AS SHOWN

1, 4" VERTICAL TRIM, TYP,

2. METAL WRAP AT 1x FINISH FASCIA, SEE DETAILS.

ARCHITECTURAL GABLE END VENT, PAINT AS NOTED, SEE ROOF PLANS.

I, CULTURED STONE, SET IN MORTAR BED WITH FLASHING AND WEEP SCREED. SEE SPECIFICATIONS. , PAINTED METAL HANDRAIL

6" EXPOSED REVEAL COUNTER FLASHING BE SIDING, PAINT TO MATCH ADJACENT SIDING.

7. MOSAIC CERAMIC TILE AT EXTERIOR SHOWER ENCLOSUR

8, SECTIONAL OVERHEAD GARAGE DOOR.

GAS METER. WINDOW OPENINGS MUST BE A MINIMUM OF 7-0" FROM GAS METERS. SEE WEP SHEETS FOR INFORMATION

D. CONDENSER UNIT, SEE WEP PLANS FOR INFORMATION

ELECTRICAL PANEL. SEE MEP SHEETS FOR INFORMATION, PAINT TO MATCH ADJACENT SIDING COLOR.

MATERIAL FINISHES TYPE 2: BLDG, TYPES B, D, & COMMUNITY BLDG.

A PRODIGY "VINTAGE WICKER" OR EQUAL

B HEAVY DASHL COLOR TO WATCH COLORTEK GREAT WALL.

ALSIDE SHAKE #195 OR EQUAL

COLOR TO MATCH ALSIDE "ADOBE CREAK

OWNES CORNING DRYSTACK LEDGESTONE "CH GAFELK WEATHERED WOOD OR EQUAL

VENT, GUTTER, AND DOWNSPOUT TO WATCH ALSIDE 'GLACIEF METAL FASCIA WRAP TO WATCH ALSIDE 'ADOBE CREAM'

NOTE: SOFFIT (NOT SHOWN) COLOR TO MATCH ALSIDE "ADOBE CREAM" ALL VENT TERMINATIONS AND OTHER EXTERNAL UTILITY EQUIPMENT TO BE PAINTED TO MATCH THE ADJACENT WALL SURFACE.

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CINNAMON VILLAS PHASE

_A4.1

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3 LAUNDRY BUILDING - ELEVATION 3

1 LAUNDRY ROOM - ELEVATION 1 N.T.S.



(4) LAUNDRY ROOM - ELEVATION 4 N.T.S.



3 LAUNDRY ROOM - ELEVATION 3 N.T.S.



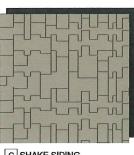
2 LAUNDRY ROOM - ELEVATION 2 N.T.S.



A STUCCO SIDING



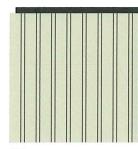
B HORIZONTAL SIDING
COLOR TO MATCH PRODIGY MINTAGE WICKER
OR EQUAL



C SHAKE SIDING COLOR TO MATCH ALSIDE SHAKE #005



D VENTS, BELLY BANDS,
WINDOW AND DOOR TRIM
COLOR TO MATCH ALBOOT "ADDRES CREAM"



E HORIZONTAL SIDING
COLOR TO MATCH PRODIGY COLONIAL IVOR



F BRICK VENEER



G ASPHALT SHINGLES
COLOR TO MATCH COLOR TO MATCH GAFELK'
WEATHERED WOOD' OR EQUIAL

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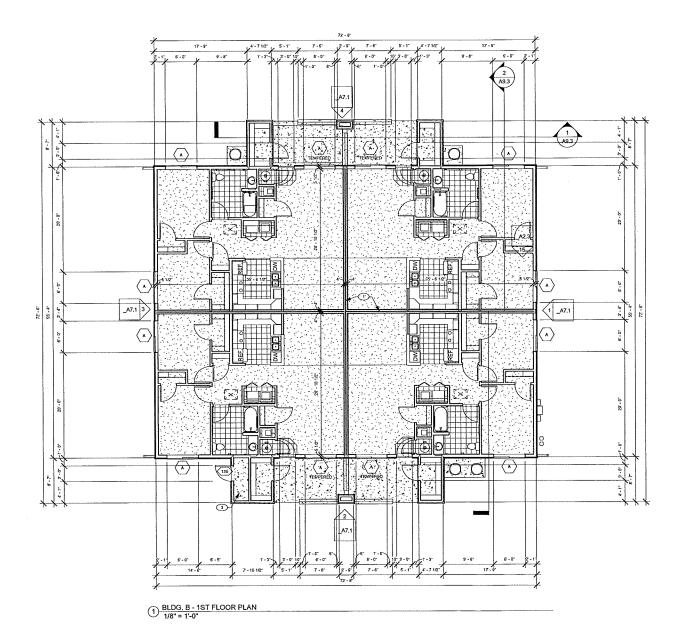
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CINNAMON VILLAS PHASE II

_A5.1



GENERAL NOTES

- REFER TO SHEETS A2.1 A2.5 FOR DETAILED FLOOR PLANS FOR TYPICAL UNIT TYPES.
- PROVIDE FULL WALL BATT INSULATION ISOLATING REDPROOMS FROM ALL ADJACENT SPACES, PROVIDE ALSO AT RESTROOM WALLS.
- PROVIDE FIRE BLOCKING IN CONCEALED SPACES, STUD WALLS AT THE CERT AND FLOOR LEVELS, AND AT 10-0" PRIERVALS BOTH VERTICAL AND HORIZONTAL.

- REFER TO ARCHITECTURAL SITE PLAN FOR EXTERIOR AVENTIES, PATIO SPECIFICATIONS, AND APPLICITEMANCES.
- SEE DETAIL SHEET FOR MORE INFORMATION AND BUILDING DETAILS. ALL WALL DIVENSIONS ARE FROM FACE OF STUD TO FACE OF STUD.

- PROVIDE BLOCKING AT ALL HAND RAILS, GRAB BARS, AND RELATED ASSEMBLIES AS INDICATED.

- G.C. TO INSTALL PORTABLE FIRE EXTINGUISHERS PER 2010 CBC. SECTION 90
- ACCITIESS AND THE DAYS LIVEREDS OF APPROVED BILLDIANS DESTRICTATION MALL BE PLACED BY A POSTRO THAT IS PLANY I FOR DIGINERAD WHISE EFFORM THE STREET OR PRODUCTION PROPERTY, 2010 CID. SECTION 1117EAS. G. OT COORDINATE LOCATION AND AWARDERS WITH ALL LOCAL JURISDOCK AUTHORITIES, WOURTHAY LEGAT HAD GREENTATION SHALL BE CONFIRMED WITH LOCAL JURISDOCK THE WORTH AND ORDER TATION SHALL BE CONFIRMED WITH LOCAL JURISDOCK THE AUTHORITIES.
- . AT ALL SINKS, SEALANT/ TYP, BOARD PATCH PLUMBING PENETRATIONS BENEATH SINKS.
- AT ALL PLATES INSTALL SWELLSTOP WATERSTOP 1" x 34" SELL PLATE SEALANT SEAL ALL PENETRATIONS IN BUILDING ENVELOPE PER CALIFORNIA TITLE 24 REQUIREMENTS.
- SEE ROOF PLANS FOR LOCATIONS A SPECIFICATIONS ON ATTIC ACCESS PANELS, COORDINATE LOCATIONS WITHOUT TRUSSES, WHERE ATTIC ACCES IS TO BE INSTALLED IN 144OUR CEILING PROVIDE RATED ASSEMBLY.
- SEE UNIT PLANS FOR DROPPED CEILING LOCATIONS.
- THRESHOLDS AT DOORWAYS SHALL NOT EXCEED 3/4" IN HEIGHT FOR SLIDING DOORS SERVING DWELLING LATTS OR 1/2" FOR OTHER DOORS PER CBC 2010 1004.1.7.

- DROPPED CEILING, AREAS TO BE A MALHEIGHT OF 7-6" A.F.F. MIN, HEIGHT IS RINSHED SURFACE TO RINSHED SURFACE, SEE DETAIL SA10.1.

- . 1-HOUR CORRIDOR WALL, SEE DETAIL 10/410-2.

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KEY NOTES (-) -

1-HOUR UNIT SEPARATION WALL PER 2010 CBC 709.1, SEE DETAIL 11/A10.1.

- I, FIRE SPRINGLER CLOSET.
- PRE-MANUFACTURED METAL STAIRS W/ CONCRETE TREADS.

- ELEVATOR, SEE DEFERRED SUBMITTALS ON T1,1 AND DETAIL 1/A10.3.

■ 1-HOUR WALL WALL LEGEND

B1 UNIT TYPE B1 UNIT TYPE B1 UNIT TYPE B1 UNIT TYPE

BLDG, B - UNIT LEGEND

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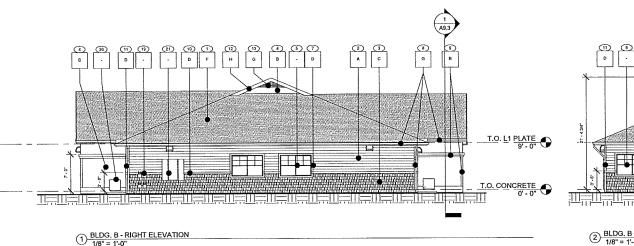
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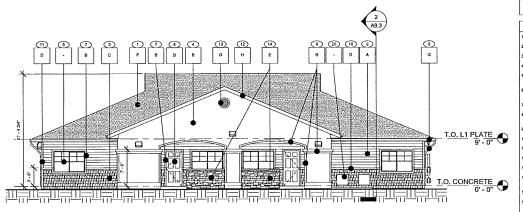
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CINNAMON VILLAS PHASE

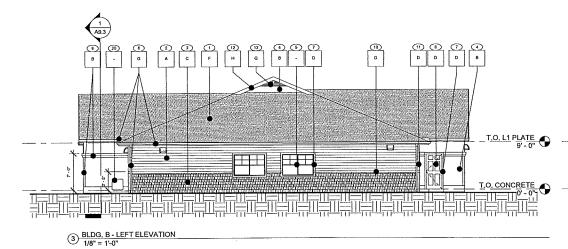
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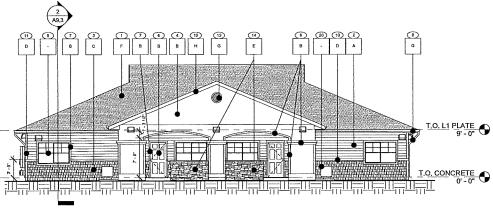
_A6.1





2 BLDG. B - REAR ELEVATION 1/8" = 1'-0"





4 BLDG, B - FRONT ELEVATION
1/8" = 1'-0"

GENERAL NOTES

- SEE BUILDING SECTIONS AND STRUCTURAL SERIES FOR PLATE HEIGHTS. ELEVATION (0-0") IS FOR REFERENCE ONLY, SEE CIVIL DRAY FLOOR ELEVATIONS.
- SEE ROOF PLANS FOR ATTIC VENTILATION REQUIREMENTS, SLOPES, AND OVERBUILD AREAS.
- SEE STRUCTURAL DR
- SEE SPECIFICATIONS FOR APPROVED MANUFACTURERS, NO SUBSTITUTION ALLOWED W/OUT PRIOR APPROVAL BY ARCHITECT OR ENGINEER OF RECO

KEY NOTES -

- PRE-FINISHED METAL GUTTER, SEE ROOF PL NOTED, DOWNSPOUT TO MATCH.
- RAISED STUCCO BANDS, COLOR SHOWS
- 24 HORZONTAL TRIM BY FRAVER W/ METAL
- . 4" VEHTICAL TRIM, TYP.
- ARCHITECTURAL GABLE END VENT, PAINT AS NOTED, SEE ROOF PLANS
- , CULTURED STONE, SET IN MORTAR BED WITH FLASHING AND WEEP SCREED SEE SPECIFICATIONS. . PAINTED METAL HANDRAIL
- 6" EXPOSED REVEAL COUNTER FLASHING B
- MOSAIC CERAMIC TILE AT EXTERIOR SHOW
- I, SECTIONAL OVERHEAD GARAGE DOOR.
- GAS METER, WINDOW OPENINGS MUST BE A MINIMUM OF 2 METERS, SEE MEP SHEETS FOR INFORMATION

MATERIAL FINISHES TYPE 2: BLDG, TYPES B, D, & COMMUNITY BLDG.

A PRODIGY WINTAGE WICKER OR EQUAL

ALSIDE SHAKE AGG OR EQUAL COLOR TO WATCH ALSIDE "ADOBE CREAM

OWNES CORNING DRYSTACK LEDGESTONE "CHAR

GAFELK WEATHERED WOOD" OR EQUAL

VENT, GUTTER, AND DOWNSPOUT TO MATCH ALSIDE "GLACIE

METAL FASCIA WRAP TO MATCH ALSIDE "ADDEE CREAM" ALL YENT TERMINATIONS AND OTHER EXTERNAL UTILITY EQUIPMENTO BE PAINTED TO MATCH THE ADJACENT WALL SURFACE.

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2 BLDG. B - ELEVATION 2 N.T.S.



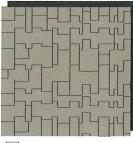
1 BLDG. B - ELEVATION 1 N.T.S.



A STUCCO SIDING COLOR TO MATCH COLORTEX "GREAT WALL"



B HORIZONTAL SIDING COLOR TO MATCH PRODIGY VINTAGE WICKER OR EQUIAL



C SHAKE SIDING



D VENTS, BELLY BANDS, WINDOW AND DOOR TRIM COLOR TO NATCH ALSIDE 'ADOBE CREAR'



E BRICK VENEER
COLOR TO MATCH COLOR TO MATCH COMMES
CORNING DRYSTACK "CHARDONNAY" OR EQUAL



F ASPHALT SHINGLES
COLOR TO MATCH COLOR TO MATCH GAFELK
WEATHERED WOOD' OR EQUAL



1 BLDG. B - ELEVATION 3 N.T.S.

2 BLDG. B - ELEVATION 4 N.T.S.



A STUCCO SIDING



B HORIZONTAL SIDING
COLOR TO MATCH PRODICY VINTAGE WICKER
OR EQUAL



C SHAKE SIDING
COLOR TO MATCH ALSIDE SHAKE 4055



D VENTS, BELLY BANDS,
WINDOW AND DOOR TRIM
COLOR TO MATCH ALRIDE* ADDRES CREAM*



E BRICK VENEER
COLOR TO MATCH COLOR TO MATCH CAMES
CORNING DRYSTACK "CHARDONNAY" OR EQUAL



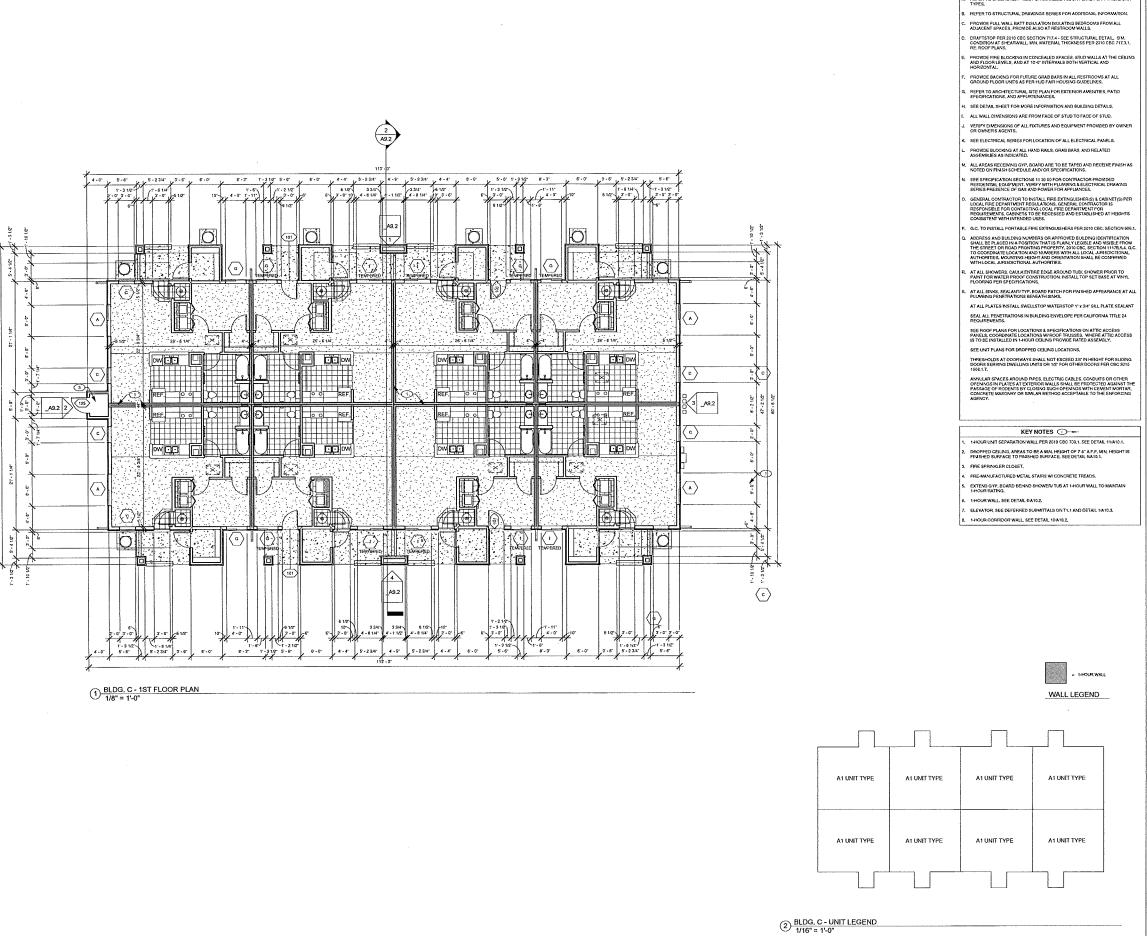
F ASPHALT SHINGLES
COLOR TO MATCH COLOR TO MATCH GAPE
WEATHERED WOOD' OR EQUAL

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_A8.2



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GENERAL NOTES

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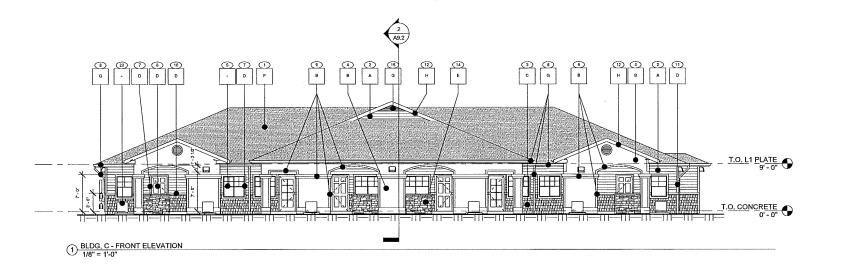
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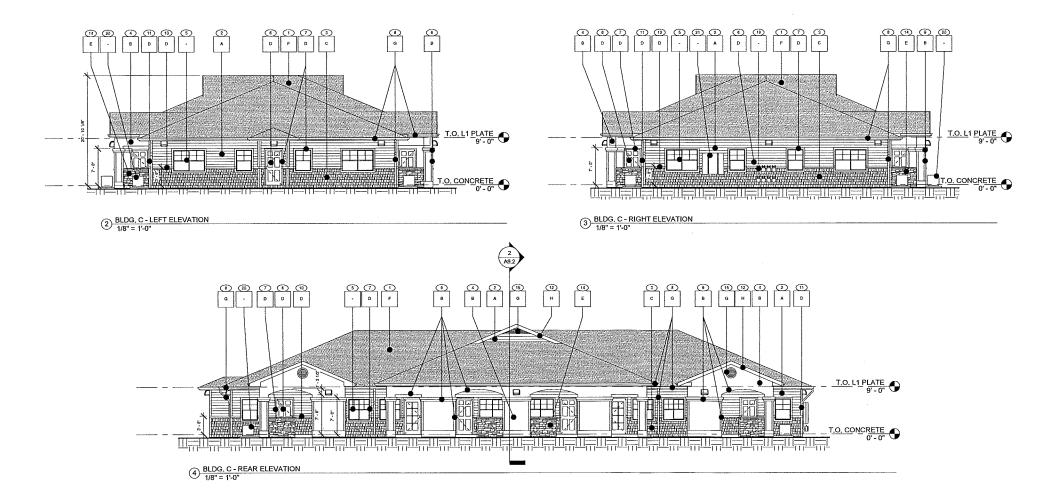
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CINNAMON VILLAS PHASE

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GENERAL NOTES

- SEE BUILDING SECTIONS AND STRUCTURAL SERIES FOR PLATE HEIGHTS.
- ELEVATION (V-0") IS FOR REFERENCE ONLY, SEE CIVIL DRAWINGS FOR ACTUAL FLOOR ELEVATIONS.
- SEE ROOF PLANS FOR ATTIC VENTILATION OVERBUILD AREAS.
- SEE STRUCTURAL DRA
- SEE CIVIL, PLUMBING, ELECTRICAL AND JOINT TRENCH DRAWINGS FOR LOCATIONS OF EXTERIOR MOUNTED UTILITIES AND METER BANKS,

KEY NOTES 🕣 🖚

- 30 YEAR TYPE 'A' COMPOSITE SHINGLE, TYP.
- AL SIDE PRODIGY SHINGLE/ SHAKE STYLE YINYI, SIDING OR APPROVED ED
- CEMENT STUCCO SYSTEM. TEXTURE HEAVY DASH.
- EXTERIOR DOOR, SEE FLOOR PLANS, DOOR SCHEDULE, AND SPECIFICATIONS PAINT AS NOTED.
- PRE-FINISHED METAL GUTTER, SEE ROOF P NOTED, DOWNSPOUT TO MATCH.
- RAISED STUCCO BANDS, COLOR SHOWN
-). 2x HORZONTAL TRUM BY FRAMER WI METAL WRAP
- 2, WETAL WRAP AT 12 FINISH FASCIA, SEE DETAILS.
- 3. ARCHITECTURAL GABLE END VENT, PAINT AS NOTED, SEE ROOF PLANS,
- CULTURED STONE, SET IN MORTAR BED WITH FLASHING AND WEEP SCREED SEE SPECIFICATIONS.
- 5. PAINTED METAL HANDRAIL
- 6" EXPOSED REVEAL COUNTER FLASHING BETW SIDING, PAINT TO MATCH ADJACENT SIDING.
- 7. MOSAIC CERAMIC TILE AT EXTERIOR SH
- P. GAS METER. WINDOW OPENINGS MUST BE A M METERS. SEE MEP SHEETS FOR INFORMATION
- . ELECTRICAL PANEL. SEE MEP SHEETS FOR INFORMATION, PAINT TO MATCH ADJACENT SIDING COLOR.

MATERIAL FINISHES TYPE 1: --

- HEAVY DASH, COLOR TO MATCH COL ALSIDE SHAKE #095 OR EQUAL
- COLOR TO WATCH ALSIDE "ADOBE CREAM
- GAFELK "WEATHERED WOOD" OR EQUAL
- VENT, GUTTER, AND DOWNSPOUT TO WATCH ALSIDE "GLACKER WHITE
- METAL FASCIA WRAP TO MATCH ALSIDE "ADOBE CREAM"

NOTE: SOFFIT INOT SHOWN COLOR TO MATCH ALSIDE "ADOBE CREAM" ALL VENT TERVINATIONS AND OTHER EXTERNAL UTILITY EQUIPMENT TO BE PAINTED TO MATCH THE ADJACENT WALL SURFACE.

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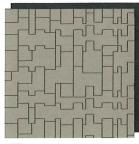
1 BLDG. C - ELEVATION 1 N.T.S.



A STUCCO SIDING COLOR TO NATCH COLOR TEXT ON A COLOR TO NATCH COLOR TEXT ON A COLOR TEXT ON A



B HORIZONTAL SIDING
COLOR TO MATCH PRODIGY COLONIAL IVORY OR EQUAL



C SHAKE SIDING
COLOR TO MATCH ALSIDE SHAKE 4056



D VENTS, BELLY BANDS,
WINDOW AND DOOR TRIM
COLOR TO MATCH ALSIDE*ADOSE CREAM*



E BRICK VENEER

COLOR TO MATCH COLOR TO MATCH CAMES
CORNING DRYSTACK "CHARDON NA" CRECIAL.



F ASPHALT SHINGLES
COLOR TO MATCH COLOR TO MATCH GAFELK
WEATHERED WOOD' OR EQUAL

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2 BLDG. C - ELEVATION 4 N.T.S.



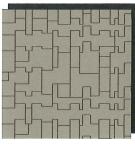
1 BLDG. C - ELEVATION 3 N.T.S.



A STUCCO SIDING
COLOR TO MATCH COLORTEX "ONE OR EQUAL



B HORIZONTAL SIDING
COLOR TO MATCH PRODICTY COLORIAL IVORY OR
EQUIA.



C SHAKE SIDING
COLOR TO MATCH ALSIDE SHAKE #095



D VENTS, BELLY BANDS,
WINDOW AND DOOR TRIM
COLOR TO MATCH ALSIDE *ADOBE CREAM*



E BRICK VENEER
COLOR TO MATCH COLOR TO MATCH OWNES
CONNING DRYSTACK COMPONIUM OR EQUAL



F ASPHALT SHINGLES
COLOR TO MATCH COLOR TO MATCH GAFELK
WEATHERED WOOD' OR EQUAL

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_A10.2



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Staff Report

To: Lemoore Planning Commission Item No. 6

From: Steve Brandt, City Planner

Date: August 3, 2020 Meeting Date: August 10, 2020

Subject: Request by Tim Palmquist, on behalf of Victory/Pharris Lemoore, LLC,

to extend the approval of Victory Village Vesting Tentative Subdivision

Map No. 2005-02, Tract 845 for one year (APN: 023-510-055).

Proposed Motion:

Approve a one-year extension of time for the Victory Village Vesting Tentative Subdivision Map No. 2005-02, Tract 845 and accompanying Planning entitlements, in accordance with Resolution No. 2020-10.

Discussion:

On July 27, 2020, Tim Palmquist, representing Victory/Pharris Lemoore, LLC, submitted a written request to extend the approval of the Tract No. 845 Tentative Subdivision Map No. 2005-02 for one year. This project was originally approved by City Council Resolution 2006-28 on June 6, 2006. Due to a combination of previous time extensions granted by the Planning Commission and automatic time extensions authorized by the State in response to the Great Recession, the map is currently set to expire on August 6, 2020.

The State Subdivision Map Act governs how cities are to process and administer the creation of new parcels and subdivisions. The Map Act states that a final subdivision map must be filed with the City before the tentative map approval expires. The filing of a final map is usually the beginning of significant investment on the part of the developer. Tentative maps are initially approved for two years. Applicants may request up to six years of extensions. The State legislated a number of automatic extensions of valid tentative maps in response to the Great Recession because there were numerous approved tentative maps with no market for home sales at that time.

There have been several additional hurdles this development project has had to endure besides the Recession, including a Readiness and Environmental Protection Integration (*REPI*) *Program* deal with Naval Air Station Lemoore to reduce the overall size of the project.

The tentative map approved 279 lots and two parks, but now only 51 lots and one park will be constructed.

Staff supports the one-year extension to give the developer more time to prepare for construction. Since the current extension date ends August 6, 2020, the filing of an application for an extension on July 27, 2020 shall stay the expiration for 60 days from the receipt of a time extension application. If the one-year extension is approved, the expiration date would be extended to August 6, 2021. There would be up to two more one year discretionary extensions that the Planning Commission could grant after that. It is not expected that the State will grant any more automatic extensions.

Site Plan Review No. 2005-01, Conditional Use Permit 2005-02, and Planned Unit Development 2005-01 were also approved. In accordance with Municipal Code Section 9-2A-9, these entitlements are also subject to a two-year expiration date if not exercised. The timeframe extensions as proposed to be granted for the Tentative Tract Map shall be applied to all accompanying entitlements. The project would still adhere to the adopted conditions as adopted originally for the project.

<u>Timeline of Extensions of Victory Village Tentative Map</u>

- Vesting Tentative Map Tract 845 Approved June 6, 2006
- Original Expiration Date June 6, 2008
- Application (2008) for Extension Received and Withdrawn
- Senate Bill 1185 (2008 1 Year Automatic) Extended Map to June 6, 2009
- Assembly Bill 333 (2009 2 Years Automatic) Extended Map to August 6, 2011
- Application (2011) for Extension Received and Withdrawn
- Assembly Bill 208 (2011 2 Years Automatic) Extended Map to August 6, 2013
- Assembly Bill 116 (2013 2 Years Automatic) Extended Map to August 6, 2015
- Senate Bill 1303 (2015 2 Years Automatic) Extended Map to August 6, 2017
- Planning Commission (2015 Additional 2 Years) Extended Map to August 6, 2019
- Planning Commission (2019 Additional 1 Year) Extended Map to August 6, 2020
- Application (2020) for a 1 Year discretionary Extension to Extend Map to August 6, 2021

Attachments:

- 1) Vicinity Map of Site Location
- 2) Draft Resolution for Approval
- 2) Vesting Tentative Subdivision Map No. 2005-02
- 3) Resolution No. 2006-23 Mitigated Negative Declaration and General Plan Amendment
- 4) Resolution No. 2006-28 Original Approval
- 5) Resolution No. 2008-26 EIR
- 6) Resolution No. 2009-15 Amendment to Resolution No. 2006-28
- 7) Resolution No. 2011-48 Bush Street Abandonment
- 8) Resolution No. 2013-30 General Plan Amendment No. 2013-01
- 9) Resolution No. 2018-18 General Plan Amendment No. 2018-01
- 10) Staff Report Item No. 5 for One Year Time Extension 07/08/2019
- 11) Approved Minutes for Planning Commission Meeting 07/08/2019



Site Location

RESOLUTION NO. 2020-10

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF LEMOORE APPROVING A ONE-YEAR TIME EXTENSION OF TENTATIVE SUBDIVISION MAP NO. 2005-02 (TRACT 845) AND ACCOMPANYING PLANNING ENTITLEMENTS LOCATED ON THE NORTHEAST CORNER OF BUSH STREET AND MARSH DRIVE, DIRECTLY NORTH OF WEST HILLS COLLEGE IN THE CITY OF LEMOORE

At a	Reg	ular Mee	ting	of the F	Plan	ning (Comm	issic	on of t	he Ci	ty c	of Len	noore (C	ity)	duly called and
held	on	August	10,	2020,	at	7:00	p.m.	on	said	day,	it	was	moved	by	Commissione
, seconded by Commissioner									, ar	nd carrie	d th	at the following			
Resc	lutio	n be add	ptec	l:											_

WHEREAS, Tim Palmquist, on behalf of Victory/Pharris Lemoore, LLC, has requested time extension for Tentative Subdivision Map 2005-02 (Tract 845), Site Plan Review No. 2005-01, Conditional Use Permit 2005-02, and Planned Unit Development 2005-01, located on the Northeast Corner of Bush Street and Marsh Drive, directly North of West Hills College (APN: 023-510-055); and

WHEREAS, Tentative Subdivision Map 2005-01 and accompanying planning entitlements were previously approved by the City of Lemoore on June 6, 2006; and

WHEREAS, the Tentative Subdivision Map has been utilizing statutory timeframe extension up until, 2017; and

WHEREAS, the Tentative Subdivision Map has been extended three years per the City of Lemoore Municipal Code Section 8-7F-10 for discretionary timeframe extensions; and

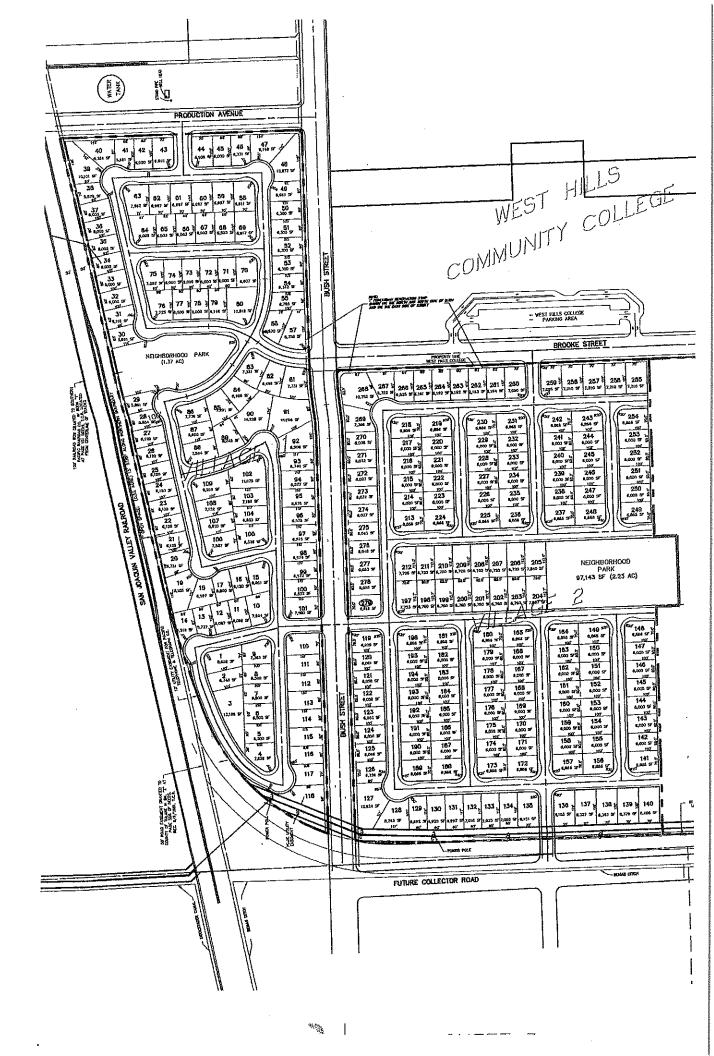
WHEREAS, the Tentative Subdivision Map may be granted two more years of timeframe extensions; and

WHEREAS, the current extension date ends August 6, 2020, upon the receipt of a time extension application, the project is subject to an automatic 60-day stay period to allow for review per the Municipal Code.

NOW THEREFORE, BE IT RESOLVED that the Planning Commission of the City of Lemoore hereby finds that the potential environmental effects of the Project were adequately analyzed by the Mitigated Negative Declaration that was adopted pursuant to the California Environmental Quality Act (CEQA) on June 6, 2006 in conjunction with the approval of the Tentative Subdivision Map and accompanying planning entitlements.

BE IT FURTHER RESOLVED that the Planning Commission of the City of Lemoore hereby grants a one-year extension to Tentative Subdivision Map 2005-01, Site Plan Review No. 2005-01, Conditional Use Permit 2005-02, and Planned Unit Development 2005-01, which extension shall expire on August 6, 2021. This extension is granted subject to compliance with the conditions of the original approval contained in Resolution 2006-28.

Passed and adopted at a Regular Meeting of the Pheld on August 10, 2020, by the following votes:	lanning Commission of the City of Lemoore
AYES: NOES: ABSTAINING: ABSENT:	APPROVED:
	Ray Etchegoin, Chairperson
ATTEST:	
Kristie Baley, Commission Secretary	



RESOLUTION NO. 2006-28

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF LEMOORE
APPROVING TENTATIVE SUBDIVISION MAP NO.2005-02/
PLANNED UNIT DEVELOPMENT NO.2005-01/ CONDITIONAL USE PERMIT NO.2005-01/
SITE PLAN REVIEW 2005-01 FOR COUNTY TRACT NO. 845 – VICTORY VILLAGE PHASES I & II
APPLICATIONS BY CENTEX HOMES & ASHLAN DEVELOPMENT

At a Regular Meeting of the City Council of the City of Lemoore duly called and held on June 6, 2006, at 7:30 p.m. on said day, it was moved by Councilmember PLOURDE, seconded by Councilmember MARTIN and carried that the following Resolution be adopted:

WHEREAS, Centex Homes and Aslan Development have submitted applications for Tentative Subdivision Map No. 2005-02/ Planned Unit Development No. 2005-01/ Conditional Use Permit No. 2005-01/ Site Plan Review 2005-01/ Environmental Assessment 2005-16 to subdivide and develop Tract 845, Victory Village consisting of approximately 71.31 acres into 279 single family lots; and

WHEREAS, the tract on which the development is proposed is located on the north and south sides of the Bush Street alignment, west of the Production Avenue alignment, and generally west of the West Hills College Lemoore site and east of the old abandoned north-south Rossi railroad alignment and is described as portions of Assessor Parcel Nos.023-510-031 for the 279 lots, 023-510-026, 029, & 030 and 023-510-011 for general plan and zone amendments only; and

WHEREAS, the Lemoore Planning Commission held a duly noticed public hearing on May 22, 2006 meeting, on the above-stated proposal as required by the City of Lemoore Municipal Code, it being established that all notice requirements as set forth in Section 9-15B-2C of the said code have been complied with; and

WHEREAS, the Planning Commission reviewed the Environmental Impact Assessment pertinent to the proposal and determined that it will not have any significant effect on the environment and recommended the Council's adoption of the Mitigated Negative Declaration pursuant to California Environmental Quality Act, as amended, and approval of an amendment to the General Plan Diagram and a separate Resolution has been proposed to City Council: and

WHEREAS, the Planning Commission reviewed proposed amendments to the Zoning Map and a separate Ordinance has been proposed to City Council); and

WHEREAS, Section 9-15B-2G of the Lemoore Municipal Code requires the City Council to review the recommendation of the Planning Commission on a Tentative Subdivision Map / Planned Unit Development / Conditional Use Permit for proposal in a meeting held more than ten (10) days after the Planning Commission's decision; and

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Lemoore does hereby approve Tentative Subdivision Map No. 2005-02/ Planned Unit Development No. 2005-01/ Conditional Use Permit No. 2005-01/ Site Plan Review 2005-01/ to subdivide and develop Tract 845, Victory Village Phases I and II consisting of approximately 71.31 acres into 279 single family lots as applied for by Aslan Development and Centex Homes with the following 38 conditions (so long as Resolution 2006-23, regarding a Mitigated Negative Declaration and General Plan Amendment for the Victory Village property and Ordinance 2006-04 approving Zone changes are also approved). Such approval shall be valid two years from the approval date of the Ordinance:

1. The Subdivision Map shall be developed in accordance to the plans submitted 5/12/06 unless otherwise conditioned in this report.

- Should the applicant wish to record a Final Subdivision map prior to the expiration date of their Williamson Act contract, it can only be processed by finalizing the Tentative Cancellation approved by City Council at their May 2, 2006 meeting and paying the appropriate cancellation fees.
- 3. The subdivision shall request inclusion in the new lighting and landscaping maintenance district (LLMD) to maintain streets, curbs, gutters, block walls, landscape easement areas and parkways along collector and arterial streets, parks, and bike path areas, center medians, surrounding landscaping, lighting and open space areas.
- 4. In order to address density shortfalls (i.e. densities below those required by the General Plan) within Phase I and II of the Victory Village subdivision areas, a document shall be recorded on Assessor's Parcel #023-510-011 requiring the provision of at least 85 units on the approximately 10.53 acres of the RM-3 zoned area that remains, but can be removed if another future phase makes up the shortfall.
- 5. Elevations, floor plans and an overall plot plan shall be submitted to the Community Development Department for Planning Commission to review at a public hearing for their approval and City Council's concurrence prior to a Final Map being recorded for any phase of development. The front yard setbacks should vary to encourage variety in the line of sight visible to the public, and such setbacks shall be delineated on the overall plot plan. Due to noise concerns as well as visual appearances, the rear yard setbacks for properties that back onto arterial and collector streets or the railroad shall have a 20' minimum rear yard setback and be delineated on the overall plot plan. Additionally, the maximum lot coverage and minimum percentage of landscape respectively shall be as follows; in low-medium density residential 40%/25%, in medium density residential 50%/20%, and in high density residential 60%/15% and reflected in the overall and individual plot plans.
- 6. All walls and fencing designs shall be submitted with the improvement plans for approval by the Planning and/or Parks Departments as generally outlined in the staff report. Seven foot (7') high decorative masonry block walls shall be installed between the residential units and landscape easement areas adjacent to the north and south sides of Bush Street and the west sides of Production Avenue and the proposed Brooke Street. These decorative masonry block walls shall also be installed along the west sides of lot 118 and lots 127 through 140 due to 115 kv power line issues.
- 7. A noise barrier of at least 7' in height is required along the northern property lines of the residential properties adjacent to the east-west railroad line to reduce the projected noise exposure level to less than 65 dB CNEL exterior for the residential units. Appropriate details shall be included in the improvement drawings to adequately insure noise is being properly addressed. If a block wall must be built, such wall should meet the decorative block wall standards, since the future bike path will abut this area. Second story homes in the line sight of the railroad or arterial/collector streets will need additional attenuation to reduce the CNEL by 10 over regular widows being used. This should be able to be accommodated through thicker windows and specifications shall be provided for such in the PUD elevation plan submittal. Planting of trees along the entire north side of the fence/noise barrier that will grow to the height of second floor spaces shall also be planted to help reduce noise from the railroad.

Fencing along lots 29, 30, 204, and 205 should be black or dark green powder coated wrought iron to maintain eyes on the park.

Chain link fencing shall not be allowed in the subdivision and CC&R's need to reflect this with the exception of temporary fencing needed along the southerly portion of Phase II's 2.23 acre park area which will square off the park.

- 8. Trees in landscape buffer areas will be planted within the landscape easement on the street side of the block wall or along the north side of the wall toward the railroad with trees planted every 20'. Parkway shall include trees spaced 40' on average. In order to improve the aesthetics of the entryways, medians shall be placed at all entryways from the arterial/collector streets into the subdivision. These median islands should be 8' in width in the center of a 40' curb-to-curb street within a 60' right-of-way.
- 9. Immediate Traffic Mitigation with project: The single stripped eastbound traffic lane on Bush between Belle Haven and the SB ramp would need to be restriped/widened to accommodate two eastbound lanes of traffic so that there is 1 separate through lane going eastbound and 1 shared through-right lane (this may be able to be accommodated within the existing paving) prior to the first occupancy being given.
- 10. Short Term Traffic Mitigation with project: It is anticipated that after 570 new dwelling units or 154,000 square feet of commercial space (or a combination thereof) are developed west of 191/2 Avenue the installation of traffic signals at both the southbound and northbound State Route 41 ramps at Bush Street may be warranted. Therefore, for each of the first 570 building permits pulled for new dwelling units or equivalent dwelling units a special "interim solution interchange fee" of \$ 1,797.80, shall be paid (fee subject to the pro-rata share reimbursements if so determined by a future impact fee study at the same cost index inflator that is used and supported by the impact fee study). This is in addition to the regular development impact fees, including fees anticipated to be in place to construct the ultimate interchange solution. Once a total of 570 dwelling units or equivalent dwelling units have been built (or combination of dwelling units and commercial space), and the sum of \$1,024,745.40 (present value) has been collected, regardless of which developments in fact contribute, this "interim solution interchange fee" will no longer be collected. If the signal warrants are not met with the construction of the 570th dwelling unit or equivalent dwelling unit, then a full signal warrant analysis shall be conducted at annual intervals as determined by the City of Lemoore (after consulting with Caltrans) until the signal warrants are met. Once met, the City will cause (directly or indirectly) the improvements to be installed.

The "interim solution interchange fee" includes those highlighted costs outlined in the attached "Preliminary Order of Magnitude Opinion of Probable Construction Costs: Victory Village, Lemoore, CA: 2010 Project (with the Project): ***Based on Existing Conditions & Belle Haven Signalization***" spreadsheet.

11. Long-Term Traffic Mitigations with the project: Victory Village shall pay their proportional shares of the various improvements outlined in the attached Table 5 from the TIS listed below, or as delineated in the Impact Fee Study based on a per unit charge.

In addition to the above listed items list in condition 9

- Bush Street at 19 ½ Avenue
 - Signalize the intersection
- Bush Street between SR 41 NB and SB Ramps
 - Restripe/widen the five (5) lane roadway to six (6) lanes, providing two (2) through lanes and a separate left-turn lane in each direction.

- Bush Street at Semas Street
 - Signalize the intersection
 - Restripe/widen the NB approach, south leg to a separate left-turn lane, one (1) through lane, and a separate right-turn lane
 - Restripe/widen the SB approach, north leg to dual left-turn lanes, one (1) through lane, and a separate right-turn
 - Restripe/widen the WB approach, east leg to a separate left-turn lane, one (1) through lane, and a separate right-turn lane
 - Restripe/widen the EB approach, west leg to a separate left-turn lane, one (1) through lane, and a separate right-turn lane
- Bush Street at Belle Haven Drive
 - Restripe/widen the NB approach, south leg to a separate left-turn lane, one (1) through lane, and dual (2) right-turn lanes
 - Restripe/widen the WB approach, east leg to dual (2) left-turn lanes, two (2) through lanes, and a shared through right-turn lane (the shared through right-turn lanes is a result of the zoning change on the 4 acres of CC and 13.6 acres of RM-3)
 - Restripe/widen the EB approach, west leg to dual (2) left-turn lanes, two (2) through lanes, and a separate right-turn lane
 - Restripe/lengthen the WB left-turn lanes from 200 feet to 300 feet
- Bush Street at College Avenue
 - Signalize the intersection
 - Restripe/widen the WB approach, east leg to a separate left-turn lane and one (1) through lane
- Coordinate signals along Bush Street from College Avenue to 19½ Avenue
- 12. The 60 right-of-way (ROW) cross sections should contain 40' of curb to curb width and include an 8' median in the center and landscaping shall include trees and drip irrigated plants to reduce water flows into the streets. 60' ROW street segments shall be included on all entryways into the subdivision from arterial or collector streets.
- 13. All other street types proposed for the subdivision shall be carried out as shown on the Vesting Tentative Subdivision Map Sheet 1. The design of the Bush Street median landscape shall be determined by the Community Development Department in coordination with other City Departments.
- 14. Secondary emergency access shall be provided to the subdivision by gravel access roads from Bush Street along the proposed Brooke Street (should a street not be in place) and tie into Pedersen Street then tie back into Belle Haven Drive or by providing secondary access from Bush Street north on Production Avenue across the railroad tracks thereby tying into Industry Way or by providing secondary access to a future interchange at State Highway 198 at the abandoned north-south railroad easement bridge at the time in which a subdivision records with more than 40 homes.
- 15. Stop signs must be installed at the intersections with arterial and collector streets and shall be included in the improvement drawings.
- 16. The developer shall submit revised street names for the subdivision to comply with the City Street Naming Policy and shall be submitted to the Community Development Department for approval prior to the submittal of any improvement drawings. Street names must follow the City's Street Naming policy and follow a theme.

- 17. Five (5) sets of landscape and irrigation plans and park layout plans shall be submitted to conform to City standards for street trees along arterial streets, in buffer areas (usually located in landscape easement areas) and include proposed landscaping for the parks with the improvement plans for review and approval by the Community Development and Parks and Recreation Department prior to recordation of a Final Map. At a minimum, each park space should include a tot lot with a rubberized surface below (or acceptable falling surface), benches, picnic areas, barbecues, grass areas, sidewalks, etc. The adequacy of the parks shall be determined through separate approval by the Community Development and Parks and Recreation Departments. Due to the size of the larger park, activities for older kids should also be available.
- 18. The 40' PG&E easement shall be incorporated into the "future collector street" rather than a permanent open space area as shown in staff's Figure 1. Due to the short north-south distance of approximately 1,000' of the future street the street section shall include only a 40' wide curb-to-curb asphalt street section and then accommodate a future parkway, 8' sidewalk and landscape/buffer area with a 7' decorative block wall (to provide noise reduction of a future street). Rearyard setbacks to be 20' from Bush Street south. North of Bush Street the 40' easement shall be incorporated into the bike path with a 10' wide asphalt path that can accommodate 40-50,000 pound trucks. This will also require that a portion of lot 14 be adjusted to accomodate the transition of the bikepath to avoid the railroad right-of-way. The remaining portions of the easement shall be landscaped and maintained by the lighting landscape maintainance district. The intersection of this area with streets shall contain removable bollards to discourage vehicular traffic but allow future access as needed to PG&E and City vehicles.
- 19. 10' wide public utility and 10' landscape easements must be shown on all lots adjacent to public streets and included in the Final Map.
- 20. An noise and odor easement must be recorded on the property, in a form acceptable to the City Attorney, to acknowledge the presence of nearby industry and the right of the industry to continue to emit such noise and odors as are otherwise allowable by law and to ensure that industry in these areas is not unreasonably hindered by residential users and owners which move nearby at a later date
- 21. A Notice and Disclosure Statement must be recorded on the property, in a form acceptable to the City Attorney, which states that neighboring residents should be prepared for the inherent and potential inconveniences and discomforts often associated with normal and usual agricultural activities and operations, and the County will not take any nuisance abatement actions against any normal and usual farming operations must be recorded (as required by 1997 EIR measure regarding the Kings County Right to Farm Ordinance). This helps farmers to continue their right-to-farm.
- 22. Developer shall make installation of solar panels option available to buyers so long as they are not located on the backside of homes facing arterial or collector streets. To encourage the purchase of such an option, one model home should be installed with solar panels and appropriate materials made available to show the cost efficiencies.
- 23. Improvement drawings must show existing and proposed sanitary sewer, storm drainage, water, street lights, fire hydrants and street infrastructure (including curb, gutter, and parkway style sidewalks) associated with the development. All proposed and existing onsite utilities shall be under grounded by the developer at their expense. Any utility which may need to be relocated will also be under grounded, with the exception of the 115kv power line. The improvement plans for the subdivision must be approved by the City of Lemoore Engineer and Public Works Departments and conform to City Standards, with the exception to those street cross-section issues addressed through the Planned Unit Development. Appropriate impact fees shall be paid for all utility services.

Development impact fees designed to mitigate the impact of water service shall be charged and all homes shall have water meters. If overdraft conditions are found on City wells, conservation programs shall be implemented.

Fire hydrants spaced 300 feet apart throughout the subdivision as approved by the Lemoore Volunteer Fire Department and shall be included in the improvement drawings with hydrants on the west or north side of the roads.

- 24. No development site drainage would be allowed to discharge untreated urban runoff into the marsh.
- 25. Signs shall be posted near the wetlands that walking of dogs within the wetland area is be prohibited and can cause potentially serious impacts to wildlife and buyers of residential lots and homes adjacent to the preserved wetlands would be informed through a notice and disclosure statement of the importance of the wetlands and the potentially serious impacts to wildlife that could be caused by pets.
- 26. No fire stations exist on the westside and will be needed to provide adequate response times for all development on the westside. Therefore, impact fees will need to be paid towards the construction of such facilities. Additionally, growth on the westside will cause the need to expand the police service facilities and appropriate impact fees shall be paid for such as outlined in the impact fee study.
- 27. The location of the cluster mailbox pads and installation of boxes are the financial responsibility of the developer. The locations of boxes should try to encourage interaction among neighbors.
- 28. The construction activities shall conform to the general control measures required by the San Joaquin Valley Air Pollution Control District, dust control measures provided in the EPA's Reasonably Available Control Measures and Best Avaliable Control Measures and the PM10 ADP, and the City of Lemoore's air quality measures.
- 29. Project-related grading and construction activities shall be subject to the recommendations outlined in the Preliminary Geotechnical Investigation Report prepared by Kleinfelder (which is located in Appendix K of the 1997 EIR) and a seperate design level geotechnical investigation that is specific to the proposed project to better evaluate areas of varying soil types which require different earthwork and foundation recommendations as well as the Technicon soils report.
 - As a mitigation to the 1997 EIR, in the event presently unknown archaeological or historical resources are discovered during development of specific projects, work shall be terminated until such time that a certified archaeological/historical consultant can investigate the findings.
- 30. The developer shall meet all provision of the "License Agreement" date May 1, 2006 between West Hills Community College District, Lemoore Union Elementary School District and Victory Lemoore as may be amended between the parties.
- 31. A digital copy of the final map and improvement plans shall be provided to the City upon prior to Final Map recordation.
- 32. The developer shall at all times comply with the Fugutive Dust Control Standards of the San Joaquin Valley Unified Air Pollution Control District and shall take complete control of dust during the preparation of the subdivision site and during construction by taking the following measures:

- a. Submit for approval of the Public Works Director a program for the control of dust, which shall include, but not limited to, a watering schedule (frequency and time of day), use of dust control emulsions, and/or other measures necessary for control of dust.
- b. Provide equipment and labor for watering of all exposed or disturbed soil surfaces, including weekends and holidays.
- c. Sweep construction area and adjacent streets of all mud and dust at the end of the workday.
- d. In addition, the developer shall deposit with the City an amount of five thousand dollars (\$5,000) which may be used by the City for dust control measures on this development, should the developer fail to adequately control dust. In case the City incurs costs for dust control in excess of the above amount, the developer shall reimburse the City for this additional amount. Upon acceptance by the City of the subdivision improvements, the deposit sum less any amount expended by the City will be refunded to the subdivider.
- 33. That the developer and contractor shall comply with all applicable provisions of the NPDES regulations, and are responsible for all General Permit applications to the Regional Water Quality Control Board. Proof of application shall be provided to the City prior to commencement of construction.
- 34. The developer shall submit to the Community Development Department the following documents for processing after approval of the Tentative Subdivision Map in addition to the above mentioned items.
 - a) Five (5) copies of the final subdivision map along with closure calculations, preliminary title report dated within 90 days of submittal, and the final map application fee. The final map shall bear the signature and seal of the licensed land surveyor or registered civil engineer licensed to practice land surveying, preparing the map. If any of these required elements are missing, no attempt will be made to review the submittal.
 - b) Five (5) copies of subdivision improvement plans shall be submitted the Community Development Department including water, sewer, storm drainage street lighting, sidewalks, curb, gutter and fire hydrants drawn at a scale of not less than 1"-40' horizontal and 1"-4' vertical to be distributed to all commenting parties. General layout and grading may be shown on 24"x36" plan sheets. Plan and profile sheets shall be provided for curb grades and all proposed underground construction, and shall identify points of crossing. Calculations for pavement sections and any nonstandard facilities shall be provided. Details for all construction not covered by City Standard Specifications and/or Detail Drawings shall be provided. Plans shall be signed and sealed by the civil engineer in responsible charge. No review will be attempted on an incomplete submittal.
 - c) Two (2) copies of the engineer's cost estimates for the proposed improvements showing quantities and unit prices. Unit prices shall conform to the City standard prices, which are available at the Public Works Department.
 - d) After review of the final map is complete, the applicant's engineer shall submit a letter certifying that monumentation is in place and ready for field inspection. Upon receipt of that letter, staff will inspect the monumentation of the tract, prior to final approval of the map. If monumentation of individual lots is to be delayed until construction, the owner shall post with the City a bond-guaranteeing placement of all required monumentation.

- e) Prior to submittal of the original tracings of the final map to the City for signature and recordation, the original tracing shall include notarized signatures (in black indelible ink) of all persons having record interest in the area within the boundary of the map, the seal and signature (in indelible ink) of the Registered Civil Engineer/Licensed Land Surveyor preparing the map, and the signature (in black indelible ink) of the Kings County Tax Collector. Use of improper ink for these signatures may cause rejection of the map by the City or the County Recorder's office.
- f) After approval of the improvement drawings, the City will maintain possession of the original drawings for the duration of the project. The developer or his representative may obtain copies through a certified blueprinting service (Western Blueprinting or Airport Blueprint) that will retrieve the originals, make the needed copies and return the originals to the City.
- g) That upon completion of the subdivision improvements and their acceptance by the City, the developer's engineer shall prepare drawings of as built improvements and shall submit to the City one reproducible and three blue line copies for the City's records.
- 35. All subdivision improvements including water, sewer, storm drainage, curbs, gutters, sidewalks, street lights, fire hydrants and street improvements shall comply with the Public Works Standards of the City unless provided differently in this report as an allowed Planned Unit Development standard and meet the approval of the City Engineer.
- 36. The subdivider shall defend, indemnify and hold harmless the City of Lemoore and all of its departments, officers, agents and employees of and from all claims, actions and proceedings of any kind or nature to attack, set aside, void or annual the actions of the Planning Commission and/or City Council in reviewing and approving the map. This condition is imposed pursuant to Government Code Section 66474.9. The City will promptly notify the subdivider of any such claim or action and will fully cooperate with the subdivider in the defense thereof.
- 37. In accordance with City Council Resolution 2005-24 adopted on June 7, 2005, the Council has authorized staff to prepare an updated impact fee study which is anticipated to be completed within one year. The project will have an impact on the need for new public facilities and improvements in the City. The costs associated with that impact and the connection between the need for new public facilities and the proposed project will be set forth in a new Resolution to be adopted by the City Council. As a result, the applicant shall be subject to and shall pay these impact fees when they are set forth; should construction commence prior to the completion of such fee schedule an interium assessment will have to be made. Development Impact Fees shall be collected upon issuance of building permits for the development at the rate in effect on the date of the building permit.
- 38. The 13.6 acre portion of Assessor's Parcel Number 023-510-011 as shown on the general plan and zoning amendment exhibits, shall be zoned RM-3 within the scope of the Traffic Impact Report and will be required to include at least 85 units due to density shortfalls and up to 180 units of multi-family housing when submitting for a Site Plan Review or construction.

Passed and adopted at a Regular Meeting of the City Council of the City of Lemoore held on the 6th day of June, 2006, by the following vote:

AYES: PLOURDE, MARTIN, GREGO, MURRAY, BUFORD NOES: NONE ABSTAINING: NONE

ABSENT: NONE

APPROVED:

PDANDAPHONATE CHARE DEDORMENCES					
ACTURACION DE COMPAÑO A DESCRIPAÇÃO					
india/dw/time		2010	2010 (Near Term) Improvements	rements	
чиневыма				City of Lemoore	Caltrans
California				Proportionate	Proportionate
-7ACOULLE A	Project Trips		2010 + Project	Share %	Share %
Intersections	AM/PM	Existing	AM/PM	ANI/PM	AMIPM
Bush Street at 19 1/2 Avenue	43/49	n/a	1,779/1,782	2,42%/2,75%	11/a
Bush Street at SR 41 NB Ramps	68/97	806/776	2,330/2,328	2.92% / 4.17%	4.46% / 6.25%
Bush Street at SR 41 SB Ramps	94/130	766/601	2,553/2,424	3.68% / 5.36%	5.26% / 7.13%
Bush Street at Belle Haven Drive	97/134	n/a	2,311/2,271	4.20% / 5.90%	n/a
Bush Street at Semas Street	146/200	n/a	1,285/1,278	11.36% / 15.65%	11/2
Bush Street at College Avenue	158/215	n/a	937/937	16.86% / 22.95%	n/a
		2030	(Long Term) Improvements	ements	
new and the second				City of Lemoore	Caltrans
accurate posts				Proportionate	Proportionate
PERSONAL	Project Trips	Existing	2030 + Project	Share %	Share %
	AMPM	AM/PM	AMPM	AMIPM	AMPM
Bush Street at 19 1/2 Avenue	14/20	11/a	2,557/2,560	0.55%/0.78%	12/2
Bush Street at SR 41 NB Ramps	27/43	9///908	3,431/3,415	0.79%/1.28%	1.03%/1.63%
Bush Street at SR 41 SB Ramps	42/58	766/601	4,141/3,991	1.01%/1.45%	1.24%/1.71%
Bush Street at Belle Haven Drive	20/69	n/a	5,095/5,031	0.98%/1.37%	10/2
Bush Street at Semas Street	123/168	n/a	3,102/3,084	3.97% / 5.45%	n/a
Bush Street at College Avenue	133/181	n/a	1,867/1,852	7.12%/9.77%	n/a
Production Avenue Railroad Crossing	9/12	n/a	254/251	3.54% / 4.78%	11/a
Brooke/SR 198 Interchange	14/21	n/a	694/694	2.02%/3.03%	2.02%/3.03%
			THE RESIDENCE AND ADDRESS OF THE PERSON OF T		Control of the Contro

n/a = not applicable

PRELIMINARY ORDER OF MAGNITUDE OPINION OF PROBABLE CONSTRUCTION COSTS VICTORY VILLAGE, LEMOORE, CA

2010 Project (with the Project)
Based on Existing Conditions & Belle Haven Signalization

PREPARED BY: T. CARPENTER

DATE:

5/30/2006

PROJECT NO. 05-950								
ITEM NAME (DESCRIPTION)	UNIT	QTY	UNIT COST	TOTAL COST	City Proportionate Share	PROJECT COST	Caltrans Proportionate Share	PROJECT COST
Bush Street at 19 1/2 Avonue Signatize the intersection Restrice/widen the NB approach, south leg, from a shared left-through-right lane, to a separate left-turn lange and a chared through-right lane						······································	Propositionale Strate	
Signalization of Intersection Signing, Striping and Pavement Markings (Removal and Installation) Apphalt (4") Aggregate Base (8") Earthwork (Grading/Compacting/Import) Utility Relocation	LS LS Ton Ton LS LS	1 62 149 1	\$220,000.00 \$20,000.00 \$150.00 \$50.00 \$3,500.00 \$10,000.00	\$220,000,00 \$20,000,00 \$12,300,00 \$7,450,00 \$3,500,00 \$10,000,00				
	COST			\$273,250.00	2.75%	\$7,514.00		
Bush St. of SR 41 NB Ramps - from halfway between the SB ramps and 19 1/2 Avenue Signalize the intersection Restripedviden the NB approach, south leg, from a shared left-through lane and a separate right-turn lane, to a separate left-turn lane, a stured left-through lane, and a separate right-turn lane RestripeAviden the SB approach, was leg, from a separate left-turn lane and one (1) through lane, to a separate left-turn lane and two (2) through lanes								
Restripe/lengthen the EB left from 125 feet to 155 feet to accommodate the projected queue								
Signalization of Intersection Right-ed-way Acquisition Signing, Steiping and Pavement Markings (Romoval and Installation) Demolition of Asphalt and Concrete Install Sidewalk Install Handicap Ramp Asphalt (6") Aggregate Base (12") Earthwork (Grading/Compacting/Import) Utility Refocation Utility Refocation	LS SF LS LF SF EA Ton Ton LS	1 7260 1 1 100 450 1 272 490	\$200,000.00 \$3.00 \$25,000.00 \$35,000.00 \$7.00 \$150.00 \$150.00 \$55,000 \$55,000.00	\$200,000.00 \$21,780.00 \$25,000.00 \$3,500,00 \$3,500,00 \$3,150.00 \$40,800.00 \$24,500,00 \$55,000.00				
Buch St. at SR 41 SB Ramps - from halfway between Belle Haven and the NB ramps	COST)	\$389,730.00	4,17%	\$16,252.00	6.25%	\$24,358.0
owen of, it is at all 150 temps - from heaven) between belief teven and the NB temps Signatize the intersection Restrips/widen the EB approach, west leg, from a shared through-right isne, to two (2) through tenes and a separate right-turn lane								
Signalization of Intersection Right-of-vay Acquisition Signing, Striping and Pavament Markings (Removal and Installation) Demotition of Alaphait and Concrete Install ScattleCutter Install Sidowalk Install Handicap Ramp Aspholt (4*) Aggregate Base (6*) Earthwest (Grading/Compacting/Import) Utility Relocation	LS SF LS LS LF SF EA Ton LS LS	1 1929 1 1 150 720 1 48 87 1	\$200,000,00 \$3,00 \$16,000,00 \$2,500,00 \$35,00 \$7,00 \$3,000,00 \$160,00 \$5,000.00 \$1,000,00	\$200,000.00 \$5,760.00 \$15,000,00 \$2,500,00 \$5,640,00 \$3,000.00 \$7,200.00 \$4,350,00 \$5,000.00 \$1,000.00				
	OST			\$263,450,00	6.36%	\$14,121.00	7.13%	****
Bush St. at Bello Haven Dr from extents of wost side widening to halfway between SB ramps Modification of Signal		***************************************			5.04 N	\$1.345,145.1-000	7.13%	\$18,784.0
Restripe/widen the NB approach, south leg, from a separate left-turn lane and a shared through right lane, to a separate left-turn lane, one (1) through lane, and a separate right-turn lane Restripe/viden the WB approach, cast leg, from a separate left-turn lane, one (1) through lane, and a separate right-turn lane, to a separate left-turn lane, two (2) through lanes, and a separate right-turn lane. Restripe/viden the EB approach, west leg, from a shared left-turn lane, two (2) through lanes, and a separate left-turn lane, two (2) through lanes, and a separate left-turn lane.								
Restripe/lengthen the WB left from 200 leet to 275 feet to accommodate the projected quoue								
Modification of Signal Installed in 2008 Right-of-way Acquisition Signitus, Striping and Pavement Markings (Removal and Installation) Demolition of Apphalt and Concrete Install Softwalk Install Handings Ramp Asphalt (4*) Aggregate Base (6*) Earthwesk (Grading/Compacting/Import) Utility Relocation Utility Relocation	LS SF LS LF SF EA Ton Ton LS LS	1 25100 1 1 500 2250 4 628 1135 1	\$150,000,00 \$3.00 \$25,000.00 \$5.500.00 \$35.00 \$3.000.00 \$150.00 \$50.00 \$40,300.00	\$150,000,00 \$75,300,00 \$75,000,00 \$5,500,00 \$17,500,00 \$15,750,00 \$12,000,00 \$94,200,00 \$94,200,00 \$40,300,00				
	OST			\$534,300.00	5.90%	\$31,524.00		
Bush St. at Semas St from extents of worst side videning to extents of east side videning Signature the interacction Restriperviden the NB approach, south leg, from a shared left-through-right lane, to a separate left-turn lane and a shared through-right lane Restriperviden the SB approach, north leg, from a shared left-through-right lane, to a separate left-turn lane and a shared through-right lane Restriperviden the VPB approach, east leg, from a shared left-through-right lane, to a separate left-turn lane and a shared through-right lane Restriperviden the SB approach, wast leg, from a shared left-through-right lane, to a separate left-turn lane and a shared through-right lane.								
 Signalization of Intersection Right-of-way Acquisition Signing, Stipling and Pavement Markings (Removal and Installation) Demoition of Asphat and Concrete Removal (Curtu/gutter and sidewalk) Install Curtu/Gutter 	LS SF LS LS CY LF	1 31500 1 1 36 160	\$200,000.00 \$3,00 \$25,000.00 \$5,500.00 \$35.00	\$200,000.00 \$94,500.00 \$25,000.00 \$5,500.00 \$1,260.00 \$5,600.00				

Install Sidewalk Install Handicap Ramp Asphall (4") Aggregate Base (6") Earthwork (Grading/Compacting/Import) Utility Refocation	SF EA Ton Fon LS LS	720 4 780 1425 1	\$7.00 \$3,000,00 \$160.00 \$50.00 \$62,000,00 \$40,300.00	\$5,040,00 \$12,000,00 \$117,000,00 \$71,250,00 \$52,000,00 \$40,300,00				
	COST			\$629,450.00	15.66%	\$98,509.00		
Coordination of Signals from Somas St. to 19 1/2 Ave.								
Synchronize and Program Signal Controllers along Bush St. Confider Conduit and Conductors Equipment Upgrade (Modern Gards, Network Communication Devices,)	LS LF EA	1 3500 5	\$35,000.00 \\ \$45,00 \$1,650.00	\$35,000,00 \$157,500,00 \$7,750,00		' needed by 2010 :	\$22,500.00 \$4,650,00	
	COST			\$200,250.00	6.77%	\$13,549.00		
	SUBTOTAL	CONSTRUCT	ON COST	\$2,290,430,00		\$181,469.00		\$43,142.00
MISC. (3%) - Includes Mobilization/Clearing and grubbing site, etc. CONTINGENCIES (20%)				\$68,712.90	7.92%	\$5,444.00		
CONSTRUCTION ENGINEERING (5%)				\$458,086.00 \$114,521,50	7.92% 7.92%	\$38,294.00 \$9,073.00		
PLANS, SPECIFICATIONS, & ENGINEERING (10%)				\$229,043.00	7.92%	\$18,147.00		
	TOTAL CO	NSTRUCTION (COST	\$3,160,793.40		\$250,427.00		
PSR EIR	EA EA	1	\$200,000.00 \$200,000.00	\$200,000.00 \$200,000.00	4.65% 4.66%	\$9,300.00 \$9,300.00	6.50% 6.50%	\$13,210.00 \$13,210.00
	GRAND TO)TAL		\$3,560,793.40		\$269,027.00	***************************************	\$69,562.00

PLEASE NOTE: ***NO STRUCTURE WIDENING INCLUDED AT THE INTERCHANGE OF HWY 41 AND BUSH ST.***

ALL COSTS REFLECT CURRENT RATES/ESTIMATES, FUTURE COSTS MAY DIFFER

SUBTOTAL OF SPECIAL INTERIN SOLUTION INTERCHANGE FEE \$735,250.00 MISC - Indicates Mobilization/Closting and grubbing sho, etc. 3% \$22,009.00 CONTINGENCIES 20% \$347.065.00 CONSTRUCTION ENGINEERING 5% \$365,766.50 PLANS, SPECIFICATIONS, & ENGINEERING 10% \$735,533.00 Up to 4 variant studies needed prior to installing signals at SB 5, NB SR 41 ranges 4 \$3,000.00 \$12,000.00 TOTAL OF INTERIM SOLUTION INTERCHANGE FEE \$1,005,765.40
Cost par unit for 670 housing units or thair equivalent

Highlighted items are those improvements that were included in the interim solution interchange toe

^{**}These Proportionate Share %'s based on weighted averages of above applicable %'s**

PRELIMINARY ORDER OF MAGNITUDE OPINION OF PROBABLE CONSTRUCTION COSTS VICTORY VILLAGE, LEMOORE, CA

2030 Project (with the Project)
Estimates Assume Recommended Improvements Have Been Made in 2010

PREPARED BY: T. CARPENTER

DATE:

5/30/2006

PROJECT NO. 05-950

ITEM NAME (DESCRIPTION)	UNIT	QTY	UNIT COST	TOTAL COST	City Proportionate Share	PROJECT COST	Caltrans Proportionate Share	PROJECT COST
Bush Stroot at 19 1/2 Avenue								
Restriping/lengthening of NS left-turn lane to accommodate increased traffic								
 Signing, Striping and Pavement Markings (Removal and Installation) Asphalt (4") 	LS Ton	1 18	\$10,000.00 \$150.00	\$10,000.00 \$2,700.00				*
 Aggregate Base (8*) Earthwork (Grading/Compacting/Import) 	Ton LS	31 1	\$50.00 \$1,500.00	\$1,550.00 \$1,500.00				
Utility Relocation	LS	1	\$5,000.00	\$5,000.00				
Bush St. at SR 41 NB Ramps - from halfway between the SB ramps to the NB ramps	COST			\$20,750,00	0.78%	\$162.00		
Restriptovides Bush Street from five (5) lanes to cit (6) Isnes between the NB and SB Ramps								
 Modification of Signal Signing, Striping and Pavement Markings (Removal and Installation) 	LS LS	1	\$160,000.00 \$10,000.00	\$150,000.00 \$10,000.00				
Demolition of Asphalt and Concrete Install Curb/Gutter	LS LF	1 450	\$3,000.00	\$3,000.00				
Install Sidowalk	SF	2250	\$35.00 \$7.00	\$15,750.00 \$15,750.00				
Install Handicap Ramp Asphalt (6")	EA Ton	2 135	\$3,000.00 \$150.00	\$6,000.00 \$20,250.00				
 Aggregate Base (12") Earthwork (Grading/Compacting/Import) 	Ton LS	243	\$50.00 \$10,000.00	\$12,150.00 \$10,000.00				
Utility Relocation The Back Walt (6')	1,8	1	\$10,000,00	\$10,000.00				
	COST	1200	\$70.00	\$84,000.00				
Bush St. at SR 41 SB Ramps - from heliway between the NB ramps to the SB ramps	0001			\$336,900.00	1.28%	\$4,312.00	1.63%	\$5,491.00
Restripe/widen Bush Street from five (5) lanes to six (6) lanes between the NB and SB Ramps								
 Modification of Signal Signing, Striping and Pavement Markings (Romoval and Installation) 	LS	1	\$150,000.00	\$150,000.00				
 Demolition of Asphalt and Concrete 	LS LS	i	\$10,000.00 \$3,000.00	\$10,000.00 \$3,000.00				
Install Curb/Gutter Install Sidowalk	LF SF	450 2250	\$35,00 \$7,00	\$15,750.00 \$15,750.00				
Install Handicap Ramp Asphall (6")	EA Ton	135	\$3,000,00	\$6,000.00				
Aggregate Base (12")	Ton	243	\$150.00 \$50,00	\$20,250.00 \$12,150.00				
Earthwork (Grading/Compacting/Import) Utility Relocation	ls Ls	1	\$10,000,00 \$10,000,00	\$10,000.00 \$10,000.00				
Tie Back Well (8')	SF	1200	\$70.00	\$84,000.00				
Inno, and a separate right-turn lane, to a separate left-turn lane, one (1) through lane, and dual (2) right-turn lanes. Restripe/widen the WB approach, east leg, from a separate left-turn lane, two (2) through lanes, and a separate right-turn lane, to dual (2) left-turn lane, two (2) through lanes, and a separate right-turn lane. Restripe/widen the EB approach, west leg, from a separate left-turn lane, two (2) through lanes, and a separate right-turn lane. Restripe/left lanes, and a separate right-turn lane. Restripe/left lanes, lanes		1 19000 1	\$150,000.00 \$3.00 \$25,000.00 \$5,500.00	\$150,000.00 \$57,000.00 \$25,000.00 \$5,500.00				
Instafi Curt/Gutter Instafi Sidewalk	LF	500	\$35.00	\$17,500,00				
Install Handicep Ramp	SF EA	2250 4	\$7.00 \$3,000.00	\$15,750.00 \$12,000.00				
Asphalt (4") Aggregate Base (8")	Yon Yon	463 860	\$150.00 \$50.00	\$69,450,00 \$43,000,00				
Earthwork (Grading/Compacting/Import) Utility Relocation	LS LS	1 1	\$31,500.00 \$40,300.00	\$31,500.00				
	COST		***************************************	\$40,300.00	1.37%	\$6,398.00		
Bush St. at Sames St from extents of wast side widening to extents of east side widening Restripstviden the NB approach, south log, from a separate left-turn lane and a shared through-right lane, to a separate left-turn lane, one (1) through lane, and a separate right-turn					1.07 18	30,500,00		
lane Restripe/wholen the SB approach, north leg, from a separate left-turn iane and a shared through-right lane, to dual (2) left-turn lanes, one (1) through lane, and a separate right-turn								
ione Rostripe/miden the WB approach, east leg, from a separate left-turn lane and a shared through-right lane, to a separate left-turn lane, one (1) through lane, and a separate right-turn								
lane Restripolyiden the EB approach, west leg, from a separate left-turn lane and a shared through-right lane, to a separate left-turn lane, one (1) through lane, and a separate right-turn lane								
Modification of Signal	LS	1	\$150,000.00	\$150,000.00				
Right-of-way Acquisition	SF	31600	\$3.00	\$94,800.00				
 Signing, Striping and Pavement Markings (Removal and Installation) Demoition of Asphalt and Concrete 	LS LS	1	\$25,000.00 \$5,500.00	\$25,000.00 \$5,500,00				
 Romoval (Curb/gutter and sidowalk) Install Curb/Gutter 	CY LF	36 160	\$35.00 \$35.00	\$1,260.00 \$5,600.00				
Install Sidovyalk Install Handicap Ramp	SF	240	\$7.00	\$1,680.00				
Asphalt (4")	EA Ton	4 782	\$3,000.00 \$150,00	\$12,000.00 \$117,300.00				
Aggregate Sase (8") Earthwork (Grading/Compscting/Import)	Ton LS	1429	\$50.00 \$53,000.00	\$71,450.00 \$53,000.00				
Utility Relocation	LS	ŧ	\$40,300.00	\$40,300.00				
·								

Restripe/viden the WB approach, east leg, from a shared left-through lane, to a separate left-turn lane and one (1) through lane

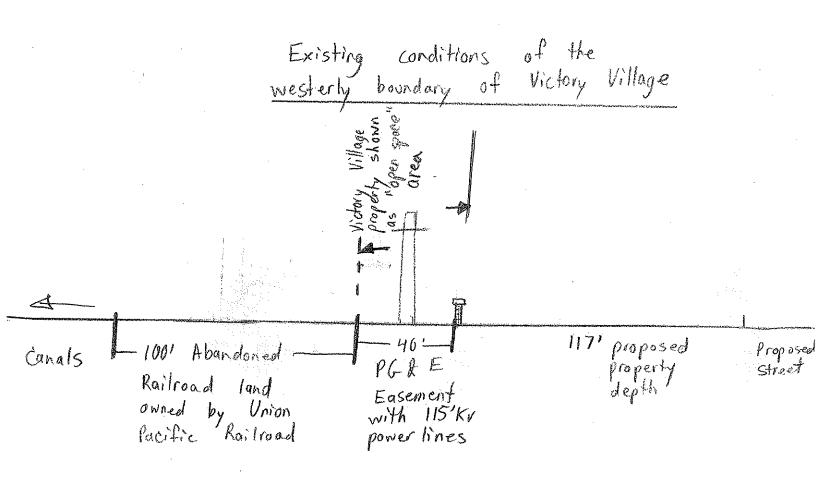
	restriction and dust (1) and after the la								
	Signalization of Intersection	LS	1	\$170,000.00	\$170,000.00				
	* Right-of-way Acquisition	SF	12500	\$3.00	\$37,500.00				
	 Signing, Stripling and Pavement Markings (Remova) and Installation) 	LS	12000	\$10,000.00	\$10,000.00				
	Demolition of Asphalt and Concrete	LS	,	\$2,000.00	\$2,000.00				
	Install Curb/Gutter	LF	100	\$35.00	\$3,500.00				
	Install Sidowalk	SF	600	\$7.00	\$4,200.00				
	Install Handicap Ramp	EA	2	\$3,000.00	\$6,000.00				
	· Asphalt (4")	Ton	309	\$150.00	\$46,350.00				
	Aggropate Base (8")	Ton	565	\$50.00	\$28,250.00				
	Earthwork (Grading/Compecting/Import)	LS	1	\$21,000.00	\$21,000.00				
	Utility Relocation	LS	,	\$40,300.00	\$40,300.00				
	dony responsi	Lo	,	\$40,300,00	\$40,300.00				
		COST			\$369,100.00	9.77%	\$38,061.00		
Coordination of S	Signals from College Ave, to 19 1/2 Ave.								
	 Synchronize and Program Signal Controllers along Bush St. Corridor 	LS	1	\$10,000.00	\$10,000.00				
	Conduit and Conductors	ĻF	1600	\$45.00	\$72,000.00				
	 Equipment Upgrade (Modern Cards, Network Communication Devices,) 	ĒA	6	\$1,550.00	\$9,300,00				
	,		*	47,000.00	45,545,00				
		cost			\$91,300,00	3.35%	\$3,059.00		
Interchange at H	Sighway 198 and Brook St.								
	Right-of-way Acquisition	ts	,	\$1,200,000.00	\$1,200,000.00				
	Signala	LF	i	\$400,000.00	\$400,000,00				
	Construction Costs	EA.	i	\$19,700,000,00	\$19,700,000.00				
			•	41037 40.000.00	410,1100,000.00				
		COST	~~~~		\$21,300,000.00	3.03%	\$645,390.00	3.03%	\$645,390.00
		SUBTOTA	L CONSTRUC	CTION COST	\$23,162,940.00		\$731,762.00		\$656,642.00
	MISC, (3%) - Includes Mobilization/Clearing and grubbing site, etc.				\$694,888.20	3.14%	\$21,806.00		
	CONTINGENCIES (20%)				\$4,632,588.00	3.14%	\$145,375,00		
	CONSTRUCTION ENGINEERING (5%)				\$1,158,147.00	3.14%	\$36,344,00		
	PLANS, SPECIFICATIONS, & ENGINEERING (10%)				\$2,316,294.00	3,14%	\$72,688,00		
					V4,010,000	******	\$1 E,000,00		
		TOTAL CO	NSTRUCTIO	N COST	\$31,964,857.20		\$1,007,975.00		
			~~~~~						
	PSR	£A.	2	\$200,000,00	\$400,000.03	1,28%	\$5,120,00	1.63%	\$6,520,00
	EIR	EA	2	\$200,000,00	\$400,000.00	1.28%	\$5,120.00	1.63%	\$6,520.00
		GRAND T	OTAL		\$32,764,857.20		\$1,018,215.00		\$669,682,00
					,,,,,		A - 1- 1- 1- 1- 1- 1- 1- 1- 1- 1- 1- 1- 1		

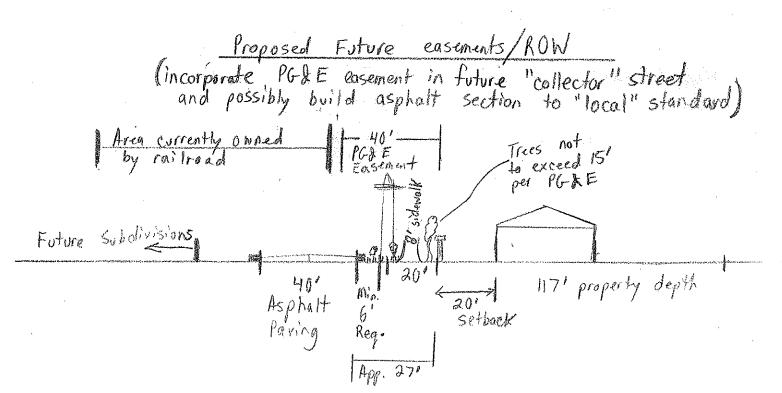
PLEASE NOTE: ***NO STRUCTURE WIDENING INCLUDED AT THE INTERCHANGE OF HWY 41 AND BUSH ST.***

[&]quot;These Proportionate Share %'s based on weighted averages of above applicable %'s"

^{***}ALL COSTS REFLECT CURRENT RATES/ESTIMATES, FUTURE COSTS MAY DIFFER***

# FIGURE 1





# **CERTIFICATE**

STATE OF CALIFORNIA	)	
COUNTY OF KINGS	)	SS
CITY OF LEMOORE	Í	

I, NANCI C.O. LIMA, City Clerk of the City of Lemoore, do hereby certify the foregoing Resolution of the City Council of the City of Lemoore was duly passed and adopted at a Regular Meeting of the City Council held on June 6, 2006.

DATED: JUNE 7 , 2006

Nanci C.O. Lima, City Clerk

# **RESOLUTION NO. 2006-23**

# A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF LEMOORE ADOPTING A MITIGATED NEGATIVE DECLARATION AND AMENDING THE GENERAL PLAN DIAGRAM IN DESIGNATED AREAS FOR THE VICTORY VILLAGE PROJECT BY ASLAN DEVELOPMENT AND CENTEX HOMES

At a Regular Meeting of the City Council of the City of Lemoore duly called and held on June 6, 2006, at 7:30 p.m. on said day, it was moved by Councilmember ________, seconded by Councilmember _______ GREGO ______ and carried that the following Resolution be adopted:

WHEREAS, the Planning Commission reviewed the Environmental Impact Assessment and the Victory Village Lemoore, ČA Traffic Impact Study (TIS) of May 2006 conducted by TPG Consulting under the direct supervision of City staff, with proposed applications to amend portions of the General Plan Diagram and Zoning Map and subdividing approximately 71.3 acres of land into 279 lots. Commission recommended that the City Council adopt a Mitigated Negative Declaration for the proposed project. However, Commission also added a condition requiring the upzoning of approximately 13.6 acres of medium density land on a portion of APN # 023-510-011 from medium density to high density in conformance with the Traffic Study. Staff therefore evaluated the impacts of the requested change and provided an addendum to the environmental analysis and determined that this additional modification would negatively affect the project nor the traffic mitigation measures originally outlined in the original environmental assessment if zoned high density. The traffic analysis was run to evaluated 4 acres of commercial area and the remaining 13.6 acres to accommodate between 85-234 housing units. The only change to the mitigation measures affected the queing at Belle Haven and Bush Drive required an additional west bound through lanes which can be shared with the previously proposed separate right turn lane and has been incorporated into the mitigation measures/conditions outlined in Resolution 2006-28. Based on this change being incorporated there will not be a negative affect to traffic and staff recommends that the Council still adopt a Mitigated Negative Declaration pursuant to California Environmental Quality Act for the project, incorporating the TIS with the addendum information; and

WHEREAS, the Lemoore Planning Commission held a duly noticed public hearing on the Victory Village project discussed above in its meeting on May 22, 2006, as required by Government Code Section 65353, it being established that all notice requirements as set forth in Government Code Section 65090 of the said code have been complied with; and

WHEREAS, the Planning Commission, as a result of evidence and written and oral testimony offered at said hearing, has established that the best interests of the City of Lemoore would be served by amending the General Plan Diagram as requested (affecting APN's # 023-510-031, 023-510-026, 029, and 030 and 023-510-011 and shown on attached Exhibits A1 and B1a; and

WHEREAS, The City Council held a duly noticed public hearing on the Victory Village project discussed above in its meeting on June 6, 2006, as required by Government Code Section 65353, it being established that all notice requirements as set forth in Government Code Section 65090 of the said code have been complied with and considered all comments made and written and oral evidence presented at their public hearing; and

**NOW, THEREFORE, BE IT RESOLVED** that the City Council of the City of Lemoore hereby determines that a Mitigated Negative Declaration be adopted based on the Environmental Assessment and TIS with the addendum in accordance with the California Environmental Quality Act and that General Plan Amendment No.2005-01 as requested by Aslan Development and Centex Homes be approved as per attached Exhibits A1 and B1 (so long as Ordinance 2006-04, regarding a Zone Change for the property and Resolution 2006-28 with its 38 conditions are also approved).

Passed and adopted at a Regular Meeting of the City Council of the City of Lemoore held on the <a href="https://example.com/en/state-12/4/2">6th day of June, 2006</a>, by the following vote:

AYES: PLOURDE, GREGO, MARTIN, MURRAY, BUFORD

NOES: NONE

ABSTAINING: NONE ABSENT: NONE

**APPROVED:** 

₮ћomas E. Buford, Mayor

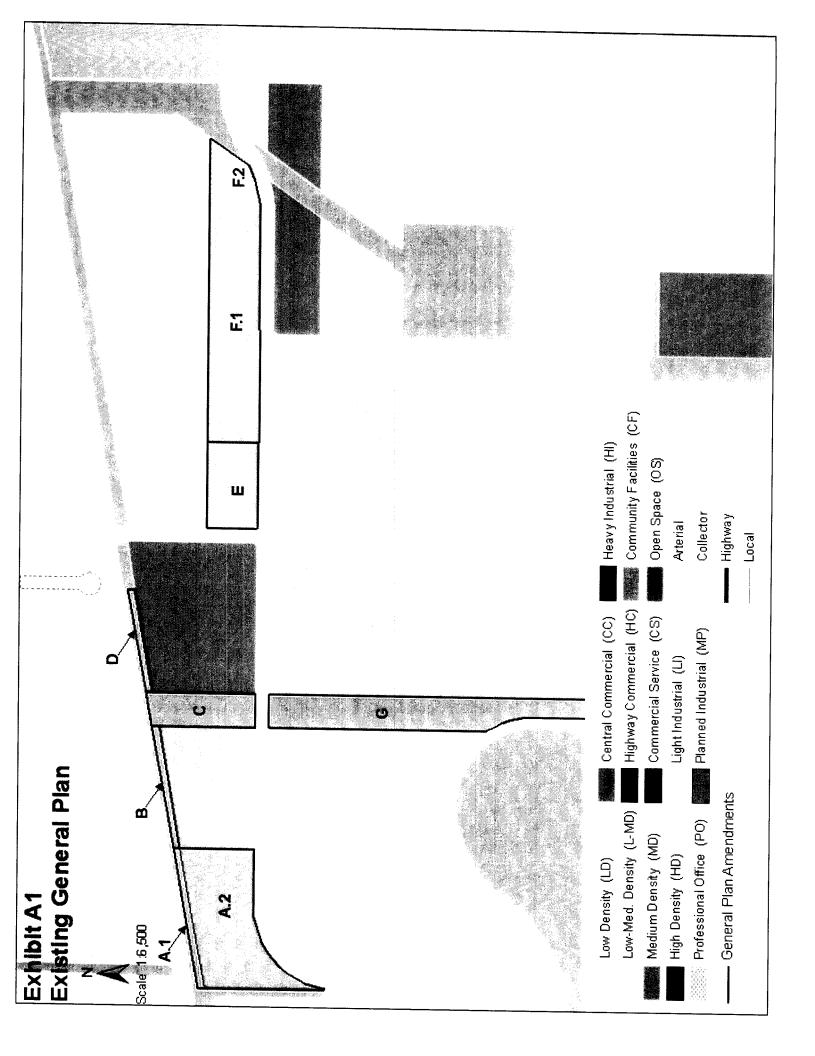
# **CERTIFICATE**

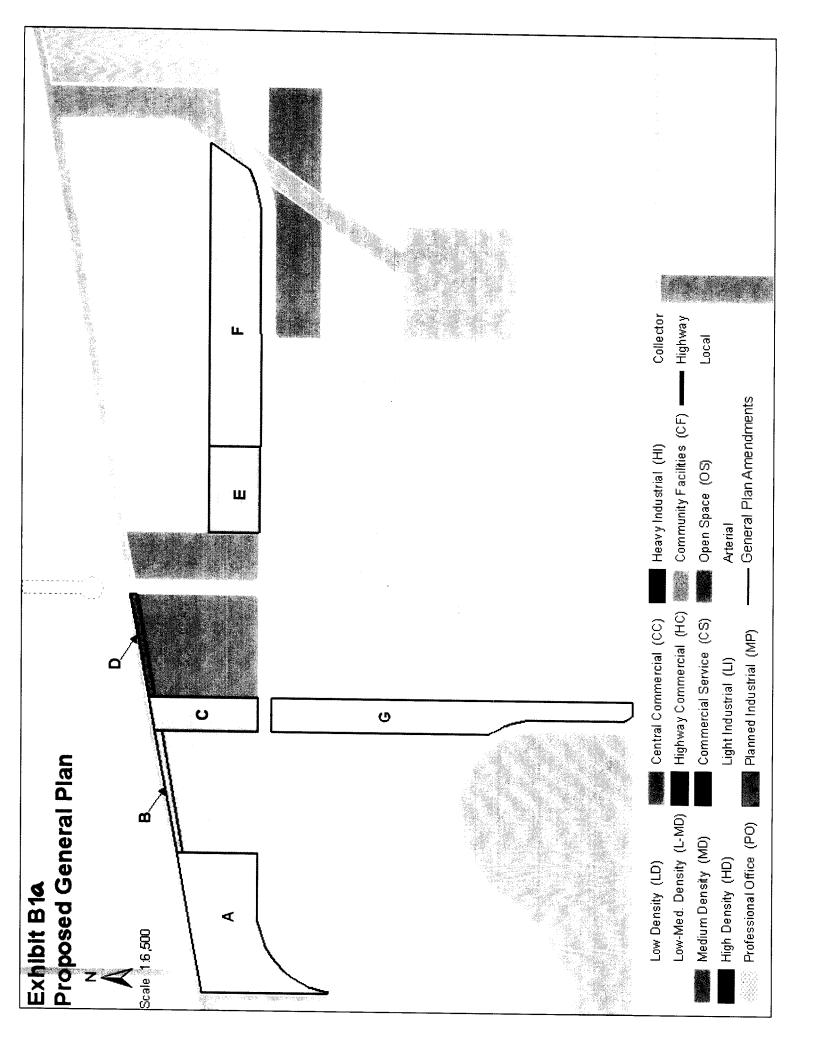
STATE OF CALIFORNIA	)	
COUNTY OF KINGS	)	SS.
CITY OF LEMOORE	ĺ	

I, NANCI C.O. LIMA, City Clerk of the City of Lemoore, do hereby certify the foregoing Resolution of the City Council of the City of Lemoore was duly passed and adopted at a Regular Meeting of the City Council held on June 6, 2006.

DATED:

Nanci C.O. Lima, City Clerk





# ADDENDUM TO NEGATIVE DECLARATION FOR THE CITY OF LEMOORE

For Tract 845 - VICTORY VILLAGE
Tentative Subdivision Map No. 2005-02/ PUD No. 2005-01/ CUP No. 2005-01
/ Site Plan Review 2005-01

The Planning Commission recommended that the City Council adopt a Mitigated Negative Declaration for the proposed project. However, Commission also added a condition requiring the upzoning of approximately 13.6 acres of medium density land on a portion of APN # 023-510-011 from medium density (zoned RM-3) to high density (zoned RM-2) in conformance with the Traffic Study.

This addendum outlines the traffic impacts if the proposed change were carried out on the project area or the Bush Street interchange area. The traffic analysis evaluated APN being changed, as shown on Exhibits B1 and B2, effecting 4 acres of commercial area and the remaining 13.6 acres to accommodate between 85-234 housing units. However, based on other concerns, no more than 180 units should be approved on this 13.6 acre piece. This increased density only affected the queing at Belle Haven and Bush Drive west bound traffic to require an additional through lane over the original mitigation measures. This additional through lane can be accommodated by sharing with the previously proposed separate right-turn lane and has been incorporated into the mitigation measures/conditions outlined in Resolution 2006-28. Based on this change being incorporated, there will not be a negative affect to traffic and staff recommends that the Council still adopt a Mitigated Negative Declaration pursuant to California Environmental Quality Act for the project, incorporating the TIS with the addendum information.

## **RESOLUTION NO. 2008-26**

A RESOLUTION OF THE COUNCIL OF THE CITY OF LEMOORE CERTIFYING FINAL ENVIRONMENTAL IMPACT REPORT (EIR) PREPARED FOR THE CITY OF LEMOORE 2030 GENERAL PLAN AREA, WITH ATTACHMENTS: (A) STATEMENT OF SIGNIFICANT POTENTIAL ADVERSE **ENVIRONMENTAL EFFECTS** AND ASSOCIATED MITIGATION MEASURES TO AVOID OR SUBSTANTIALLY LESSEN THOSE EFFECTS TO A LEVEL OF LESS THAN SIGNIFICANT; (B) FINDINGS ON PROJECT ALTERNATIVES; (C) STATEMENT OF UNAVOIDABLE SIGNIFICANT ADVERSE EFFECTS THAT ARE MITIGATED TO THE EXTENT FEASIBLE; AND (D) STATEMENT OF OVERRIDING CONSIDERATIONS THAT THE PROJECT BENEFITS OUTWEIGH THE UNAVOIDABLE SIGNIFICANT ADVERSE EFFECTS (STATE CLEARINGHOUSE NO. 200608113)

WHEREAS, the City of Lemoore (hereinafter "City") has prepared an Environmental Impact Report (EIR) relating to and environmentally assessing environmental effects related to the City of Lemoore 2030 General Plan (hereinafter, "Project"); and,

WHEREAS, in accordance with the provisions of the California Environmental Quality Act ("CEQA") and the Guidelines for the Implementation of the California Environmental Quality Act ("CEQA Guidelines"), a Notice of Preparation of the Draft EIR was filed and the Draft EIR was routed to the State Clearinghouse, responsible agencies, other governmental and trustee agencies having discretionary approval or jurisdiction by law over natural resources affected by the Project, the County of Kings, affected transportation agencies, affected taxing entities, and other interested persons and agencies; and,

**WHEREAS**, a 45-day public review and comment period was provided on the Draft EIR in accordance with CEOA; and,

WHEREAS, a Notice of Availability/Notice of Completion of the Draft EIR was published in The Lemoore Advance and posted with the County of Kings Clerk's Office; and,

WHEREAS, at the close of the public review and comment period on the Draft EIR, a Final EIR was prepared that incorporates comments received on the Draft EIR, list of persons, organizations and public agencies commenting on the EIR, the City's response to significant environmental points raised in the comments, and necessary changes to the text of the Draft EIR; and,

WHEREAS, the City's response to comments were transmitted to those agencies and individuals who submitted comments on the Draft EIR; and,

WHEREAS, the EIR consists of both the Draft EIR and the Final EIR; and,

WHEREAS, the EIR was considered by the City of Lemoore Planning Commission at public hearings on February 25, 2008 and March 10, 2008 after considering testimony presented at said

public meetings, the Planning Commission made its recommendation that the City Council certify the Final EIR; and,

WHEREAS, a duly noticed public hearing was held by the City Council on May 6, 2008 to consider all written and oral evidence and testimony related to the EIR; and,

WHEREAS, the 2030 General Plan includes an area, west of State Route 41, which is impacted by significant noise generated by the overflight of military aircraft of the adjacent Lemoore Naval Air Station; and,

**WHEREAS**, the City desires to create policies and mitigation measures which, to the greatest extent allowed by law, will address and mitigate the ambient noise in this area of the General Plan based on findings under Health and Safety Code section 17922.7.

**NOW THEREFORE, BE IT RESOLVED**, by the Council of the City of Lemoore that the City hereby certifies that the EIR has been prepared and completed in compliance with CEQA and the CEQA Guidelines.

**BE IT FURTHER RESOLVED** that the EIR has been presented to the Council as the decision-making body of the City, and that the Council has independently reviewed, considered, and evaluated the information contained in both the Draft and Final EIR and written and oral testimony presented at the aforementioned public hearing; and

**BE IT FURTHER RESOLVED** that the final EIR reflects the independent judgment of the Council; and

BE IT FURTHER RESOLVED that in the independent judgment of the Council, said body finds that certain changes, alterations, or mitigation measures will substantially lessen or avoid potentially significant adverse environmental effects identified in the Final EIR and will be incorporated into the Project as conditions of any future Project entitlements, permits and agreements. The potentially significant effects and the mitigation measures to be required in, or incorporated into, the Project to avoid or substantially lessen the significant effects to a level of less than significant are set forth in attached Exhibit "A," which is incorporated herein; and

**BE IT FURTHER RESOLVED** that based upon substantial evidence in the record of these proceedings, the "No Project" and other alternatives which are described and evaluated in the EIR as a reasonable range of potentially feasible alternatives to the Project would result in the same or greater adverse environmental effects as the proposed Project and could not feasibly attain most of the basic objectives of the Project, as set forth in Exhibit "B" and incorporated herein; and

**BE IT FURTHER RESOLVED** that certain unavoidable significant environmental effects resulting from Project implementation and mitigation measures to reduce these effects have been identified by the EIR, but it is infeasible to avoid or substantially lessen these effects due to specified economic, social, or other considerations. These environmental effects and mitigation measures, together with the considerations that make it infeasible to avoid or substantially lessen such effects, are set forth in attached Exhibit "C," which is incorporated herein; and

**BE IT FURTHER RESOLVED** that as required by CEQA, the City has balanced the benefits of the Project against the unavoidable significant environmental effects or risks in determining whether to approve the Project, and the Council has independently determined that the benefits of the Project outweigh the unavoidable significant environmental effects for the reasons stated in the Statement of Overriding Considerations, which is attached to this Resolution as Exhibit "D" and incorporated herein.

BE IT FURTHER RESOLVED that, pursuant to Health and Safety Code section 17922.7, the City hereby finds that the noise policies and noise attenuation mitigation measures as found in the EIR and in Exhibit "A" herein are necessary to address significant noise impacts emanating from the adjacent Lemoore Naval Air Station (LNAS) and its military aircraft on the areas located west of State Route 41. Pursuant to Health and Safety Code section 17922.7, the Council further finds that said policies and mitigation measures shall be applied to all single family dwellings, as well as multifamily dwellings and other structures located west of State Route 41. To the extent that the policies and mitigation measures exceed comparable standards referred to in section 17922.7 of the Health and Safety Code, then the more stringent standard shall apply. The Council hereby specifically finds that said standards are needed, in light of the extensive public testimony given before the Planning Commission and City Council related to noise impacts emanating from military flight activities in and around the adjacent LNAS, which testimony is incorporated herein by reference, to fully mitigate the impacts of noise.

**BE IT FURTHER RESOLVED** that all environmental impact mitigation measures and policies established by certification of the EIR are intended to be self-mitigating and, therefore, are made specific conditions of the Project, shall be binding upon the City, and shall be conditions of Project entitlements, permits, agreements and other legally-binding instruments.

**BE IT FURTHER RESOLVED** that the Mayor and Clerk of the City of Lemoore are hereby authorized and directed to make the appropriate certification upon the originals of the EIR documents and file them as so certified as a permanent record in the office of the City Clerk.

The foregoing resolution was approved and adopted at a regular meeting of the City Council of the City of Lemoore held on the <a href="https://example.com/hay_neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting-neeting

AYES: MARTIN, RODARMEL, HORNSBY, GREGO, MURRAY

NOES: NONE ABSTAIN: NONE ABSENT: NONE

APPROVED:

John F. Murray, Mayor

Janci C O Lima City Clerk

# EXHIBIT A

STATEMENT AND FINDINGS OF SIGNIFICANT POTENTIAL ADVERSE ENVIRONMENTAL EFFECTS, AND ASSOCIATED MITIGATION MEASURES TO AVOID OR SUBSTANTIALLY LESSEN THOSE EFFECTS TO A LEVEL OF LESS THAN SIGNIFICANT

Table ES-6 Summary of impacts and Proposed General Policies that Reduce the Impact

Proposed General Plan Policies that Reduce the Impact

Mitigation Significance after

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3.2-2. Implementation of the proposed General Plan will create traffic that will increase congestion. but not create unacceptable levels			proposed General Plan, an acceptable level of service will be maintained on City roadways.	3.2-1. Implementation of the proposed General Plan will cause an increase in traffic that is substantial in relation to the existing traffic load and capacity of the street system, but with planned improvements required by the
IU-1-41	C-I-11	C-I-10	C-I-9	C-1-7
Allow up to 1.5 million square feet of non-residential development in the Business, Technology & Industrial Reserve Area, with up to 60 percent industrial, 35 percent office flex/R&D space/support services, and 15 percent retail space. A specific plan, including a financing plan for public facilities and services must be percent retail space. A specific plan, including a financing plan for public facilities and services must be	Establish and implement additional programs to maintain adequate peak hour LOS at intersections and along roadway segments as circumstances warrant, including the following actions: Collect and analyze traffic volume data on a regular basis (at least every 5 years) and monitor current intersection and roadway segment levels of service on a regular basis. Use this information to update and refine the City's travel forecasting model, so that estimates of future conditions are more strongly based upon local travel behavior and trends. Consider, on a case by case basis, how to shift travel demand away from the peak period by changing work shift starting times, especially in those situations where peak traffic problems result from a few major generators (e.g. the West Hills College area and Bush Street corridor and the Industrial Area south of the City). Perform routine, ongoing evaluation of the efficiency of the urban street traffic control system, with emphasis on traffic signal timing, phasing and coordination to optimize traffic flow along arterial corridors. Use traffic control systems to balance arterial street utilization (e.g. timing and phasing for turn movements, peak period and off-peak signal timing plans).	Require traffic impact studies for any proposed General Plan amendment that will generate significant amounts of traffic (such as 100 or more peak hour trips).	Establish a Transportation Performance Monitoring (TPM) program for the Business, Technology, and Industrial Reserve Area, generally located in the Southwest quadrant of SR-198 and SR-41, to monitor and control traffic arising from new development.	Develop and manage the roadway system to obtain Level of Service (LOS) D or better for two hour peak periods (a.m. and p.m.) on all major roadways and arterial intersections in the City. This policy does not extend to local residential streets (i.e., streets with direct driveway access to homes) or state highways and their intersections, where Caltrans policies apply. Exceptions to LOS D policy may be allowed by the City Council in areas, such as Downtown, where allowing a lower LOS would result in clear public benefits, social interaction and economic vitality, and help reduce overall automobile use.

location, undergrounding of utilities, and signs; Open space protection program, including designation of Exhibit A - Page I of 31

permanent open space and buffers adjacent to farmlands and the freeway corridors; implementation

including standards and guidelines for building scale and location, landscaping, parking design and

Infrastructure necessary to support the area and benefit the City, Development and design program,

at a minimum to provide for: Road connections and traffic capacity to accommodate planned development;

would be maintained on the Marsh Drive interchange at SR-198 and other access roads. The specific plan is permitted under an approved specific plan only if the City determines that acceptable levels of service prepared prior to consideration of any development proposals. More non-residential space may be

LTS

but not create unacceptable levels

of service on State highways, with

planned improvements.

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

omformant .	capital and other improvements, phasing of development financing measures, ple	program, including Zoning Ordinance amendments (if required), cost and respo		Proposed General Plan Policies that Reduce the Impact
	sures, plan administration and	ad responsibility for necessary	Mitigation	Significance after
		į	İ	-3

by reference. The policies listed under Impact 3.2-1 serve to reduce this impact to a less than significant level and thus are incorporated here

- 3.2-3. Implementation of the 2nes requirements for left turn storage major access routes to future impacts at intersections and along General Plan may create local proposed General Plan could 3.2-4. Implementation of the in vehicles queuing and developments, including increases 7 by reference The policies listed under Impact 3.2-1 serve to reduce this impact to a less than significant level and thus are incorporated here LTS
- right-of-way and pavement standards to reflect adjacent land use and/or anticipated traffic, and permit Adopt street standards that provide flexibility in design, especially in residential neighborhoods. Revise reduced right-of-way dimensions where necessary to maintain neighborhood character. SLI
- 51.5 guidance of City staff. street designations and cross-street section standards. Further, ensure that either the Capital Improvement Require all new developments to provide right-of-way and improvements consistent with the General Plan Alternative improvements shall be considered if supported by a traffic assessment conducted under the Program budget or new development carries out the planned improvements described in the General Plan.

or incompatible uses as well as

emergency access needs. potentially adversely affect curves or dangerous intersections)

to a design feature (e.g., sharp

substantially increase hazards due

CI3 Provide for greater street connectivity by: Incorporating in subdivision regulations requirements for a where feasible; Requiring bicycle and pedestrian connections from cul-de-sacs to nearby public areas and housing); Encouraging the construction of roundabouts instead of traffic signals and 4-way stop signs. two access points for every 10 acres of development, with additional access, if warranted, for multi-family minimum number of access points to existing local or collector streets for each development (e.g. at least

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

		3.2-5. Implementation of the General Plan will have an impact on pedestrian circulation and bicycling.		
C-I-27	C-I-23	C-I-20	C-I-12	Proposed G
Implement the Lemoore Bikeway Plan in coordination with the County's Regional Bicycle Plan, which is updated every four years.	Provide incentives for City employees to commute by public transit, car-pool, or use alternative fuel technology vehicles.	Coordinate with Caltrans and Kings Area Rural Transit to identify and implement Park & Ride sites with LTS convenient access to public transit.	main streets; and Requiring new residential communities on undeveloped land planned for urban uses to provide stubs for future connections to the edge of the property line. Where stubs exist on adjacent properties, new streets within the development should connect to these stubs.  To increase roadway safety and maintain emergency access between Cedar Lane and Lemoore Avenue: Construct the extension of Cedar Lane to Lemoore Avenue as a two-lane collector roadway with restricted access on Lemoore Avenue to only allow northbound left, southbound right, and eastbound right turn movements; Construct a center median along Lemoore Avenue to restrict all other movements (such as eastbound left turns from Cedar Lane onto northbound Lemoore Avenue). Further, do not provide any pedestrian crossings of Lemoore Avenue at this location; Provide crosswalks across Cedar Lane at Lemoore Avenue; Provide a stop control sign on the Cedar Lane approach to the intersection with Lemoore Avenue. No signalized traffic controls will be provided on Lemoore Avenue; and, Cul-de-sac Larish Street approximately 220 feet west of Lemoore Avenue to eliminate cut-through traffic and better facilitate southbound left turn movements into the High School parking lot.	Proposed General Plan Policies that Reduce the Impact
		LTS		Significance after Mitigation

needed; Providing bicycle paths or lanes on bridges and overpasses; Ensuring that all new and improved streets have bicycle-safe drainage grates and are kept free of hazards such as uneven pavement, gravel, and other debris; Providing adequate signage and markings warning vehicular traffic of the existence of merging

Increase bicycle safety by: Sweeping and repairing bicycle lanes and paths on a regular basis; Ensuring that bikeways are delineated and signed in accordance with Caltrans' standards, and lighting is provided, where

Establish bicycle lanes, bike routes, and bike paths consistent with the General Plan.

C-I-28

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

	7		
	Amend the Zoning Ordinance to require large employers to implement a Traffic Demand Management program that combines parking restrictions with transit or bicycle subsidies, such as promoting carpooling, free bus passes, priority bicycle parking and car share programs.	C-1-36	create additional demand for parking facilities but on-street and off-street parking capacity will
LTS	Ensure that all residential development provides adequate on-site parking for residents and guests.	C-I-35	3.2-6. Implementation of the proposed General Plan Undate will
	Amend the Zoning Ordinance to include standards in all new development for pedestrian circulation including: patterned concrete sidewalks across vehicular streets, crossing signalization, bulb-outs, bicycle parking and lockers integrated with parking areas, and street lighting.	C-I-34	
	Establish specific standards for pedestrian facilities to be accessible to physically disabled persons, and ensure that roadway improvement projects address mobility or accessibility for bicyclists or pedestrians.	C.I-33	
	Provide for pedestrian-friendly zones in conjunction with the development, redevelopment, and design of mixed-use neighborhood core areas, the Downtown area, schools, parks, and other high use areas by Providing intersection "bulb outs" to reduce walking distances across streets in the Downtown and other high use areas; Providing pedestrian facilities at all signalized intersections; Providing landscaping and shade that encourages pedestrian use; Constructing adequately lit and safe access through subdivision sites; and Providing mid-block electronic warning lights and signals, where warranted, to inform motorist of the presence of pedestrian at the crosswalk.	C-1-32	
	Develop a series of continuous walkways within new office parks, commercial districts, and residential neighborhoods so they connect to one another.	C-I-31	
,	Amend the Zoning Ordinance to require bicycle parking facilities at large commercial and industrial employer sites, including racks and lockers that are integrated into the overall site and building design.	C-I-30	
	or crossing bicycle traffic where bike routes and paths make transitions into or across roadways; Working with the Lemcore Union School districts to promote classes on bicycle safety in the schools; and Installing large sidewalks along arterial and median parkway streets so that children may ride safely away from traffic (e.g., Lemoore Avenue and Hanford-Armona Road).		
Significance after Mitigation	Proposed General Plan Policies that Reduce the Impact	Proposed t	
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Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

		Designate land for public uses to be maintained through capital projects for parks and open spaces, police and fire services, water and sanitary facilities, infrastructure and other City services.	LU-I-43	which may exceed supply.
	LTS	Create, maintain, or upgrade Lemoore's public and private infrastructure to support future land use and planned development under the General Plan.	LU-I-6	3.3-2. Implementation of the proposed General Plan will increase demand for nublic water
		Develop partnerships with Lemoore Union Elementary School District and Lemoore Union High School District to optimize the joint use of school facilities for community benefit.	PSCF-J-15	
		Develop partnerships with the Lemoore Union Elementary School District, Lemoore Union High School District, private schools, community organizations, and West Hills College to facilitate planning for new school sites and facilities and infrastructure improvements which are compatible with City plans.	PSCF-J-14	
		Ensure adequate elementary and high school sites are reserved in new subdivisions, consistent with the Land Use Diagram and State law.	LU-I-42	
		Allow development only when adequate public facilities and infrastructure are available or planned in conjunction with the use, consistent with the traffic level of service (LOS) standards and standards for public facilities and services established in this Plan.	LU-L-8	capacity of existing schools, and require new school facilities.
	LTS	Require new development to pay its fair share of the costs of public infrastructure, services and LTS transportation facilities, in accordance with State law.	EU-I-7	3.3-1. Implementation of the proposed General Plan will increase enrollment, exceed the
				Public Utilizes and Services
		Amend the City's Parking Design Standards to promote multiple benefits, including shared parking for mixed-use projects, solar panels on parking structures to generate energy for parking lot lighting, and pervious pavement for parking lots to improve groundwater recharge.	CI-38	
		Amend the Zoning Ordinance to allow shared parking for mixed-uses where peak parking demands do not overlap.	C-I-37	accommodate these needs.
ofter.	Significance after Mitigation	Proposed General Plan Policies that Reduce the Impact	Proposed Gen	

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

Proposed G	Proposed General Plan Policies that Reduce the Impact  CD-I-59 Require new development to reduce storm water run-off, control water pollution, and promote water recharge through sustainable hydrological design. Measures should include, but are not limited to, the following: Reducing imperviousness by limiting building footprint and using permeable paving or
	following: Reducing imperviousness by limiting building footprint and using permeable paving or landscaping to break up expanses of impervious surfaces; Using canopy trees or shrubs to absorb rainwater and slow water flow; Removing curbs and gutters from streets and parking areas, where appropriate, to allow storm water sheet flow into vegetated areas; Incorporating drainage design into the infrastructure, such as roof downspouts, retention cells, or infiltration trenches, to filter and direct storm water into vegetated areas or water collection devices; and Promoting the installation of sub-surface water retention facilities (for large development) to capture rainwater for use in landscape irrigation and non-potable uses.
· PU-I-1	Update the City's Urban Water Management Plan every five years and ensure its contents are consistent with the California Water Code and General Plan policies, including prioritization and identification of funding sources.
PU-I-2	Provide and maintain a system of water supply distribution facilities capable of meeting existing and future daily and peak demands, including fire flow requirements, in a timely and cost effective manner.
PU-I-3	Monitor the demands on the water system and, as necessary, manage development to mitigate impacts and/or facilitate improvements to the water supply and distribution systems.
PU-I-4	Continue to support the Laguna Water District's groundwater recharging (water banking) efforts, in consultation with the State Department of Water Resources and County water management authorities.
S-I-Ad	Require that necessary water supply infrastructure and storage facilities are in place concurrently with new development, and approve development plans only when a dependable and adequate water supply for the development is assured.
6-I-nd	Promote the use of evapotranspiration (ET) water systems in irrigating large parks and large landscaped areas.
PU-1-11	Revise regulations to allow the safe use of reclaimed water ("gray water") by homes and businesses where E-18

# Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

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feasible. Examples of areas where "gray water" might be safely used include: Irrigation of parks and residential yards, and irrigation for farming; Cooling towers and HVAC systems in commercial or industrial buildings; and Water cistems in flush toilets.	Proposed General Plan Policies that Reduce the Impact N	Suitilling on the control of the con
	znificance after Nigation	

- PU-I-12 as to protect against aquifer overdrafts and water quality degradation. irrigation districts to retain surface water rights and supply following annexation and urban development so Establish and implement a program of cooperative surface water use with local water purveyors and
- PU-I-14 Drill additional wells within the City when other water supply alternatives are not feasible, and demand warrants their development.
- COS-1-26 Establish water conservation guidelines and standards for new development and for municipal buildings and facilities.
- COS-I-27 Become a signatory to the California Urban Water Conservation Council and implement all Demand Management Measures as soon as they become feasible.
- COS-I-28 Develop a schedule for the retrofitting of existing public buildings with water conservation features, and budget accordingly.
- COS-I-29 Annually check for leaks throughout the City's main water supply and distribution system, and initiate repairs when necessary to reduce water waste.
- COS-I-30 Consider establishing rebate and/or incentive programs for the replacement of leaking, aging and/or inefficient plumbing with more efficient, water saving plumbing and for the use of water efficien
- COS-I-31 Educate the general public about the importance of water conservation, water recycling and groundwater by schools or organized groups; Encouraging educators to include water conservation in their curriculums; Providing tips to business groups on water conservation and recycling. recharge through the following means: Making water production and treatment facilities available for tours

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

Table ES-6	Summary of Impa	cts and Pro	Summary of Impacts and Proposed General Policies that Keduce the Impact	
		Proposed Ge	Proposed General Plan Policies that Reduce the Impact Mit	Significance after Mitigation
3.3-3. Implementation of proposed General Plan will	Implementation of the ad General Plan will	PU-I-15	Maintain existing levels of wastewater service by expanding treatment plant and disposal facilities as LTS required by growth and by the Regional Water Quality Control Board.	Si .
generate waste exceed the tre capacity of the	generate wascewater nows that exceed the treatment and disposal capacity of the existing wastewater	PU-I-16	Update the Wastewater Master Plan by 2010 and construct planned facilities to serve development under this General Plan.	
treatment plant	7	PU-I-17	Establish impact fees and sewer rates adequate to finance required wastewater treatment and disposal facilities upgrades or replacements.	
3.3-4. Implementation of proposed General Plan will	Implementation of the ed General Plan will	PU-I-19	Continue to require property owners to provide recycling containers in refuse collection areas that are LTS within buildings or screened so as not to be visible from public streets and residential neighborhoods.	75
solid waste that a disposal capacity.	generate auditorial autoritis or solid waste that exceed available disposal capacity.	PU-1-20	Reduce waste production by using post-consumer recycled paper and other recycled materials in all City operations.	
		PU-I-21	Implement programs to reduce waste at home and in businesses through public education efforts that use many different forms of communication.	
		PU-I-22	Amend local ordinances to further support KWRA requirements for proper handling and storage of solid waste and recyclables and diversion of solid waste from landfills.	
		PU-I-23	Explore ways to provide financial incentives for recycling by reducing the cost for recycling and increasing the cost for garbage disposal.	
		PU-I-24	Actively promote reuse by supporting existing and future swap meets, flea markets and consignment/second-hand shops and providing information on donation pick-up or drop off locations, as well as other waste reduction programs, on the City website.	
3.3-5. Imple proposed Gel	3.3-5. Implementation of the proposed General Plan will place a kigher demand on available police	LU-1-43	Designate land for public uses, to be maintained through capital projects for parks and open spaces, police LTS and fire services, water and sanitary facilities, infrastructure and other City services.	TS
and fire prote	and fire protection services and	SN-I-13	Ensure Fire Department personnel are trained in wildfire prevention, response and evacuation procedures.  E-20	
			E-20 .	

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

	Proposed G	Propased General Plan Policies that Reduce the Impact Mi	Significance after Mitigation
increase the risk from crime and	SN-I-15	Enforce the Uniform Fire Code the approval of construction plans and final occupancy permits.	
	SN-I-22	Assess the manpower, facility, and equipment needs of Police and Fire services at least every three years in order to provide all residents with an optimal level of protection.	,
	SN-I-24	Develop an additional police station with improved access to parts of Lemoore west of SR-41 and parts south of SR-198, when necessary to maintain performance and response standards.	
	SN-I-25	Maintain mutual aid agreements with Kings County, Naval Air Station Lemoore, neighboring law enforcement agencies and the California Highway Patrol.	
	SN-I-26	Collaborate, and exchange information with other local, state and federal agencies and with utility service providers in activities related to terrorism prevention and response.	
	SN-I-27	Maintain Fire Department performance and response standards at Class 3 ISO rating or better, including building and staffing a new fire station in West Lemoore if necessary.	
	SN-I-28	Require adequate access for emergency vehicles in all new development, including adequate widths, turning radii, and vertical clearance on new streets.	
	SN-1-29	Require sprinklers in buildings exceeding 5,000 square feet and all mixed use development to protect residential uses from non-residential uses, which typically pose a higher fire risk.	
	SN-1-30	Maintain mutual aid agreements with Kings County, California Department of Forestry, Naval Air Station Lemoore, and nearby cities for fire and disaster services.	
	The policie	The policies LU-I-7, LU-I-8 under Impact 3.3-1 and LU-I-43 under Impact 3.3-2 serve to reduce this impact to a level that is less than significant and thus are incorporated here by reference.	

3.4-). Buildout of the General Plan will increase the ratio of PSCF-I-1 Establish a goal of 6 acres of parkland per thousand residents which will be met by: Dedication and LTS

Table ES-6. Summary of Impacts and Proposed General Policies that Reduce the Impact

	Improve the 19th Avenue Park and other existing parks that are not fully developed with features such as landscaping, trails, children's play areas, and a hedge or wall where the park adjoins the freeway.	PSCF-I-10	the facility could occur or be accelerated.
LTS	Develop new parks with high quality facilities, universal accessibility, durability and low maintenance in mind. Existing parks will be improved, if feasible and economically justified, to reduce maintenance cost and water use, as well as improve park safety and aesthetics.	PSCF-I-4	3.4-3. Buildout of the General Plan will result in the increase in use of existing parks such that substantial physical deterioration of
٠.	The policies PSCF-I-1, PSCF-I-3, and PSCF-I-13 under Impact 3.4-1 also help to increase accessibility are incorporated here by reference.	The policies in the preference.	
	Develop a system of consistent, recognizable and pedestrian-scale signage for the parks and trail system throughout the City, including bikeways, pathways and sidewalks that link key community resources (e.g. schools, public facilities, and transit) to the parks and open space network.	PSCF-I-7	residents living within ½ mile of a community park and ½ mile of neighborhood or pocket park
N B II	Require that at least 75 percent of new residents live within a half mile or less of a public park facility, using the development permit review and approval processes.	PSCF-I-2	3.4-2. Implementation of the proposed General Plan will increase the percentage of
.,	Adopt and implement a 10-year Parks and Recreation Master Plan to be reviewed biennially and updated quadrennially.	PSCF-I-13	
	Require non-residential developers to contribute to the City's parks and open space system based on proportional share of needs generated and use of facilities, in compliance with the State Mitigation Fee Act and other applicable laws.	PSCF-I-3	•
	reservation requirements, consistent with the Quimby Act, for development of landscaped open spaces, parks, trail systems, and/or special community service facilities in new residential developments based on a standard of 5 acres of developed parkland per thousand residents; and A standard of one acre per thousand residents to be met with an impact fee for City-owned and operated parks and special recreation areas that serve all residents.	,	parkland from the existing 5 acres per thousand residents but still fall short of the City's goal of 6 acres per thousand residents.
Significance after Mitigation	Proposed General Plan Policies that Reduce the Impact	Proposed Gen	

The policies PSCF-I-1, PSCF-I-3, and PSCF-I-13 under Impact 3.4-1 serve to reduce this impact to a level that is less than

3.5-2. Implementation of the proposed Lemoore General Plan could have a substantial adverse effect on riparian habitat identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service,

3.5-3. Development under the proposed General Plan could have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (Including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological

incorporated here by reference. The policies summarized under Impact 3.5-1 serve to reduce this impact to a level that is less than significant and thus are LTS

incorporated here by reference. The policies summarized under Impact 3.5-1 serve to reduce this impact to a level that is less than significant and thus are LTS

### interruption, or other means, Hygirology land. Water Quality

3.6-1. Implementation of the PU-I-2 proposed General Plan could indirectly contribute to violations of water quality standards.

PU-I-3

daily and peak demands, including fire flow requirements, in a timely and cost effective manner. Provide and maintain a system of water supply distribution facilities capable of meeting existing and future LTS

Monitor the demands on the water system and, as necessary, manage development to mitigate impacts and/or facilitate improvements to the water supply and distribution systems.

COS-I-16 Develop programs to monitor the quality of local groundwater and runoff that discharges directly into

### Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

COS-I-17

waterways to prevent pollution, in conjunction with rederal, State, local agencies, and the private sector.		Proposed General Plan Policies that Reduce the Impact	THE PARTY OF THE P
	Mitigation	Significance after	

COS-1-21 Protect groundwater recharge areas by carefully regulating the type of development within and adjacent to Require developers to construct and maintain permanent water control facilities (storm water basins or Engineer, to control storm water and protect areas from flooding. Facilities shall incorporate the following retention ponds) for new development in the Westside and other areas deemed necessary by the City

COS-I-45 COS-I-23 Utilize more plants and trees in public area landscaping, focusing on those that are documented as more Continue to prohibit septic tanks and drain fields to prevent pollution of subsurface water resources currently lack sufficient water to survive; and Other design features consistent with the Regional Water A fenced "low-flow" area to contain potential contaminants; Regularly-tilled top soil to maintain good Quality Control Board's Best Management Practices. percolation; When feasible, storm drainage facilities to channel water into the re-created wetlands which

PU-I-7 a water management plan: Large projects will be required to submit planting plans, irrigation plans, Require all major new development projects with more than 200,000 square feet of floor area overall to have projects will be required to submit plans for water recycling. They will also be required to submit irrigation schedules, and water use estimates for City approval prior to issuance of building permits; industrial efficient pollutant absorbers.

LTS

COS-I-24 Control use of potential water contaminants through inventorying hazardous materials used in City operations, listing possible changes that can be made, educating City employees, and designing and implementing a re-placement/reduction program.

surrounding water resources (from the regional storm drain system or nonpoint source pollution entering

either construction or long-term

development).

storm water runoff and entering

plans for proposed landscaping

proposed General Plan would

3.6-2. Implementation of the result in substantially increased

significant and thus are incorporated here by reference. Policies COS-L-25 under Impact 3.5-1 and COS-L-16 under Impact 3.6-1 serve to reduce this impact to a level that is less than

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

	Proposed Ge	Proposed General Plan Policies that Reduce the Impact	Significance after
			Mitigation
3.6-3. Implementation of the	COS-I-18	Update, monitor and implement the City's Storm Drainage Master Plan.	LTS
result in storm drainage systems being inadequate to accommodate in year flood flows	COS-J-19	Establish and implement a storm drainage fee in conjunction with the recreation impact fee to acquire and develop combined pond basin/park sites and/or other drainage facilities to mitigate growth impacts.	
Too Just Hoose Horses	COS-I-20	Require temporary on-site storm drainage basins in subdivisions and other development proposals, if needed, until storm drainage plans for that area are completed and formal connections are implemented. Design should take into consideration the properties of soils on the site.	
	COS-I-22	Require on-site storm drainage to drain away from the streets in areas with no curbs and gutters.	
	SN-I-8	Require all new development within a flood zone to comply with the City's Flood Damage Prevention Ordinance.	,
	SN-I-9	Identify and remedy deficiencies in the existing storm drainage infrastructure in partnership with regional and federal agencies.	
	LU-I-6	Create, maintain, or upgrade Lemoore's public and private infrastructure to support future land use and planned development under the General Plan.	
	LU-1-7	Require new development to pay its fair share of the costs of public infrastructure, services and transportation facilities, in accordance with State law.	
	. LU-1-8	Allow development only when adequate public facilities and infrastructure are available or planned in conjunction with use, consistent with the traffic level of service (LOS) standards and other standards for public facilities and services established in this Plan.	
3.6-4. Implementation of the proposed General Plan would result in increased rates and amounts of runoff due to additional impervious surfaces, higher runoff	CD-I-59	Require new development to reduce storin water run-off, control water pollution, and promote water recharge through sustainable hydrological design. Measures should include, but are not limited to, the following: Reducing imperviousness by limiting building footprint, using permeable paving or landscaping to break up expanses of impervious surfaces; Using canopy trees or shrubs to absorb rainwater and slow	LTS

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

	3.6-6. Implementation of the SN-I-10 R proposed General Plan would m	e to the CD-1-32 rylous	3.6-5. Implementation of the PU-I-4 C proposed General Plan would cc	The policies summarized undd incorporated here by reference.	values, or alterations to drainage wasystems that could cause potential was flood hazards.	Proposed Genera
SN-I-11 Require developers to provide for the ongoing maintenance of detention basins.  The policies SN-I-8 and SN-I-9 listed under Impact 3.6-3 serve to reduce this impact to a level that is less than significant and thus are incorporated here by reference.	Require new development to prepare hydrologic studies and implement appropriate mitigation measures to minimize surface water runoff and reduce the risk of flooding.	Promote the innovative treatment of parking areas in order to reduce their negative environmental impact and avoid the appearance of a "sea of asphalt". Strategies include but are not limited to: Establish specific standards for shading and plant large trees throughout the parking area as well as along streets, sidewalks, and pathways; Separate pedestrian pathways from car lanes where possible; and Use porous paving and a variety of drainage features according to the site.	Continue to support the Laguna Irrigation District's ground water recharging (water banking) efforts, in LTS consultation with the State Department of Water Resources and county water management authorities.	The policies summarized under Impact 3.6-3 serve to reduce this impact to a level that is less than significant and thus are incorporated here by reference.	water flow; Removing curbs and gutters from streets and parking areas, where appropriate, to allow storm water sheet flow into vegetated areas; Incorporating drainage design into the infrastructure, including roof down-spouts, retention cells, or infiltration trenches, to filter and direct storm water into vegetated areas or water collection devices; and Requiring the installation of sub-surface water retention facilities (for large development) to capture rainwater for use in landscape irrigation and non-potable uses.	Proposed General Plan Policies that Reduce the Impact
	LTS		LTS			Significance after Mitigation

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

Prop	exposure in the "Conditionally	Acceptable (perween 80 ds and SN-I-32 SN-I-32	Unacceptable" (between 70 dB and SN-I-33 75 dB) ranges; or cause interior noise levels in habitable rooms in	multi-family dwellings to exceed 45 SN-I-34 dB due to traffic noise.	SN-1-35	2N-1-26
Proposed General Plan Policies that Reduce the Impact	enforcement procedures.	Use the community noise compatibility standards, shown in Table 3.8-4 as review criteria for new land uses.	33 Consider an increase of five or more dBA to be "significant" if the resulting noise level would exceed that described as "normally acceptable" in Table 3.8-4.	Apply performance-based noise standards within zoning classifications likely to encompass sensitive land uses.	Require that all new residential development achieve noise level reductions to meet the land use compatibility standards through acoustical design and construction of the building elements: Residential building designs must be based upon a minimum interior design noise level reduction of 40 dB in all habitable areas (i.e., garages, storage areas, etc. are excepted). The 40 dB criteria must provide a minimum constructed noise level reduction of 35 dB; and Residential building designs must also be based upon a minimum design noise level reduction of 45 dB in all bedrooms. The 45 dB criteria must provide a minimum constructed noise level reduction of 40 dB.	36 Establish standards for the basic elements of noise reduction design for new dwellings exposed to DNL
Significance after Mitigation		w land uses.	exceed that	ensitive land	ne land use : Residential 40 dB in all a minimum ased upon a st provide a	read to DAT

- with substantial weight and insulation; Sound-rated windows providing noise reduction performance and heavy roofs with ceilings of two layers of gypsum board on resilient channels. noise level reduction requirements it will likely be necessary to use double-stud construction, double doors, ventilation system affording comfort under closed-window conditions is required; and To meet the highest entries; Acoustic baffling of vents is required for chimneys, fans and gable ends; Installation of a mechanical providing noise reduction performance similar to that of the façade must be included for all exterior similar to that of the façade must be included for habitable rooms; Sound-rated doors or storm doors above 65 dB (anticipated for areas west of SR-41), including the following: All facades must be constructed
- SN-1-37 compliance with compatibility standards, including: Pre-manufactured housing and mobile homes built with framing less than  $2 \times 4$  inches; Facades using aluminum, vinyl or other exterior siding weighing less Prohibit construction materials and methods that do not provide enough noise insulation to ensure

## Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

Proposed General Plan Policies that Reduce the impact

Significance after Mitieation

than 5 psf; Façade construction without insulation; Flat roofs without an interstitial cavity space or with a space less than 10 inches (i.e., no monolithic T&G roof/celling systems); Jalousie or other lightweight or poor-sealing window systems; and Packaged terminal air-conditioning (PTAC) units (i.e., through-the-wall air-conditioning).

- SN-I-38 noise level reduction has been prepared by a Board Certified Acoustical Engineer for each dwelling uni Require that all residential building designs for sites where the CNEL will exceed 65dBA include supporting the Lemoore Building Department with building permit applications. This letter must be received and information for City review and approval demonstrating that an acoustical design providing the necessary and approval prior to the issuance of an occupancy permit. approved prior to the issuance of a building permit; Following construction, a letter by the Board Certified prior to construction. Elements of this acoustical review process shall include: A letter by a Board Certified dwelling unit (or group of units, if identical), submitted to the Lemoore Building Department for review Engineer approving the acoustical design of each dwelling unit (or group of units, if identical), submitted to Engineer showing noise level reduction test results for a minimum of two habitable areas within each
- Develop uniform guidelines for acoustical studies based on current professional standards in the Noise Control Ordinance.
- SN-1-40 of permit approval through appropriate means, including, but not limited to: Screen and control noise Require developers to mitigate the noise impacts of new development on adjacent properties as a condition arterials when compatible with aesthetic concerns and neighborhood character. This would be a developer and trash pickup, to minimize noise impacts; and As a last resort, construct noise walls along highways and design, landscaping and running water to mask sounds; Control hours of operation, including deliveries buffers; Use soundprooting materials and double-glazed windows; Use open space, building orientation and setbacks for noise sources from adjacent dwellings; Retain fences, walls, and landscaping that serve as noise sources, such as parking and loading facilities, outdoor activities, and mechanical equipment, Increase
- SN-1-41 Promote the use of noise attenuation measures to improve the acoustic environment inside residences

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

	Proposed G	Proposed General Flan Policies that Reduce the Impact Mitigation	Ę,
Address of the state of the sta		where existing single-family residential development is located on an arterial street.	1
	SN-I-42	Establish criteria for evaluating applications from residents for exceptions to residential noise level requirements for the operation of standby electrical equipment used to meet medical needs.	
3.8-2. Implementation of the	SN-1-43	Require new noise sources to use best available control technology (BACT) to minimize noise emissions.	
potentially expose existing noise- sensitive uses to construction-	SN-J-44	Require noise from permanent mechanical equipment to be reduced by sound-proofing materials and sound-deadening installation.	
related noise consisting of groundborne vibration and ambient SN-I-45 noise.	SN-1-45	Minimize vehicular and stationary noise sources and noise emanating from temporary activities, such as those arising from construction work.	

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

		1		
				has the potential to create structural damage from placing
	LTS	The policies summarized under Impact 3.9-1 serve to reduce this impact to a level that is less than significant and thus are LTS incorporated here by reference.	Тъе ро іпсогро	3.9-3. Implementation of the proposed Lemoore General Plan
		Control erosion of graded areas with vegetation or other acceptable method	5N-I-6	
		Require erosion and sedimentation plans for new development activities, including: The location and description of existing soil features and characteristics; The location and description of proposed changes to the site; and A schedule for the installation of control measures for each phase of development.	COS-1-6	substantial soil erosion or the loss of topsoil.
	r.	Adopt soil conservation measures to reduce erosion caused by landscaping, construction of new roadways and paths, building construction, and off-road vehicles.	COS-1-5	3.9-2. Implementation of the proposed Lemoore General Plan
•		Establish location standards and inspection requirements for above-ground storage tanks to minimize potential risks to life and property.	SN-I-7	
		Require utilities be designed to withstand probable seismic forces to be encountered in Lemoore.	SN-I-5	
		Require mitigation for structural alterations on load-bearing and un-reinforced masonry buildings to ensure structural safety.	SN-I-4	
		Facilitate stricter safety provisions for important or critical-use structures (such as hospitals, schools, fire, police, and public assembly facilities; substations and utilities) through input during site selection and a comprehensive geotechnical investigation.	SN-I-3	ground shaking, seismic related ground failure, landslides or liquefaction.
		Maintain and enforce appropriate building standards and codes to avoid or reduce risks associated with geologic constraints and to ensure that all new construction is designed to meet current safety regulations.	SN-I-2	structures to potential substantial adverse effects, including the risk of the following the f
	Ţ	Review proposed development sites at the earliest stage of the planning process to locate any potential geologic or seismic hazard.	SN-I-1	3.9-1. Implementation of the proposed General Plan has the proposed in expose people of
				Seismicandidadidadidadidad
ofter	Significance after Mitigation	Proposed General Plan Policies that Reduce the Impact	Proposed	
		Summary of impacts and recover General contest that weather the impacts	acts and	Table ES-6 Summary of imp

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

Proposed General Plan Policies that Reduce the Impact	Significance ofter
	Mitigation
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development on a potentially	,

proposed Lemoore General Plan may have the potential to create 3.9-4. Implementation of the unstable geologic unit or soil. risk to life or property by placing development on expansive soils.

incorporated here by reference. The policies summarized under Impact 3.9-1 serve to reduce this impact to a level that is less than significant and thus are LTS

release of hazardous materials to the environment. SN-I-19	public or the environment through SN-I-18 reasonable foreseeable upset and	through the routine transport, use, COS-I-25 or disposal of hazardous materials	proposed General Plan would increase the probability of hazards to the public or the environment	3.10-1. Implementation of the COS-1-24	Hazamons Materials and Militaries of
nospitats, and residential neighborhoods.  Require remediation and cleanup of sites contaminated with hazardous substances.	Prohibit locating of businesses or expansion of businesses meeting federal Emergency Planning and Community Right-to-Know Act (EPCRA) reporting requirements within a quarter mile of schools,	Reduce the use of pesticides, insecticides, herbicides, or other toxic chemicals by households and farmers by providing education and incentives for Integrated Pest Management (IPM) practices.	operations, listing possible changes that can be made, educating City employees, and designing and implementing a re-placement/reduction program.	Control use of potential water contaminants through inventorying hazardous materials used in City	Hizzardoriki Materials vaid Wildinss

SN-1-20 Coordinate enforcement of the Hazardous Material Disclosure Program with the Kings County Health

SN-1-21 public education and awareness. Promote the reduction, recycling and safe disposal of household and business hazardous wastes through

Department to identify facilities producing, utilizing, or storing hazardous wastes.

Policies SN-I-22 and SN-I-30 listed under Impact 3.3-5 serve to reduce this impact to a less than significant level and thus are incorporated here by reference.

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

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entropy of the state of the sta	Proposed General Plan Policies that Reduce the Impact	Significance after Mitigation
3.10-2. Facilities developed under the proposed General Plan could emit hazardous emissions or	The policy SN-I-18 listed under Impact 3.10-1 serve to reduce this impact to a less than significant level and thus is incorporated here by reference.	
handle hazardous or acutely hazardous materials, substances, or		
waste within one-quarter mile of an existing or proposed school.		
3.10-3. New development under	The policies summarized under Impact 3.10-1 serve to reduce this impact to a level that is less than significant and thus are	LTS
the proposed General Plan could	incorporated here by reference.	
included on a list of hazardous		
materials sites compiled pursuant		
to government code section		
create a significant hazard to the		
brone of the distronment		
3.10-4. Buildout of the proposed General Plan could impair	The policies summarized under Impact 3.10-1 serve to reduce this impact to a level that is less than significant and thus are LTS incorporated here by reference.	LTS
implementation of or physically interfere with an adopted		
emergency response plan or emergency evacuation plan.		
3.10-5. Implementation of the proposed General Plan could increase the likelihood of people or	SN-1-14 Continue the City's Weed Abatement Program administered by the Volunteer Fire Department to reduce fire hazards before the fire season.	LTS
structures being exposed to wildiand fires, including where	SN-I-16 Utilize existing or new public awareness programs through the Volunteer Fire Department to highlight the dangers of open burning and how home owners can protect their properties from wildfres.	
wildlands are adjacent to urbanized areas or where residences are	SN-I-17 Update news media and City residents on current wildfire threat levels during drought periods.	
intermixed with Wildiands.	SN-1-27 Maintain Fire Department performance and response standards at Class 3 ISO rating or better, including	

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

SN-I-28 radii, and vertical clearance on new streets. Require adequate access for emergency vehicles in all new development, including adequate widths, turning

and thus are incorporated here by reference. Additionally, policies SN-I-13 and SN-I-15 listed under Impact 3.3-5 serve to reduce this impact to a less than significant level

(CITEMATINE SOURCES)		CHITTS H. R.S. CHPCE. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1.
3.1 I-1. New development under	LU-I-31	Promote the rehabilitation of historic structures in the Downtown in order to preserve the historic identity
the proposed General Plan has the potential to adversely affect		of the City for future generations.
historic resources that appear on	COS-I-32	Establish an inventory of historical buildings in Lemoore, including schools, churches, commercial
National or State historical or		buildings, public buildings, and residential buildings.
ar dideological airesticories or may		
be eligible for inclusion on such	COS-I-33	Require that new development analyze and avoid potential impacts to archaeological, paleoniological, and
lists.		historic resources by. Requiring a records review for development proposed in areas that are considered
		archaeologically or paleontologically sensitive; Determining the potential effects of development and
		construction on archeological or paleontological resources (as required by CEQA); Requiring pre-
		construction surveys and monitoring during any ground disturbance for all development in areas of
		historical and archaeological sensitivity; and Implementing appropriate measures to avoid the identified
		impacts, as conditions of project approval.
	COS-1-35	COS-I-35 Provide technical assistance with the registration of historic sites, buildings, and structures in the National

- Register of Historic Places, and inclusion in the California Inventory of Historic Resources.
- COS-1-36 Adopt a Landmarks and Historic Preservation District Overlay Zone or Ordinance to preserve all City, State, and federally-designated historic sites and structures to the maximum extent feasible.
- COS-I-37 Establish an interim design review process for proposed demolitions and extenor alterations and additions to non-residential buildings that are more than 75 years old. The Planning Commission will be the review

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

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authority, with their decisions subject to appeal to the City Council. Criteria to be considered in approving or conditionally approving the proposed change will include: For proposed alterations and additions: The project design is compatible with Secretary of the Interior Standards for the Treatment of Historic Properties and with the Downtown Revitalization Plan; and For proposed demolitions: The applicant has demonstrated that the existing use can not generate a reasonable rate of return; the existing building constitutes a hazard to public safety and is economically infeasible to rehabilitate, the design quality of the replacement building will be superior to the existing building and will be compatible with adjacent buildings and the character of Downtown Lemoore, or the proposed demolition or removal is necessary to allow a project that will have public benefits outweighing the public benefits of retaining the existing building.

- 8 and restore the Lemoore Canal to its natural appearance, and study the possibility of providing a bicycle Work with the Lemoore Canal and Irrigation Company and other canal companies to retain open canals trail along the canal.
- CD-I-4 Maintain scenic vistas to the Coalinga Mountains, other natural features, and landmark buildings.
- CD-1-24 corners. Corner buildings may be provided with special architectural features, including but not limited to: building only. Corners: Vertical volumes and changes in height to break up long facades are encouraged at Setbacks: Buildings must be built to the property line to create a uniform edge and a continued pedestrian openings on the lower floor for retail, and regularly spaced windows in upper floors. Massing: Buildings Require site and building design be consistent with Downtown's historic character: Design: All new and path. They may include recessed entry doors. Access: All new buildings must be orientated to face primary must follow the thythm of bays found in the district, each spaced approximately 20 to 30 feet apart renovated buildings must follow the traditional design patterns found along D Street, with large window landscaping will require canopy shade trees. arcades, or street furniture. Landscaping: Visible setbacks must be paved or landscaped, and parking lot Larger towers, gables, and turrets, a rounded or angled facet, pilasters, overhangs, a corner en-trance, corner streets and be accessible by a pedestrian entry. Access for loading and unloading may be via the rear of the

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

3.11-3. Implementation of the COS-I-33 Require proposed General Plan could historic adversely affect unidentified archaec paleontological resources.

human remains.

1-33 Require that new development analyze and avoid potential impacts to archaeological, paleontological, and historic resources by: Requiring a records review for development proposed in areas that are considered archaeologically or paleontologically sensitive; Determining the potential effects of development and construction on archeological and paleontological resources (as required by CEQA); Requiring preconstruction surveys and monitoring during any ground disturbance for all development in areas of historical and archaeological sensitivity; and Implementing appropriate measures to avoid the identified impacts, as conditions of project approval.

LTS

COS-1-34 include preparation of a written report documenting the find and describing steps taken to evaluate and include avoidance, preservation in place, excavation, documentation, and/or data recovery, and shall always If, prior to grading or construction activity, an area is determined to be sensitive for paleontological protect significant resources. resources, retain a qualified paleontologist to recommend appropriate actions. Appropriate action may

thus are incorporated here by reference. Additionally, policies summarized under Impact 3.11-1 serve to reduce this impact to a level that is less than significant and

# 3.12-1. Implementation of the proposed General Plan has the potential to adversely affect scenic views of peripheral agricultural lands, grasslands, and wetlands as seen from public viewing areas from inside Lemoore.

Require contiguous development within the Sphere of Influence (SOI) unless it can be demonstrated that land which is contiguous to urban development is unavailable or development is economically infeasible. STJ

Work with the County on a Memorandum of Understanding (MOU) in which the County will commit to: Retaining agriculture and open space areas around the City, consistent with the General Plan; and Notifying the City of development applications within the "secondary" SOI adjacent to the City's Planning Area for

	Proposed C	Proposed General Plan Policies that Reduce the Impact	Significance after Mitigation
		comment to avoid potential conflicts.	
	CD-I-2	Maintain views into the agricultural lands on the rural side of the roadways by not planting within the right-of-way and spacing trees farther apart.	
	CD-1-4	Maintain scenic vistas to the Coalinga Mountains, other natural features, and landmark buildings.	
	COS-I-1	Protect lands designated for Agricultural/Rural/Conservation uses with appropriate zoning consistent with the General Plan.	
3.12-2. Implementation of the proposed General Plan will alter the scale and visual character of the City	LU-I-11	Ensure that the scale, operation, location, and other characteristics of community facilities, including parks, schools, child care facilities, religious institutions, other public and quasi-public facilities, enhance the character and quality of neighborhoods.	LTS
die Gery.	LU-I-12	Require new residential development adjacent to established neighborhoods to provide a transition zone where the scale, architectural character, pedestrian circulation and vehicular access routes of both new and old neighborhoods are well integrated.	
	CD-1-14	Continue the City's utility undergrounding program to replace existing wooden utility poles and overhead lines with underground utility lines along major thoroughfares, and require undergrounding of utilities in all new development.	
	CD-I-34	Require design of buildings in neighborhood centers to be consistent with Lemoore's small town character. Style: Buildings must adopt a style that is non-intrusive in character, i.e. they must conform to the surrounding neighborhood and Lemoore's small town character. Inappropriate architecture will not be allowed. Color: Exterior color and materials must be compatible with those of existing buildings. Proposed external materials must be high quality and durable. Color schemes will be evaluated on a case by case basis during site plan approval. Height: The height of buildings must not differ by more than one story from adjacent buildings. The rhythm of window openings or architectural elements must be consistent across each block Landscare: At least 15 percent of the site must be planted with trees, shrubs, or groundcover.	

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

	Proposed General Plan Policies that Reduce the Impact	Reduce the Impact Significa	nce ofter
	Proposed General Flan Folicies mat Ne	Neduce are impact.  Ningoti	מט מינים
THE REAL PROPERTY OF THE PERSON NAMED AND THE PERSO	CD-I-42 Require landscaped by	CD-I-42 Require landscaped buffers and screening along the perimeter of industrial areas abutting residential areas,	:
	major streets and edge of town	top of town.	

- establishing the following standards in the subdivision ordinance: Maximum block length: 500 feet, except Ensure that new residential development enhances Lemoore's neighborhood character and connectivity by attributes. Loop-outs: Encourage use of loop-out streets rather than cul-de-sacs. than ten percent of the length of all streets in a subdivision map, where constrained by surrounding land designed with traffic calming features to control speed. Cul-de-sacs: Limit use of cul-de-sacs to no more alleys to form a continuous vehicular and pedestrian network. Local, internal streets should be narrow and pedestrian connection); Required connectivity: All new streets and alleys must connect to other streets and for blocks with single-family residential uses that may be up to 600 feet long (750 feet with a mid-block
- COS-I-32 Establish an inventory of historical buildings in Lemoore, including schools, churches, commercial buildings and public buildings, and residential buildings.
- COS-I-37 additions: The project design is compatible with Secretary of the Interior Standards for the Treatment of Establish an interim design review process for proposed demolitions and exterior alterations and additions necessary to allow a project that will have public benefits outweighing the public benefits of retaining the adjacent buildings and the character of Downtown Lemoore, or the proposed demolition or removal is approving or conditionally approving the proposed change will include: For proposed alterations and review authority, with their decisions subject to appeal to the City Council. Criteria to be considered in to non-residential buildings that are more than 75 years old. The Planning Commission would be the existing building. quality of the replacement building will be superior to the existing building and will be compatible with applicant has demonstrated that the existing use can not generate a reasonable rate of return; the existing Historic Properties and with the Downtown Revitalization Plan; and For proposed demolitions: The building constitutes a hazard to public safety and is economically infeasible to rehabilitate, the design

Additionally, the policy CD-I-11 listed under Impact 3.5-1 will help reduce the impact and is thus incorporated here by

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

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	i oposoca wa		Mitigation
		reference.	
3.12-3. Implementation of the proposed General Plan will create new sources of light or glare and affect the night sky.	CD-1-63	Establish Outdoor Lighting Standards in the Zoning Ordinance where: All outdoor lighting fixtures shall be designed, shielded, aimed, located and maintained to shield adjacent properties and to not produce glare; New street lighting shall be provided in accordance with the requirements of the California Energy Commission's Outdoor Lighting Standards and follow recommendations put forth by the Illuminating Engineering Society of North America (IESNA) Design of Roadway Lighting (RP-8); All outdoor lighting fixtures shall be energy-efficient. Parking lot light fixtures and light fixture on buildings shall be full cut-off fixtures and all permanently installed exterior lighting shall be controlled by either a photocell or an astronomical time switch; Lighting design in parking lots, commercial and industrial areas shall be coordinated with the landscape plan to ensure that vegetation growth will not interfere with the intended illumination; Light levels in all new development, parking lots, and street lights to not exceed industry and State standard for urban areas.	i. i.
	CD-I-64	Create and adopt a Dark Sky Ordinance to minimize glare, light trespass, excessive lighting and other forms of light pollution to preserve the enjoyment of the night sky and night environment.	
	CD-1-65	Do not allow continuous all night outdoor lighting in sport stadiums, construction sites, and rural areas unless they are required for security reasons.	
	CD-I-66	Educate the public about light trespass and light pollution and establish a voluntary program to encourage existing sources of light pollution to convert to non-polluting, energy efficient lighting systems.	•
3.12-4. Implementation of the proposed General Plan will change the existing visual quality of Lemoore's edges and entryways.	CD-1-1	Establish an open country character for new development facing the countryside along Marsh Drive, the Lemoore Canal, and portions north of West Glendale Avenue, Belle Haven Drive, Industry Way, Idaho Avenue, Jackson Avenue, and other areas generally illustrated by figures 3-1, 3-2, and 3-3 of the General Plan, to demarcate the urban edge. This will be accomplished by: Enforcing a 50 foot minimum setback requirement on new development along these roads; Creating a 30 foot wide landscaped buffer within the middle of trees and search of the sear	LTS

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

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Proposed General Plan Policies that Reduce the Impact	

impermeability, and using drought resistant indigenous trees where appropriate; Providing only minimal street lighting, at a rate that is 50 percent of comparable City standard; Prohibiting the use of solid walls along these edges (all fences must be visually permeable); and Ensuring the scale and character of development does not overwhelm the surroundings by stepping down building heights at the edges.

- 91:3 Work with the Lemoore Canal and Irrigation Company and other canal companies to retain open canals trail along the canal and restore the Lemoore Canal to its natural appearance, and study the possibility of providing a bicycle
- 9-13 Create entry gateways at the intersection of SR-198 and Houston Avenue, the SR-41 off ramp at Bush and additional local signage will be placed on Bush Street to direct vehicles to City attractions, such as piece of land where commercial uses are existing. Placing vehicle-oriented street lights with welcoming signs proposed off-ramp at SR-198 near Marsh Drive with distinctive features, as follows: Intersection of SR-198 Street, the intersection of SR-41 and Hanford Armona Road, along SR-41 south of Idaho Avenue, and the sign, consistent with Caltrans standards, will be placed in the right-of-way or on the Marsh Drive turn-out retail is proposed at the northeastern corner to take advantage of the area's high visibility. A welcoming it remains. Landscaping and signage will announce entry to the City. SR-198 near Marsh Drive: Regional development is proposed at this gateway. Improvements to the Lemoore Midget Raceway will be needed it welcoming sign will announce the entry into the City. SR-41 south of Idaho Avenue: Industrial heavy landscaping on chain link fence area. Densely landscaped trees both sides of the street and a developments are planned around this intersection. Existing gas line station should be camouflaged with Downtown and City Hall. Intersection of SR-41 and Hanford Armona Road: Low density residential signage at least half a mile in advance of the off-ramp (indicating major resources to be found at this exit) this area will attract traffic from SR-41 to the City. The exit will be clearly marked with regular highway where the roads are marked with fewer trees. SR-41 off ramp at Bush Street: Regional retail planned around closely on each side of the street will provide another transitional element as one enters from the rural area attached on each side will accentuate both the City gateway and the development nearby. Lush trees planted and Houston Avenue. This entry is located at an area where the two roads merged to form a triangulated A landscaped greenway at the northern edge of SR-198 will create an attractive buffer as well as

### Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

	•		Proposed C
Marsh Drive.	accommodate storm detention needs. Rural edge treatment will also be provided at the western sid		Proposed General Plan Policies that Reduce the Impact
	2,	Mitigation	Significance after

- 0-1-15 of truck loading and refuse collection areas; and Commercial or retail signs and logos. SR-198. Design guidelines should address the following: Building facades, roofing, and façade materials, and colors; Use of focal elements (such as articulated rooflines or towers) to serve as visual landmarks; Screening Establish design standards and architectural guidelines for non-residential development facing SR-41 and
- 9-1-16 Require "street friendly" designs in new and transitioning commercial developments along SR-41.
- 00-1-17 to local streets through improvements to off-ramps, through-streets, traffic signs and signals; improving the Creating a green buffer along parts of SR-198 and SR-41 adjoining residential land; Improving connections safety and aesthetics of fencing structures on bridges; Establishing aesthetic standards for the design and Work with Caltrans to identify needed improvements to its highway facilities. Improvements include: color of concrete highway dividers and walls; and Establishing standards for streetscape improvements including tree planting along highways, and adding landscaping, artwork, or stamped concrete for future
- 972 Design streetscape and landscape elements to enhance the sense of arrival from SR-198 towards Houston Avenue and East D Street
- Work with property owners, law enforcement officials, and the public in removing abandoned equipment, trailers, and other items that litter open space east of SR-41 along Idaho Avenue, Bush Street, and 19 ½ Ave.

significant and is included by reference. Policies LU-I-4, CD-I-2 in Impact 3.12-1, and CD-I-42 in Impact 3.12-2 will also help ensure this impact remains less than

### proposed General Plan will result 3.3-1. Development under the C-I-23 Provide incentives for City employees to commute by transit, car-pool or use alternative fuel technology vehicles.

In a substantial increase in total

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

	Proposed Ge	Proposed General Plan Policies that Reduce the Impact  A	Significance after Mitigation	after
vehicle miles traveled (VMT) as well as an increase in VMT per capita, potentially leading to an	C-I-24	Offer alternative work hours and telecommuting when appropriate to City employees to reduce VMT and trips to work.		
increase in the per capita generation of greenhouse gases.	C-I-25	Purchase hybrid gasoline-electric, bio-diesel fuel or electric vehicles for the City fleet.		
	C-I-26	Ensure that new development is designed to make public transit a viable choice for residents. Options include: Locate medium-high density development whenever feasible near streets served by public transit; and Link neighborhoods to bus stops by continuous sidewalks or pedestrian paths.		
	COS-I-48	Educate employees and department managers about sustainability with a focus on specific operational changes that can be made to reduce greenhouse gas emissions, such as fuel efficient driving and reducing energy use at work.		
3.13-2. Development under the proposed General Plan will result in a substantial increase in the total amount of electricity energy consumed by residential and non-	CD-1-58	Require new development to incorporate passive heating and natural lighting strategies to the extent feasible and practical. These strategies should include, but are not limited to, the following: Using building orientation, mass and form, including façade, roof, and choice of building materials, color, type of glazing, and insulation to minimize heat loss during winter months and heat gain during the summer months;	NSB	

consumed by residential and nonresidential users in Lemoore.

09-1-60

Incorporate green building standards into the Zoning Ordinance and building code to ensure a high level of energy efficiency in new development, retrofitting projects, and City facilities. These standards should

include, but are not limited to, the following: Require the use of Energy Star® appliances and equipment in

to a minimum; and Reducing heat-island effect of large concrete roofs and parking surfaces.

Designing building openings to regulate internal climate and maximize natural lighting, while keeping glare

ր **Հ**  per year of electricity to install photovoltaic energy systems.

new residential development to be pre-wired for optional photovoltaic energy systems and/or solar water

heating on south facing roofs; and Require all new projects that will use more than 40,000 kilowatt hours

new and substantial renovations of residential development, commercial development, and City facilities; Require all new development incorporate green building methods to qualify for the equivalent of LEED

Certified "Silver" rating or better (passive solar orientation must be a minimum component); Require all

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

	Proposed General Plan Policies that Reduce the Impact	
Mitigation	Significance	

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- CD-I-61 Adopt a Green Building Design Ordinance.
- 8-1-62 subdivision applications on sites greater than five acres to submit a construction waste management plan use of cost-effective design and construction strategies that reduce resource and environmental impacts, for City approval; Promoting the purchase of locally or regionally available materials; and Promoting the Promoting use of products that are durable and allow efficient end-of-life disposal (recyclable); Requiring Facilitate environmentally sensitive construction practices by: Restricting use of chlorofluorocarbons (CFCs), hydrochlorofluorocarbons (HCFCs) and halons in mechanical equipment and building materials:
- CD-1-63 fixtures shall be energy-efficient. Parking lot light fixtures and light fixtures on buildings shall be full cut-of Engineering Society of North America (IESNA) Design of Roadway Lighting (RP-8); All outdoor lighting New street lighting shall be provided in accordance with the requirements of the California Energy Establish Outdoor Lighting Standards in the Zoning Ordinance where: All outdoor lighting fixtures shall be state standards; and Lighting along the urban-rural edge should be designed to provide one-half the light illumination; Light levels in all new development, parking lots, and street lights to not exceed industry and coordinated with the landscape plan to ensure that regetation growth will not interfere with the intended astronomical time switch; Lighting design in parking lots, commercial and industrial areas shall be Commission's Outdoor Lighting Standards and follow recommendations put forth by the Illuminating designed, shielded, aimed, located and maintained to shield adjacent properties and to not produce glare: standard for urban areas. fixtures and all permanently installed exterior lighting shall be controlled by either a photocell or an
- CD-I-65 Do not allow continuous all night outdoor lighting in sports stadiums, construction sites, and rural areas unless they are required for security reasons.
- COS-I-38 Compile and update an inventory of greenhouse gas emissions from City operations and track related solid waste, energy, economic, and environmental data
- Support State efforts to reduce greenhouse gases and emissions through local action that will reduce motor vehicle use, support alternative forms of transportation, require energy conservation in new construction,

## Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

Proposed General Plan Policies that Reduce the Impact

Significance after Mitigation

and energy management in public buildings.

COS-I-40 Prepare a Greenhouse Gas Emissions Reduction Plan, focusing on feasible actions the City can take to and the City's internal government operations, and feasible GHG emission reduction measures whose source" or "source" as defined in section 38505 of the California Global Warming Solutions Act ("AB 32") what is a source of GHG emissions, the City may rely on the définition of "greenhouse gas emissions greenhouse gases (GHGs) that currently exist in the City and sources that existed in 1990. In determining attributable to the City's discretionary land use decisions and the City's internal government operations. purpose shall be to meet this reduction target by regulating those sources of GHG emissions reasonably emissions reasonably attributable to the City's discretionary land use decisions under the 2030 General Plan information from to state and regional air quality boards, supplemented by information obtained by the or its governing regulations. The inventory may include estimates of emissions drawing on available include but will not be limited to: An inventory of all known, or reasonably discoverable, sources of minimize the adverse impacts of Plan implementation on climate change and air quality. The Plan wil from state and regional air quality boards and agencies. A target for the reduction of those sources of future estimates, supported by substantial evidence, of future emissions from planned land use and information GHGs emitted by the City's internal government operations. The projected inventories will include due to the City's discretionary land use decisions pursuant to the 2030 General Plan Update, as well as new City. A projected inventory of the new GHGs that can reasonably be expected to be emitted in the year 2030

Policy COS-1-48 listed under Impact 3.13-1 also helps to reduce this impact. No other mitigation is feasible

Source: Dyett & Bhatia, 2007.

### EXHIBIT B

FINDINGS ON PROJECT ALTERNATIVES

### EXHIBIT B

### FINDINGS ON PROJECT ALTERNATIVES

The EIR identified and analyzed a reasonable range of feasible alternatives to the proposed 2030 General Plan. The alternatives described in the EIR were:

- No Project Alternative
- Alternative A: West Hills Focus
- Alternative B: Corridor and Core Focus

The Council of the City finds that the alternatives identified above to the proposed project would not provide for a reduction of or avoidance of the significant unavoidable environmental effects associated with the adoption of the General Plan and would not achieve the long term planning and growth goals of the City.

The No Project Available is not environmentally superior to the proposed project.

Based on the important environmental goals of reducing agricultural land conversion, protecting habitats and wildlife corridors, and reducing the production of greenhouse gases and criteria and toxic air pollutants, Alternative B appears to be the environmentally superior alternative. Though the No Project Alternative converts slightly fewer acres of agricultural land for urban uses and thus protects biological and water resources better than the other buildout alternatives, the existing General Plan does not provide adequate policies to control the quantity, type or direction of future growth, nor does it explicitly promote "no net loss" of habitat. Furthermore, Alternative B proposes fewer jobs and less population growth than all but the No Project Alternative, both of which should result in fewer vehicles, fewer vehicle miles traveled, lower electricity use and resulting greenhouse gas emissions than Alternative A or the proposed General Plan.

However, there are tradeoffs associated with Alternative B. The development potential of Alternative B does not meet the City's long term economic development needs, a factor that inspired the creation of the Business, Technology, and Industrial Reserve Area for the proposed General Plan. Fewer jobs in Alternative B also means that this alternative is less likely to achieve efficiencies that are possible with a better jobs-housing balance, wherein people may live and work in the same city, or closer to their jobs.

### EXHIBIT C

STATEMENTS OF FINDINGS OF UNAVOIDABLE
SIGNIFICANT ADVERSE ENVIRONMENTAL EFFECTS
WHICH ARE MITIGATED TO THE EXTENT FEASIBLE
THROUGH THE MEASURES IDENTIFIED BELOW BUT
CANNOT BE ELIMINATED OR SUBSTANTIALLY LESSENED
TO A LEVEL OF LESS THAN SIGNIFICANT

## Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

Proposed General Plan Policies that Reduce the Impact

Significance after Mitigation

	Q	α	भे			,		Ω	TU		3.1-1. Buildout of the proposed LU General Plan would convert
	COS-I-2	COS-I-1	PU-I-10					0-1-1	LU-1-3	LU-I-2	TO-1-I
Personal and the second	Identify a secure funding mechanism for the purchase of conservation easements to support farmland preservation and a green space buffer on County land surrounding the Lemoore Planning Area, with particular emphasis on land east of the City.	Protect lands designated for Agricultural/Rural/Conservation uses with appropriate zoning consistent with the General Plan.	Require that developers of agricultural land to be annexed to the City offer the water rights associated with this land to the City.	along these edges (all fences must be visually permeable); and Ensuring the scale and character of development does not overwhelm the surroundings by stepping down building heights at the edges.	impermeability, and using drought resistant indigenous trees where appropriate; Providing only minimal street lighting, at a rate that is 50 percent of comparable City standard; Prohibiting the use of solid walls	requirement on new development along these roads; Creating a 30 foot wide landscaped buffer within the public right-of-way or landscape easement; Planting multiple layers of trees closely for visual	to demarcate the urban edge. This will be accomplished by: Enforcing a 50 foot minimum setback	Establish an open country character for new development facing the countryside along Marsh Drive, the Lemoore Canal, and portions north of West Glendale Avenue, Belle Haven Drive, Industry Way, Idaho Avenue, Jackson Avenue and other areas generally illustrated in figures 3-1, 3-2 and 3-3 of the General Plan,	Require contiguous development within the Sphere of Influence (SOI) unless it can be demonstrated that land which is contiguous to urban development is unavailable or development is economically infeasible.	Seek LAFCO approval of a Sphere of Influence (SOI) line that is co-terminus with the General Plan Urban Growth Boundary.	Establish an Urban Growth Boundary (UGB) in the General Plan Land Use Diagram that limits the extent of urban development up to the year 2030.
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Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

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	Proposed G	Proposed General Plan Policies that Reduce the Impoct	Significance after Mitigation
	COS-I-3	Work with the County to evaluate the need for and feasibility of creating a County Farmland Trust or Open Space District to negotiate open space transactions, hold easements, pursue local open space and farmland preservation policies.	
	COS-1-7	Work with Kings County to preserve State-designated Prime Farmland, retain agricultural use designations and encourage the continuation of farming activities outside the City.	
	COS-I-8	Require developers to prepare detailed stormwater run-off analyses and mitigation plans for any new development adjoining existing Prime Farmland, grassland or wetlands.	
	COS-I-9	Require developers to inform subsequent buyers of potential continued agricultural production and the lawful use of agricultural chemicals, including pesticides and fertilizers adjacent to the new development	•
3.1-2. Buildout of the proposed General Plan will conflict with	The policie serve to res	The policies LU-I-1, LU-I-2, LU-I-3, COS-I-1, COS-I-2, COS-I-3, COS-I-7, COS-I-8 and COS-I-9 listed under Impact 3.1-1 SU serve to reduce this impact and thus are incorporated here by reference.	SU .
existing zoning for agricultural use and result in the conversion of lands under Williamson Act contract to urban use.			

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

Proposed General Plan Policies that Reduce the Impact

Significance after Mitigation

significant and thus are incorporated here by reference.

3						3.5-1. Implementation of the proposed General Plan could result in substantial adverse effects on candidate, sensitive, or special status species, their habitats, or other sensitive vegetation communities.			
CD-1-11	COS-I-25	COS-1-15	COS-I-14	COS-I-13	COS-I-12	COS-I-11	COS-I-10		
Preserve and protect heritage trees: Adopt a Tree Protection Ordinance; Require developers to preserve protected trees and submit an inventory and a site plan showing the locations of all trees miner to any grading, demolition, or site work. Cutting of protected trees will require a permit and will only be allowed if trees are diseased, dying, or pose a danger to human activity; and Require developers replace a similar tree of like size and species within 50 feet of its original location if a protected tree is removed during	Reduce the use of pesticides, insecticides, herbicides, or other toxic chemical substances by households and farmers by providing education and incentives for Integrated Pest Management (IPM) practices.	Prohibit the use of invasive plant species, such as Pampas grass, adjacent to wetlands and other sensitive habitat, where such landscaping could adversely impact wildlife habitat.	Consult with trustee agencies (California Department of Fish and Game, U.S. Fish and Wildlife Service, the U.S. Army Corps of Engineers, Environmental Protection Agency, and Regional Water Quality Control Board) during environmental review of individual projects and Plan implementation activities when special status species, sensitive natural communities, or wetlands or vernal pools may be adversely affected.	Establish a "no net loss" standard for sensitive habitat acreage, including wetlands and vernal pools potentially affected by development.	Require drainage basin buffers, maintenance of adequate water supply and reduced disturbance of the water table and wetlands systems.	Periodically monitor existing and future stormwater drainage system pre-treatment and flows entering wetlands.	Require protection of sensitive habitat areas and "special status" species in new development in the following order: 1) avoidance; 2) onsite mitigation, and 3) offsite mitigation. Require assessments of biological resources prior to approval of any development within 300 feet of any creeks, sensitive habitat areas, or areas of potential sensitive status species.		

construction.

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

Proposed General Plan Policies that Reduce the Impact

Significance after Mitigation

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proposed General Plan would	<u>ب</u> 4.	Develop a multi-modal transit system map integrating preycie, public transportation, pedestrian and vehicle of linkages within the City to ensure circulation gaps are being met. Safe Routes to School and any necessary	
result in a cumulatively considerable net increase of	,	related improvements will also be shown on this map, and costs and priorities indicated based on need.	
criteria pollutants. Future growth in accordance with the Plan and traffic associated with the Plan	C-I-24	Offer alternative work hours and telecommuting when appropriate to City employees to reduce VMT and trips to work.	
would generate emissions exceeding the annual SIVAPCD	C-I-25	Purchase hybrid gasoline-electric, bio-diesel fuel, or electric vehicles for the City fleet.	
thresholds for NO _X and ROG.	C-I-26	Ensure that new development is designed to make public transit a viable choice for residents. Options include: Locate medium-high density development whenever feasible near streets served by public transit; and Link neighborhoods to bus stops by continuous sidewalks or pedestrian paths.	
	008-1-41	Amend the Zoning Ordinance to prohibit locating new "sensitive receptor" uses—hospitals, residential care facilities and child care facilities—within: 500 feet of a freeway, urban roads carrying 100,000 vehicles per day, or rural roads carrying 50,000 vehicles per day. 1,000 feet of a distribution center (that accommodates more than 100 trucks a day, more than 40 trucks with operating transport refrigeration units (TRUs) a day, or where TRU operation exceeds 300 hours per week). 300 feet of any dry cleaning operation that uses toxic chemicals. For operations with two or more machines, provide 500 feet. For operations with three or more machines, consult your local air district. 300 feet of a large gas station (defined as a facility with a throughput of 3.6 million gallons or more per year).	
	COS-I-42	Conforming to the SJVAPCD Fugitive Dust Rule, require developers to use best management practices (BMPs) to reduce particulate emission as a condition of approval for subdivision maps, site plans and all	

# 5 2 0

construction-related equipment shall be maintained in good working order to reduce exhaust.

motorized vehicles shall have their tires watered before exiting a construction site; The area disturbed by demolition, clearing, grading, earth-moving, or excavation shall be minimized at all times; and All

dust emissions shall be controlled by regular watering, paving of construction roads, or other dustpreventive measures; All materials excavated or graded shall be either sufficiently watered or covered by
canvas or plastic sheeting to prevent excessive amounts of dust; All materials transported off-site shall be
either sufficiently watered or covered by canvas or plastic sheeting to prevent excessive amounts of dust; All

grading permits. BMPs include: During cleaning, grading, earth-moving or excavation operations, fugitive

Table ES-6 Summary of Impacts and Proposed General Policies that Reduce the Impact

"normally unacceptable" SN-I-46 Require generated by aircraft originating from or destined for the Lemoore Naval Air Station facility.		3.8-3 Implementation of the LU-I-4 Work wi proposed General Pian would Retaining expose about 7,000 persons to the City noise in excess of 65 dB commen ("conditionally acceptable" or	3.7.2. Implementation of the The policies summarize proposed General Plan would additional mitigation is expose sensitive receptors to substantial pollutant concentrations.	Additionally, the policy this impact and thus are	COS-I-48 Educate employees changes that can be energy use at work	COS-I-47 Coordina causing a	COS-I-46 Establish the linkag	COS-1-45 Utilize m efficient p	COS-I-44 Seek gran with EPA	cerified v permitted	Proposed General Plan P
	Require a noise study and mitigation measures for all new projects that have aircraft noise exposure greater than "normally acceptable" levels. Mitigation measures may include noise insulation, noise disclosure, buyer beware programs, or avigation easements, as shown in Table 3.8-6.	Work with the County on a Memorandum of Understanding (MOU) in which the County will commit to: Retaining agriculture and open space areas around the City, consistent with the General Plan; and Notifying the City of development applications within the "secondary" SOI adjacent to the City's Planning Area for comment to avoid potential conflicts.	The policies summarized under Impact 3.7-1 serve to reduce this impact and thus are incorporated here by reference. No additional mitigation is deemed feasible, thus Impact 3.7-2 remains significant and unavoidable.	Additionally, the policy C-I-3 listed under Impact 3.2-4 and policies C-I-24, C-I-28 listed under Impact 3.2-5 serve to reduce this impact and thus are incorporated here by reference.	s and department managers about sustainability wit e made to reduce greenhouse gas emissions, such as i	Coordinate air quality planning efforts and CEQA review of discretionary projects with potential for causing adverse air quality impacts with other local, regional and State agencies.	Establish a Clean Air Awards Program to acknowledge outstanding effort and to educate the public about the linkages between land use, transportation and air quality.	Utilize more plants and trees in public area landscaping, focusing on those that are documented as more efficient pollutant absorbers.	Seek grant funding for a "change-out" program to help homeowners replace old word-burning fireplaces with BPA-certified wood-burning appliances.	certified wood heaters or approved wood-burning appliances in new developments or replacements. Lists permitted and prohibited fuels; and Describes a "No Burn" policy on days when the air quality is poor.	Proposed General Plan Palicies that Reduce the Impact
		S	SU				,				Significance after Mitigation

SN-I-48 Minimize noise impacts of NAS Lemoore flight operations on noise-sensitive development.

The policies summarized under Impact 3.8-1 serve to reduce this impact and thus are incorporated here by reference.

### EXHIBIT D

STATEMENT OF OVERRIDING CONSIDERATIONS

### EXHIBIT D

### STATEMENT OF OVERRIDING CONSIDERATIONS

The Environmental Impact Report (EIR) identifies that the proposed Project may result in the generation of significant adverse effects as specified in Exhibit "C" to the Resolution. The Council hereby determines that the benefits of the Project analyzed and discussed in the EIR outweigh the unavoidable effects and risks.

The four (4) significant and unavoidable impacts on the projects as determined by the City are listed below. Also, findings and facts supporting the findings in connection therewith are listed. The following areas were discussed in the Final EIR:

### A. Land Use and Agriculture

1. Impact 3.1-1:

Buildout of the proposed 2030 General Plan will result in the conversion of substantial amounts of Farmland of Statewide Importance to non-agricultural use.

Mitigation Measures:

Policy LU-I-1: Establish as

Establish an Urban Growth Boundary (UGB) in the General Plan Land Use Diagram that limits the extent of urban development up to the year 2030.

Policy LU-I-2:

Seek LAFCO approval of a Sphere of Influence (SOI) line that is co-terminus with the General Plan Urban Growth Boundary.

Policy LU-I-3:

Require contiguous development within the Sphere of Influence (SOI) unless it can be demonstrated that land which is contiguous to urban development is unavailable or development is economically infeasible.

Policy CD-I-1:

Establish an open country character for new development facing the countryside along Marsh Drive, the Lemoore Canal, and portions north of West Glendale Avenue, Belle Haven Drive, Industry Way, Idaho Avenue, Jackson Avenue and other areas generally illustrated in figures 3-1, 3-2 and 3-3 of the General Plan, to demarcate the urban edge. This will be accomplished by: Enforcing a 50

foot minimum setback requirement on new development along these roads; Creating a 30 foot wide landscaped buffer within the public right-of-way or landscape easement; Planting multiple layers of trees closely for visual impermeability, and using drought resistant indigenous trees where appropriate; Providing only minimal street lighting, at a rate that is 50 percent of comparable City standard; Prohibiting the use of solid walls along the edges (all fences must be visually permeable); and Ensuring the scale and character of development does not overwhelm the surroundings by stepping down building heights at the edges.

Policy PU-I-10:

Require that developers of agricultural land to be annexed to the City offer the water rights associated with this land to the City.

Policy COS-I-1:

Protect lands designated for Agricultural/Rural/Conservation uses with appropriate zoning consistent with the General Plan.

Policy COS-I-2:

Identify a secure funding mechanism for the purchase of conservation easements to support farmland preservation and a green space buffer on County land surrounding the Lemoore Planning Area, with particular emphasis on land east of the City.

Policy COS-I-3:

Work with the County to elevate the need for and feasibility of creating a County Farmland Trust or Open Space District to negotiate open space transactions, hold easements, pursue local open space and farmland preservation policies.

Policy COS-I-7:

Work with Kings County to preserve State-designated Prime Farmland, retain agricultural use designations and

encourage the continuation of farming activities outside the City.

Policy COS-I-8: Require developments to prepare detailed stormwater run-off analyses and

mitigation plans for any new

development adjoining existing Prime Farmland, grassland or wetlands.

Policy COS-I-9:

Require developers to inform subsequent buyers of potential continued agricultural production and the lawful use of agricultural chemicals, including pesticides and fertilizers adjacent to the new development site.

Findings:

Based upon the EIR and the entire record before the City, the City finds that there are no feasible mitigation measures identified which will reduce or avoid the conversion of these farmlands to urban uses.

2. Impact 3.1-2:

Buildout of the 2030 General Plan will conflict with existing zoning for agricultural use and result in the conversion of lands under Williamson Act contract to urban use.

Mitigation Measures:

The policies LU-I-1, LU-I-2, LU-I-3, COS-I-1, COS-I-2, COS-I-3, COS-I-7, COS-I-8 and COS-I-9 listed under Impact 3.1-1 serve to reduce this impact and thus are incorporated here by reference, but these will not reduce the impact to a level less than significant.

Findings:

Based upon the EIR and the entire record before the City, the City finds that there are no feasible mitigation measures identified which will reduce or avoid the conversion of some Williamson Act contracted lands to urban uses.

### **Biological Resources**

1. Impact 3.5-1:

Implementation of the proposed 2030 General Plan could result in substantial adverse effects on candidate, sensitive, or special status species, their habitats, or other sensitive vegetation communities.

Mitigation Measures:

Policy COS-I-10:

Require protection of sensitive habitat areas and "special status" species in

new development in the following order: 1) avoidance; 2) onsite mitigation, and 3) offsite mitigation. Require assessments of biological resources prior to approval of any development within 300 feet of any creeks, sensitive habitat areas, or areas of potential sensitive status species.

Policy COS-I-11:

Periodically monitor existing and future stormwater drainage system pre-treatment and flows entering wetlands.

Policy COS-I-12:

Require drainage basin buffers, maintenance of adequate water supply and reduced disturbance of the water table and wetlands systems.

Policy COS-I-13:

Establish a "no net loss" standard for sensitive habitat acreage, including wetlands and vernal pools potentially affected by development.

Policy COS-I-14:

Consult with trustee agencies (California Department of Fish and Game, U.S. Fish and Wildlife Service, the U.S. Army Corps of Engineers, Environmental Protection Agency, and Regional Water Quality Control Board) during environmental review of individual projects and Plan implementation activities when special status species, sensitive natural communities, or wetlands or vernal pools may be adversely affected.

Policy COS-I-15:

Prohibit the use of invasive plant species, such as Pampas grass, adjacent to wetlands and other sensitive habitat, where such landscaping could adversely impact wildlife habitat.

Policy COS-I-25:

Reduce the use of pesticides, insecticides, herbicides, or other toxic chemical substances by households and farmers by providing education and incentives for Integrated Pest Management (IPM) practices.

Policy CD-I-11:

Preserve and protect heritage trees: Adopt a Tree Protection Ordinance; Require developers to preserve protected trees and submit an inventory and a site plan showing the locations of all trees prior to any grading, demolition, or site work. Cutting of protected trees will require a permit and will only be allowed if trees are diseased, dying, or pose a danger to human activity; and Require developers replace a similar tree of like size and species within 50 feet of its original location if a protected tree is removed during construction.

Findings:

There are no feasible mitigation measures which have been identified that would reduce the impacts to a level that is less than significant. However, there are numerous policies in the proposed 2030 General Plan that would mitigate the impact, as listed above.

# C. Air Quality

1. Impact 3.7-1:

Implementation of the 2030 General Plan will result in a cumulatively considerable net increase of criteria pollutants. Future growth in accordance with the Plan and traffic associated with the Plan would generate emissions exceeding the annual SJVAPCD thresholds for NO_x and ROG.

Mitigation Measures:

Policy C-I-4:

Develop a multi-modal transit system map integrating bicycle, public transportation, pedestrian and vehicle linkages within the City to ensure circulation gaps are being met. Safe Routes to School and any necessary related improvements will also be shown on this map, and costs and

priorities indicated based on need.

Policy C-I-24:

Offer alternative work hours and telecommuting when appropriate to City employees to reduce VMT and trips to work.

Policy C-I-25:

Purchase hybrid gasoline-electric, bio-diesel fuel, or electric vehicles for the City fleet.

Policy C-I-26:

Ensure that new development is designed to make public transit a viable choice for residents. Options include: Locate medium-high density development whenever feasible near streets served by public transit; and Link neighborhoods to bus stops by continuous sidewalks or pedestrian paths.

Policy COS-I-41:

Amend the Zoning Ordinance to prohibit locating new "sensitive receptor" uses - hospitals, residential care facilities and child care facilities - within: 500 feet of a freeway, urban roads carrying 100,000 vehicles per day, or rural roads carrying 50,000 vehicles per day. 1,000 feet of a distribution center (that accommodates more than 100 trucks a day, more than 40 trucks with operating transport refrigeration units (TRUs) a day, or where TRU operation exceeds 300 hours per week). 300 feet of any dry cleaning operation that uses toxic chemicals. For operations with two or more machines, provide 500 feet. For operations with three or more machines, consult your local air district. 300 feet of a large gas station (defined as a facility with a throughput of 3.6 million gallons or more per year).

Policy COS-I-42:

Conforming to the SJVAPCD Fugitive Dust Rule, require developers to use best management practices (BMPs) to reduce particulate emission as a condition of approval for subdivision maps, site plans and all grading permits. BMPs include: During clearing, grading, earth-moving or excavation roads, or other dust-preventive measures; All materials excavated or graded shall be either sufficiently watered or covered by canvas or plastic sheeting to prevent excessive amounts of dust; All materials transported off-site shall be either sufficiently watered or covered by canvas or plastic sheeting to prevent excessive amounts of dust; All motorized vehicles shall have their tires watered before exiting a construction site; The area disturbed by demolition, clearing, grading, earth-moving, or excavation shall be minimized at all times; and All construction-related equipment shall be maintained in good working order to reduce exhaust.

Policy COS-I-43:

Enact a wood-burning ordinance complaint with District Rule 4901 that: Regulates the installation of EPA-certified wood heaters or approved wood-burning appliances in new developments or replacements; Lists permitted and prohibited fuels; and Describes a "No Burn" policy on days when the air quality is poor.

Policy COS-I-44:

Seek grant funding for a "change-out" program to help homeowners replace old wood-burning fireplaces with EPA-certified wood-burning appliances.

Policy COS-I-45:

Utilize more plants and trees in public area landscaping, focusing on those that are documented as more efficient

pollutant absorbers.

Policy COS-I-46:

Establish a Clean Air Awards

Program to acknowledge outstanding effort and to educate the public about the linkages between land use, transportation and air quality.

Policy COS-I-47:

Coordinate air quality planning

efforts and CEQA review of discretionary projects with potential for causing adverse air quality impacts with other local, regional and

State agencies.

Policy COS-I-48:

Educate employees and department

managers about sustainability with a focus on specific operational changes

that can be made to reduce

greenhouse gas emissions, such as fuel efficient driving and reducing

energy use at work.

Additionally, the policy C-I-3 listed under Impact 3.2-4 and policies C-I-24, C-I-28 listed under Impact 3.2-5 serve to reduce this impact and thus are incorporated here by reference.

Findings:

There are no feasible mitigation measures which have been identified that would reduce the impacts to a level that is less than significant. However, there are numerous policies in the proposed 2030 General Plan that would mitigate the impact, as listed above.

2. Impact 3.7-2:

Implementation of the 2030 General Plan would expose sensitive receptors to substantial pollutant concentrations.

Mitigation Measures:

The policies summarized under Impact 3.7-1 serve to reduce this impact and thus are incorporated here by reference. No additional mitigation is deemed feasible, thus Impact 3.7-2 remains significant and unavoidable.

Findings:

Based upon the EIR and the entire record before this City, the City finds that there are no feasible mitigation measures that have been identified that would reduce the impacts of the listed emissions to a level that is less than significant.

#### D. Noise

1. Impact 3.8-3:

Implementation of the 2030 General Plan would expose about 7, 000 persons to noise in excess of 65 dB ("conditionally acceptable" or "normally unacceptable") generated by aircraft originating from or destined for the Lemoore Naval Air Station facility.

Mitigation Measures:

Policy LU-I-4:

Work with the County in a Memorandum of Understanding (MOU) in which the County will commut to: Retaining agriculture and open space areas around the City, consistent with the General Plan; and Notifying the City of development applications within the "secondary" SOI adjacent to the City's Planning Area for comment to avoid potential conflicts.

Policy SN-I-46:

Require a noise study and mitigation measures for all new projects that have aircraft noise exposure greater than "normally acceptable" levels. Mitigation measures may include noise insulation, noise disclosure, buyer beware programs, or aviation easements, as shown in Table 3.8-6.

Policy SN-I-47:

Coordinate with NAS Lemoore to incorporate their Air Installation Compatible Use Zone (AICUZ) study into future updates to the City Zoning Ordinance and General Plan to the extent consistent with the City's compatibility standards and noise level reduction requirements.

Policy SN-I-48:

Minimize noise impacts of NAS Lemoore flight operations on noisesensitive development.

The policies summarized under Impact 3.8-1 serve to reduce this impact and thus are incorporated here by reference.

Findings:

There are no feasible mitigation measures which have been identified that would reduce the noise impacts to less than significant. However, there are numerous mitigation measures that will significantly reduce the impacts.

# **Statement of Overriding Considerations**

CEQA requires a public agency to balance the benefits of a proposed project against its unavoidable environmental risks in determining whether to approve the project. CEQA requires the City Council to state in writing specific reasons for approving a project in a "statement of overriding considerations" if the EIR identifies significant impacts of the Project that cannot feasibility be mitigated to below a level of significance. Pursuant to California Public Resources Code section 21081 and CEQA Guidelines section 15093, the City Council adopts and makes the following Statement of Overriding Considerations regarding the remaining significant and unavoidable impacts of the Project, as discussed above, and the anticipated benefits of the Project.

The City finds and determines that the majority of the potentially significant impacts of the Project will be reduced to less-than-significant levels by the mitigation measures recommended in the EIR. However, as set forth above, the City's approval of the Project as proposed will result in significant adverse environmental noise, loss of farmland, increase air pollutants, and will impact sensitive animal and plant species that cannot be avoided even with the incorporation of all feasible mitigation measures into the Project, and there are no feasible Project alternatives which would mitigate to a level of significance or avoid the significant environmental impacts.

In light of the environmental, social, economic, and other considerations set forth below related to this Project, the City chooses to approve the Project, because in its view, the economic, social, and other benefits resulting from the Project will render the significant effects acceptable.

The following statement identifies the reasons why, in the City's judgment, the benefits of the Project outweigh the significant and unavoidable effects. The substantial evidence supporting the enumerated benefits of the Project can be found in the preceding Findings, which are herein incorporated by reference, in the Project itself, and in the record of proceedings. Each of the overriding considerations set forth below constitutes a separate and independent ground for finding that the benefits of the Project outweigh its significant adverse environmental effects and is an overriding consideration warranting approval.

- 1. The Project allows the City to plan for growth in an orderly manner to meet future land needs based on projected population and job growth.
- The Project allows the City to meet the City's job/housing balance objective, the need for additional housing in the community, and State Law requirements for Lemoore's allocation of regional housing needs.

- 3. The Project promotes economic development of the community, maintains and improves the quality of life in the community, preserves and enhances environmental resources, and conserves the natural and built environment.
- 4. The Project integrates economic development into the General Plan and underscores the City's goals for fiscal health, a strong regional center, a vibrant Downtown, and retail strength.
- 5. The Project protects and enhances community assets, including well planned communities with distinctive character, a strong sense of community, a diverse population, high quality building design, convenient shopping, broad choice in employment and entertainment, a family atmosphere with excellent recreational activities, and job opportunities close to where people live.
- 6. The Project provides for the positive direction for the future physical development of the City, such as supporting mixed use development, transit supporting land uses and economic revitalization or underutilized sites to create more economic vitality in these commercial corridors.
- 7. The Project serves a critical need to protect wetlands and other significant environmental resources.
- 8. The Project promotes a well-integrated and coordinated transit network and safe and convenient pedestrian and bicycle circulation.
- 9. The Project results in the preservation and enhancement of the "green belt" open area surrounding NAS Lemoore, and provides housing necessary for the continued and increased use by the personnel of NAS Lemoore.
- 10. The Project provides for the future construction of all needed public utilities and thus is also an important component of an effective disaster response and planning for the City.
- 11. The Project serves a critical need to allow the City to plan for the equitable distribution of community facilities and services to meet the needs of all segments of the population and provide services for special needs that increase and enhance the community's quality of life while avoiding over-concentration in any one area.

# **CERTIFICATE**

STATE OF CALIFORNIA	)	
COUNTY OF KINGS	)	SS
CITY OF LEMOORE	)	

I, NANCI C. O. LIMA, City Clerk of the City of Lemoore, do hereby certify the foregoing Resolution of the City Council of the City of Lemoore was duly passed and adopted at a Regular Meeting held on the  $6^{\rm th}$  day of May, 2008.

**DATED:** May 7, 2008

Nanci C. O. Lima

City Clerk

# **RESOLUTION NO. 2009-15**

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF LEMOORE
AMENDING RESOLUTION #2006-28 IN REGARDS TO CONDITION OF APPROVAL #5
FOR VESTING TENTATIVE SUBDIVISION MAP NO.2005-02/
PLANNED UNIT DEVELOPMENT NO.2005-01/ CONDITIONAL USE PERMIT NO.2005-01/
SITE PLAN REVIEW 2005-01 FOR COUNTY TRACT NO. 845 – VICTORY VILLAGE
BY TIM PALMQUIST OF TWA COMMUNITIES

At a Regular Meeting of the City Council of the City of Lemoore duly called and held on April 7, 2009, at 7:30 p.m. on said day, it was moved by Council Member _______, seconded by Council Member _______ and carried that the following Resolution be adopted:

WHEREAS, Tim Palmquist of TWA Communities submitted a March 23, 2009 revised request to be able to satisfy Condition #5 of City Council Resolution 2006-28 by either being allowed to use the individual plot plan review process with Planning Commission approval or defer the condition so that it would be satisfied prior to the issuance of any permits; and

WHEREAS, condition #5 states that "floor plans and an overall plot plan shall be submitted to the Community Development Department for Planning Commission to review at a public hearing for their approval and City Council's concurrence prior to a Final Map being recorded for any phase of development. The front yard setbacks should vary to encourage variety in the line of sight visible to the public, and such setbacks shall be delineated on the overall plot plan. Due to noise concerns as well as visual appearances, the rear yard setbacks for properties that back onto arterial and collector streets or the railroad shall have a 20' minimum rear yard setback and be delineated on the overall plot plan. Additionally, the maximum lot coverage and minimum percentage of landscape respectively shall be as follows; in low-medium density residential 40%/25%, in medium density residential 50%/20%, and in high density residential 60%/15% and reflected in the overall and individual plot plans."

WHEREAS, the Planning Commission held a duly noticed public hearing on March 23, 2009 on the request, took public testimony and unanimously recommended that the City Council modify condition #5 to allow deferral of the project's design review until after the Final Map is recorded but prior to grading permits being pulled; and

WHEREAS, modification to condition #5 requires that the original Resolution 2006-28 be amended and adopted by the City Council; and

**WHEREAS**, the City Council held a duly noticed public hearing on April 7, 2009, took testimony on the above item.

- NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Lemoore does hereby approve the Planning Commission's recommendation to modify condition #5 of City Council Resolution 2006-28 to read as follows and such approval's expiration shall run with the original project approval and any related extension (be it State imposed or locally processed extension):
- 5. Floor plans and an overall plot plan shall be submitted to the Planning Department for Planning Commission to review at a public hearing for their approval and City Council's concurrence prior to grading permits being pulled for any phase of development. The front yard setbacks should vary to encourage variety in the line of sight visible to the public, and such setbacks shall be delineated on the overall plot plan. Due to noise concerns as well as visual appearances, the rear yard setbacks for properties that back onto arterial and collector streets or the railroad shall have a 20' minimum rear yard setback and be delineated on the overall plot plan. Additionally, the maximum lot coverage and minimum percentage of landscape respectively shall be as follows; in low-medium density residential 40%/25%, in medium density residential 50%/20%, and in high density residential 60%/15% and reflected in the overall and individual plot plans.

Passed and adopted at a Regular Meeting of the City Council of the City of Lemoore held on April 7, 2009 by the following votes:

AYES: PLOURDE, SIEGEL, MURRAY NOES: RODARMEL, HORNSBY ABSTAINING: NONE

ABSENT: NONE

APPROVED:

Nanci C.O. Lima, CMC

City Clerk

# CERTIFICATE

STATE OF CALIF	FORNIA)
<b>COUNTY OF KIN</b>	GS ) ss
CITY OF LEMOO	RE )

I, NANCI C.O. LIMA, City Clerk of the City of Lemoore, do hereby certify the foregoing Resolution of the City Council of the City of Lemoore was duly passed and adopted at a Regular Meeting of the City Council held on April 7, 2009.

**DAT⊭D**: April 8, 2009

Nanci C.O. Lima, CMC

City Clerk

#### **RESOLUTION #2011-48**

# A RESOLUTION OF INTENT THE CITY COUNCIL OF THE CITY OF LEMOORE DECLARING ITS INTENTION TO ABANDON THE WESTERLY PORTION OF BUSH STREET WEST OF WEST HILLS COLLEGE AND THE MARSH DRIVE ALIGNMENT

At a Regular Meeting of the City Council of the City of Lemoore duly called and held on December 20, 2011, at 7:30 p.m. on said day, it was moved by Council Member Plourde, seconded by Council Member Murray and carried that the following Resolution be adopted:

WHEREAS, Pharris is the owner of approximately 70 acres of unimproved land (the "Victory Village subdivision") located north and west of West Hills College (the "College"), in the City of Lemoore, California (the "City") containing existing Assessor's Parcel Numbers 023-510-034 and 023-510-036 and designated low-density single family residential (SF).

WHEREAS, Pharris has previously prepared and processed Vesting Tentative Tract Map #845 known as "Victory Village" through the City of Lemoore, which secured entitlements for 279 homes which is valid through August 2013.

**WHEREAS**, Pharris subsequently prepared all necessary civil plans and documentation to process and record a Final Map (the "Final Map") over a 13.32 +/- acre portion of Victory Village to accommodate 51 SF homes and a neighborhood park directly north of the College, east of the Marsh Drive alignment.

**WHEREAS**, Pharris intends to retain the portion of Victory Village located in the Final Map area of 13.32 +/- acres and is not part of any abandonment.

WHEREAS, the Department of the Navy ("Navy") is sponsoring a Readiness Environmental Preparedness Initiative (REPI) program, designed to preserve lands adjacent to naval installations, including lands near the Lemoore Naval Air Station ("LNAS"), as permanent open space.

**WHEREAS**, as part of the REPI program, the LNAS desires a Restrictive Use Easement ("Conservation Easement") over portions of the Victory Village to restrict future development including the abandonment of Bush Street in the middle of the proposed Conservation Easement areas.

WHEREAS, Pharris is negotiating with the Navy to commit the portions of Victory Village not included in the Final Map Area, specifically, 52.88+/- acres directly west of the college (the "Dedication Parcel"), to the LNAS as part of the Navy's REPI program. This Dedication Parcel will include approximately 228 SF lots that were part of the originally approved Victory Village project (Lots #52 through #279 respectively) and needs to also include the adjacent portion of Bush Street west of the Marsh Drive alignment depicted in the attached Abandonment Exhibit.

**WHEREAS**, Bush Street was dedicated to the City of Lemoore via Parcel Map 2005-03 Book 18 Page 6 king County Book of Maps.

**WHEREAS,** if the Conservation Easement is executed, portions of Bush Street would no longer be required to ever be built west of West Hills College.

**WHEREAS**, the current General Plan designates the proposed abandonment area as an arterial street in the Circulation Element and the adjacent land uses proposed in the Conservation Easement as low-density residential.

WHEREAS, the City staff recommends vacating Bush Street right-of-way and unneeded adjacent public utility easements so long as the Conservation Easement is executed for the Dedication Parcel and a General Plan and Zoning Amendment are processed to remove the segment of Bush Street from the arterial street designation and the land use is changed to agriculture or another non-developable land use and zone designation.

**WHEREAS**, the City has had an initial discussion of the proposed Bush Street vacation at its Special meeting December 12, 2011 and preliminary finds that the proposed vacations should not be detrimental to the public, health, safety and welfare; and

WHEREAS, no public utility facilities or streets are located in the above listed abandonment areas allowing for the summary vacation of such right-of-way and public utility easement under California Streets and Highways Code Sections 8330-8336 and does not require noticed publication or postings; and

**WHEREAS**, the needed General Plan Amendment does require a public hearing before Planning Commission and City Council before City Council can consider final abandonment of the area shown on Exhibit A.

NOW THEREFORE, BE IT RESOLVED, the City Council of the City of Lemoore:

- 1. Initiates the abandonment process for the area shown on the attached "Abandonment Exhibit",
- 2. Directs the Planning Commission to determine General Plan conformity of the intent to vacate the westerly portion of Bush Street right-of-way and adjacent public utility easements west of the college and Marsh Drive alignment, shown in the attached "Abandonment Exhibit" in accordance with Government Code Section 65402 at their next regular meeting (anticipated to be January 9, 2012)
- 3. Will consider the Abandonment once they receive the Planning Commission's General Plan conformity determination after a General Plan Amendment is concurrently considered (anticipated to be February 7, 2012).

Passed and adopted at a Regular Meeting of the City Council of the City of Lemoore held on the 20th day of December, 2011, by the following vote:

AYES: Plourde, Murray, Gordon, Siegel, Rodarmel

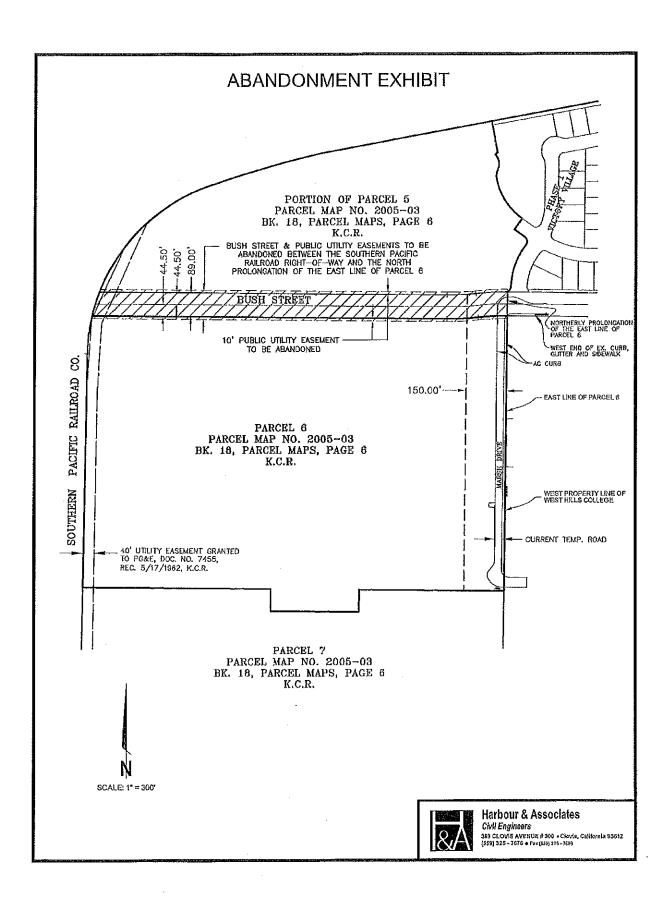
NOES: None

ABSTAINING: None ABSENT: None

APPROVED:

Willard Rodarmel, Mayor ATTEST:

Nanci O. Lima, MMC City Clerk



# CERTIFICATE

STATE OF CALIFORNIA	)	
COUNTY OF KINGS	Ú	SS
CITY OF LEMOORE	í	

I, Nanci O Lima, City Clerk of the City of Lemoore, do hereby certify the foregoing Resolution of the City Council of the City of Lemoore was duly passed and adopted at a Regular Meeting of the City Council held on December 20, 2011.

DATED: December 21, 2011

Vanci O. Lima, MMC

City Clerk

#### RESOLUTION #2013-30

# A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF LEMOORE APPROVING GENERAL PLAN AMENDMENT 2013-01

At a Regular Meeting of the City Council of the City of Lemoore duly called and held on December 3, 2013, at 7:30 p.m. on said day, it was moved by Councilmember member with seconded by Councilmember following Resolution be adopted:

WHEREAS, Pharris, LLC, represented by Tim Palmquist, has submitted applications to:
1) remove the collector street and bikeway status from Bush Street west of Marsh Drive; 2) to change the land use designation from Low Density Residential and Parks & Recreation to Community Facilities; and 3) to change the zoning map from Low Density Residential (RLD) and Parks & Recreation (PR) to Public Services & Community Facilities (CF); and

WHEREAS, an Initial Study was prepared in accordance with the California Environmental Quality Act (CEQA), which disclosed that no significant environmental impacts would result from the project; and

WHEREAS, the Planning Commission of the City of Lemoore, after duly published notice, held a public hearing before said Commission on October 28, 2013, and then recommended approval of the General Plan Amendment and Zone Change; and

WHEREAS, the City Council, after duly published notice, held a public hearing on December 3, 2013;

NOW THEREFORE, BE IT RESOLVED that the City Council of the City of Lemoore concurs that no significant environmental impacts would result from the identified project, and certify that the Negative Declaration was prepared consistent with the California Environmental Quality Act (CEQA.)

**BE IT FURTHER RESOLVED** that the City Council of the City of Lemoore approves General Plan Amendment 2013-01, based on the evidence presented and the following specific findings:

- 1. The general plan amendment is in the public interest and the General Plan, as amended, will remain internally consistent.
- 2. The zone change is consistent with the general plan goals, policies, and implementation programs.
- 3. Given the provisions of the intended conservation agreement between the applicant, the U.S. Navy, and the West Hills Community College District, the Community Facilities land use designation is the appropriate designation for the site.
- 4. Given the provisions of the intended conservation agreement between the applicant, the U.S. Navy, and the West Hills Community College District, the removal of required collector streets is the appropriate for the site

- 5. Given the provisions of the intended conservation agreement between the applicant, the U.S. Navy, and the West Hills Community College District, the Public Services & Community Facilities (CF) is the appropriate zone for the site.
- 6. Retaining the residential designation and zoning on the site would provide a false impression of what will now be the permanent land use disposition of the site, and would overly inflate the statistics of vacant residential land inventory in the city.

PASSED AND ADOPTED at a Regular Meeting of the City Council of the City of Lemoore held on the 3rd day of December, 2013, by the following vote:

AYES: GORDON, RODARMEL, Siegel, Wynne

NAYS:

ABSENT: New ABSTAINING:

APPROVED:

William M. Siegel, Jr., Mayor

ATTEST:

Brooke Austin

Chief Deputy City Clerk

# **CERTIFICATE**

STATE OF CALIFORNIA	)
COUNTY OF KINGS	) ss.
CITY OF LEMOORE	í

I, BROOKE AUSTIN, Chief Deputy City Clerk of the City of Lemoore do hereby certify the foregoing Resolution of the City Council of the City of Lemoore was duly passed and adopted at a Regular meeting of the City Council of the City of Lemoore called and held on 3rd day of December 2013.

Dated: December 4, 2013

BROOKE AUSTIN Chief Deputy City Clerk

# **RESOLUTION NO. 2018-18**

# A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF LEMOORE APPROVING GENERAL PLAN AMENDMENT NO. 2018-01 TO CHANGE THE PLANNED WIDTH OF BUSH STREET BETWEEN MARSH DRIVE AND SEMAS DRIVE FROM SIX LANES TO FOUR LANES

**WHEREAS**, the City of Lemoore's General Plan, adopted in 2008, identifies the planned width of Bush Street west of Highway 41 as between four and eight lanes; and

WHEREAS, an Environmental Impact Report (EIR) was certified for the 2008 General Plan, which identified environmental effects of future citywide development under the General Plan, including significant effects, mitigated effects, and insignificant effects: and

**WHEREAS**, the 2008 General Plan EIR concluded that the build-out of the 2008 General Plan would have a less than significant impact on the local transportation system; and

WHEREAS, Victory/Pharris Development, LLC controls three properties adjacent to the segment of Bush Street west of State Highway 41; and

WHEREAS, Victory/Pharris Development, LLC (agent: Tim Palmquist) has requested that the Lemoore General Plan be amended to change the planned width of Bush Street west of State Highway 41 from the six lanes to four lanes; and

WHEREAS, Table 4.3 in the 2008 General Plan indicates that Bush Street west of Semas Drive, when it was analyzed in 2008, will operate at Level of Service (LOS) C or better with four lanes at the planned build-out, and that Bush Street between Semas Drive and State Highway 41 will operate at LOS C or better with six lanes at the planned build-out; and

WHEREAS, since 2008, land that was identified for residential development in the General Plan has been redesignated as future wetlands, thereby reducing the future traffic demand on Bush Street west of Semas Drive to a level less than what was analyzed in 2008; and

WHEREAS, no land use designation changes are proposed; and

**WHEREAS**, the Lemoore Planning Commission held a duly noticed public hearing at its April 9, 2018, meeting and recommended approval of the general plan amendment; and

**WHEREAS**, the Lemoore City Council held a duly noticed public hearing at its May 1, 2018 meeting.

**NOW THEREFORE, BE IT RESOLVED** that the City Council of the City of Lemoore makes the following findings regarding California Environmental Quality Act (CEQA) compliance:

1. An environmental impact report (EIR) was prepared and certified when the General Plan was adopted in 2008.

- 2. Pursuant to Section 15183 of the CEQA Guidelines (14 Cal. Code Regs. §15183), environmental review for this project shall be limited to examination of any significant project-specific environmental effects not analyzed in the 2008 General Plan EIR.
- 3. There are no project-specific environmental effects peculiar to this project.
- 4. There are no environmental effects that were not analyzed in the 2008 General Plan EIR.
- 5. There are no potentially significant off-site impacts and cumulative impacts which were not discussed in the EIR prepared for the 2008 General Plan.
- 6. No mitigation measures from the 2008 General Plan EIR are relevant or have been made part of the project.
- 7. The change to four lanes is consistent with the 2008 General Plan because the analysis in the General Plan states that four lanes at general plan build-out would result in a traffic LOS C, which is below the General Plan's threshold of significance for traffic.
- 8. The project has no effect on the density of development in the 2008 General Plan. General plan amendments approved after 2008 have reduced the density of development in the surrounding area, which will reduce the projected traffic on Bush Street west of Semas Drive.
- 9. Based on the above findings, CEQA requires no additional environmental review for the proposed General Plan Amendment.

**BE IT FURTHER RESOLVED** that the City Council of the City of Lemoore hereby approves General Plan Amendment No. 2018-01 to approve a change to Bush Street from six lanes to four lanes only from Marsh Drive to Semas Drive based on the evidence presented and the following specific findings:

- 1. The general plan amendment is in the public interest, and the General Plan, as amended, will remain internally consistent.
- 2. Table 4.3 in the General Plan indicates that Bush Street west of Semas Drive will still operate at LOS C or better with four lanes when it was analyzed when the General Plan was updated in 2008.
- 3. Since 2008, land that was identified for residential development in the General Plan has been redesignated as future wetlands, thereby reducing the future traffic demand on Bush Street west of Semas Drive to even less than what was analyzed in 2008.

PASSED AND ADOPTED by the City Council of the City of Lemoore at a Regular Meeting held on 1st day of May 2018 by the following vote:

AYES:

Chedester, Neal, Brown, Blair, Madrigal

NOES:

None

ABSENT:

None

ABSTAIN:

None

ATTEST:

APPROVED:

Mary J. Venegas City Clerk

Ray Madrigal

Mayor



119 Fox Street • Lemoore, California 93245 • (559) 924-6744 • Fax (559) 924-9003

# **Staff Report**

To: Lemoore Planning Commission Item No. 5

From: Steve Brandt, City Planner

Date: June 27, 2019 Meeting Date: July 8, 2019

Subject: Request by Tim Palmquist, Victory/Pharris Lemoore, LLC to Extend

Approval of Victory Village Vesting Tentative Subdivision Map No. 2005-

02, Tract 845 for One Year.

# **Proposed Motion:**

I move to approve a one-year extension of time for the Victory Village Vesting Tentative Subdivision Map No. 2005-02, Tract 845.

# **Discussion:**

On June 9, 2019, Tim Palmquist, representing Victory/Pharris Lemoore, LLC, submitted a written request to extend the approval of the Tract No. 845 Tentative Subdivision Map No. 2005-02 for one year. This tentative map was originally approved by the City Council on June 6, 2006. Due to a combination of previous time extensions granted by the Planning Commission and automatic time extensions authorized by the State in response to the Great Recession, the map is currently set to expire on August 6, 2019.

The State Subdivision Map Act governs how cities are to process and administer the creation of new parcels and subdivisions. The Map Act states that a final subdivision map must be filed with the City before the tentative map approval expires. The filing of a final map is usually the beginning of significant investment on the part of the developer. Tentative maps are initially approved for two years. Applicants may request up to six years of extensions. The State legislated a number of automatic extensions of valid tentative maps in response to the Great Recession because there were numerous approved tentative maps with no market for home sales at that time.

There have been a number of additional hurdles this development project has had to endure besides the Recession, including a deal with Naval Air Station Lemoore to reduce the overall size of the project. The tentative map approved 279 lots and two parks, but now only 51 lots and one park will actually be built.

Staff supports the one-year extension to give the developer more time to prepare for construction. If approved, the expiration date would be extended to August 6, 2020. There would be up to three more years of discretionary extensions that the City could grant after that. It is not expected that the State will grant any more automatic extensions.

# <u>Timeline of Extensions of Victory Village Tentative Map</u>

- Vesting Tentative Map Tract 845 Approved June 6, 2006
- Original Expiration Date June 6, 2008
- Application (2008) for Extension Received and Withdrawn
- Senate Bill 1185 (2008 1 Year Automatic) Extended Map to June 6, 2009
- Assembly Bill 333 (2009 2 Years Automatic) Extended Map to August 6, 2011
- Application (2011) for Extension Received and Withdrawn
- Assembly Bill 208 (2011 2 Years Automatic) Extended Map to August 6, 2013
- Assembly Bill 116 (2013 2 Years Automatic) Extended Map to August 6, 2015
- Senate Bill 1303 (2015 2 Years Automatic) Extended Map to August 6, 2017
- Planning Commission (2015 Additional 2 Years) Map Expiration August 6, 2019

# **Attachments:**

- 1) Vicinity Map
- 2) Vesting Tentative Subdivision Map No. 2005-02
- 3) Resolution No. 2006-23 Mitigated Negative Declaration and General Plan Amendment
- 4) Resolution No. 2006-28 Original Approval
- 5) Resolution No. 2008-26 EIR
- 6) Resolution No. 2009-15 Amendment to Resolution No. 2006-28
- 7) Resolution No. 2011-48 Bush Street Abandonment
- 8) Resolution No. 2013-30 General Plan Amendment No. 2013-01
- 9) Resolution No. 2018-18 General Plan Amendment No. 2018-01



Vicinity Map
Victory Village Tentative Map Time Extension

# Minutes of the LEMOORE PLANNING COMMISSION Regular Meeting July 8, 2019

ITEM NO. 1 Pledge of Allegiance

ITEM NO. 2 Call to Order and Roll Call

The meeting was called to order at 7:00 PM.

Chair:

Clement

Vice Chair:

Etchegoin

Commissioners:

Boerkamp, Franklin, Rogers

Absent:

Koelewyn, Meade

City Staff and Contract Employees Present: Community Development Director Holwell, City Planner Brandt (QK), Commission Secretary Baley

ITEM NO. 3 Public Comment

There was no comment.

ITEM NO. 4 Approval – Minutes – Special Meeting, May 28, 2019

Motion by Commissioner Etchegoin, seconded by Commissioner Franklin, to approve the Minutes of the Planning Commission Special Meeting of May 28, 2019.

Ayes: Etchegoin, Franklin, Boerkamp, Rogers, Clement

Absent: Koelewyn, Meade

ITEM NO. 5 Request by Tim Palmquist, Victory/Pharris Lemoore, LLC to extend approval of Victory Village Vesting Tentative Subdivision Map No. 2005-02, Tract 845 for one year.

City Planner Brandt presented the extension request and answered Commissioners' questions.

Tim Palmquist spoke and offered to answer questions.

Motion by Commissioner Etchegoin, seconded by Commissioner Rogers to approve the extension of Victory Village Vesting Tentative Subdivision Map No. 2005-02, Tract 845 for one year.

Ayes: Etchegoin, Rogers, Boerkamp, Franklin, Clement

Absent: Koelewyn, Meade

ITEM NO. 6 Director's Report – Judy Holwell

Community Development Director Holwell provided information regarding:

The second reading of the cannabis ordinance recommended by the Planning Commission was approved by City Council on July 2, 2019 and will take effect 30 days from that date.

The City Manager is working with several developers to draft cannabis related development agreements that will be up for review after the cannabis ordinance takes effect.

Staff received a letter of intent from Assemi Group requesting the annexation of approximately 156 acres of land located at the southeast corner of Lacey Blvd. and 18th Avenue to develop a housing project.

Staff has been communicating with a developer regarding a housing project on the west side and noted that the details are confidential until an application has been received.

Staff reviewed a site plan for a new elementary school to be developed at the northwest corner of 19th Avenue and Cinnamon Drive. It is currently being reviewed by the Public Works Department:

ITEM NO. 7 - Commission's Reports and Requests for Information

Commissioner Rogers asked for an update on the mixed use project that was approved for the southeast corner of Hanford-Armona Road and Highway 41.

Holwell reported that the developer has not submitted anything further to date.

Commissioner Rogers complimented the parking lot improvements at Gateway Plaza.

Commissioner Clement asked if the City would require road improvements for the project to be considered at Lacey Blvd. and Lemoore (18th) Avenue.

Holwell indicated that road improvements would be required.

Brandt expanded on the review process.

ITEM No. 8 – Adjournment

The meeting adjourned at 7:24 PM.

Approved the 12th day of luquest 2019.

APPROVED:

Bob Clement, Chairperson

ATTEST:

Kristie Baley, Commission Secretary



711 W. Cinnamon Drive • Lemoore, California 93245 • (559) 924-6744

# **Staff Report**

To: Lemoore Planning Commission Item No. 7

From: Steve Brandt, Planner

Date: August 3, 2020 Meeting Date: August 10, 2020

Subject: A request by Daley Enterprises, Inc. to extend approval of Tentative

Subdivision Map No. 2018-01 (Tract 793) and Major Site Plan Review No.

2018-03 for one year (APN 023-320-005).

# **Proposed Motion:**

Approve a one-year extension of time for Tentative Subdivision Map No. 2018-01 (Tract 793) and Major Site Plan Review No. 2018-03, in accordance with Resolution No. 2020-11.

# **Discussion:**

On July 29, 2020, Daley Enterprises, Inc. submitted a written request to extend the approval of the Tract No. 793, Tentative Subdivision Map No. 2018-01 for one year. This tentative map was originally approved by the City Council on August 13, 2018. In accordance with the Lemoore Municipal Code and the Subdivision Map Act, the map is currently set to expire on August 13, 2020, two years after the original approval.

The State Subdivision Map Act governs how cities are to process and administer the creation of new parcels and subdivisions. The Map Act states that a final subdivision map must be filed with the City before the tentative map approval expires. The filing of a final map is usually the beginning of significant investment on the part of the developer. Tentative maps are initially approved for two years. Pursuant to Municipal Code Section 8-7F-10, applicants may request up to six one year extensions.

The applicant has stated that coordination with the utility companies has taken longer than anticipated and has ultimately delayed the construction of the project. However, the applicant anticipates that construction will begin in the Spring of 2021 and that the subdivision will be completed by the end of Summer 2021.

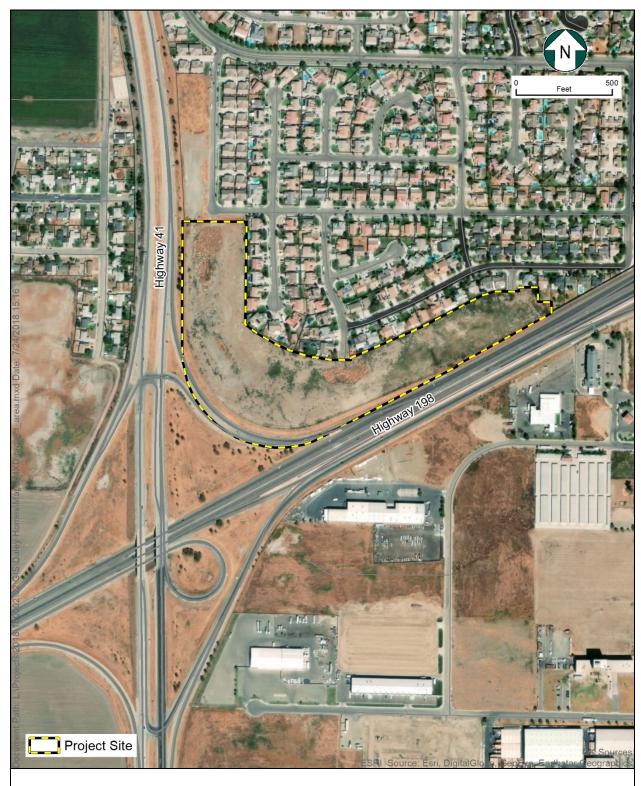
Staff supports the one-year extension to give the developer more time to prepare for construction. If approved, the expiration date would be extended to August 13, 2021. There

would be up to five more years of discretionary extensions that the City could grant, as this proposal constitutes the first extension request by the applicant.

Major Site Plan Review No. 2018-03 was also approved in accordance with the subdivision map. In accordance with Municipal Code Section 9-2A-7, this entitlement is subject to a two-year expiration date if not exercised. The timeframe extensions as proposed for the Tentative Tract Map shall be applied to all accompanying entitlements. The project would still adhere to the adopted conditions as adopted originally for the major site plan review application.

# **Attachments:**

- Vicinity Map Aerial Photo
- Draft Resolution for Approval
- Resolution No. 2018-06 Approving Tentative Subdivision Map No. 2018-01 (Tract 793) and Major Site Plan Review No. 2018-03
- Original City Planner Comments (July 27, 2018)
- Adopted Tentative Subdivision Map
- Site Map
- Multi-family Floor and Elevation Plans
- Example of Single-Family Floor Plans and Elevations
- Mitigated Negative Declaration (Adopted in 2018)



Site Location

# **RESOLUTION NO. 2020-11**

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF LEMOORE APPROVING A ONE-YEAR TIME EXTENSION OF TENTATIVE SUBDIVISION MAP NO. 2018-01 (TRACT 793) AND ACCOMPANYING PLANNING ENTITLEMENTS LOCATED AT THE NORTHEAST CORNER OF HIGHWAY 198 AND HIGHWAY 41 IN THE CITY OF LEMOORE

At a Regular Meeting of the Planning Commission of the City of Lemoore (City) duly called and held on August 10, 2020, at 7:00 p.m. on said day, it was moved by Commissioner

, seconded by Commissioner Resolution be adopted:	, and carried that the following
WHEREAS, Daley Enterprises, Inc. has requested to Subdivision Map 2018-01 (Tract 793) and Major Site Plan Review northeast corner of Highway 198 and Highway 41 in the City (APN)	w No. 2018-03, located at the
WHEREAS, Tentative Subdivision Map 2018-01 and Majo 03 was previously approved by the City of Lemoore on August 13,	
NOW THEREFORE, BE IT RESOLVED that the Plannin Lemoore hereby finds that the potential environmental effects of analyzed by the Mitigated Negative Declaration that was adopted Environmental Quality Act (CEQA) on August 13, 2018 in conjunt Tentative Subdivision Map.	f the Project were adequately ted pursuant to the California
<b>BE IT FURTHER RESOLVED</b> that the Planning Commissextension to Tentative Subdivision Map 2018-01 and Major Site which extension shall expire on August 13, 2021. This extension is with the conditions of the original approval contained in Resolution	Plan Review No. 2018-03, at granted subject to compliance
Passed and adopted at a Regular Meeting of the Planning Commeld on August 10, 2020, by the following votes:	nission of the City of Lemoore
AYES: NOES: ABSTAINING: ABSENT:	
APPROVED:	
Ray Etchegoin	, Chairperson
ATTEST:	
Kristie Baley, Commission Secretary	

#### **RESOLUTION NO. 2018-06**

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF LEMOORE
APPROVING TENTATIVE SUBDIVISION MAP NO. 2018-01 (TRACT 793)
AND MAJOR SITE PLAN REVIEW NO. 2018-03 TO DIVIDE 17.87 ACRES
INTO 30 SINGLE-FAMILY LOTS, 12 MULTI-FAMILY LOTS WITH A REMAINDER LOCATED AT THE
NORTHEAST CORNER OF HIGHWAY 198 AND HIGHWAY 41 IN THE CITY OF LEMOORE

At a Regular Meeting of the Planning Commission of the City of Lemoore (City) duly called and held on August 13, 2018, at 7:00 p.m. on said day, it was moved by Commissioner <a href="Meade">Meade</a>, seconded by Commissioner <a href="Koelewyn">Koelewyn</a>, and carried that the following Resolution be adopted:

WHEREAS, Daley Enterprises, Inc. has requested approval of a Tentative Subdivision Map and Major Site Plan Review for a project located at the northeast corner of Highway 198 and Highway 41 in the City (APN 023-320-005); and

**WHEREAS,** the proposed site is 17.81 acres in size, and is zoned both Low Density Residential (RLD) and Low-Medium Density Residential (RLMD); and

**WHEREAS**, the proposed project contains 30 single-family lots in the portion of the site zoned RLD, and 12 multi-family lots (35 units) in the portion of the site zoned RLMD; and

WHEREAS, the portion of the Tentative Subdivision Map proposed for the 12 multifamily lots has been zoned RLMD since before 2008; and

**WHEREAS**, the portion of the Tentative Subdivision Map proposed for the 30 single-family lots was not given a land use designation in the 2008 General Plan, and was left without any zoning designation when the Zoning Ordinance was updated in 2012; and

**WHEREAS**, on August 15, 2017, the City Council approved General Plan Amendment No. 2016-02 and Zone Change Amendment No. 2016-02, designating and zoning the portion of the site where the 30 single-family lots are proposed as RLD.

WHEREAS, as part of General Plan Amendment No. 2016-02 and Zone Change Amendment No. 2016-02, a Mitigated Negative Declaration contemplating approximately 134 residential units was prepared and adopted by the City Council pursuant to the California Environmental Quality Act (CEQA); and

**WHEREAS**, the City Planning Commission held a duly noticed public hearing at its August 13, 2018, meeting.

**NOW THEREFORE, BE IT RESOLVED** that the Planning Commission of the City of Lemoore hereby makes the following findings regarding the proposed projects:

 The proposed subdivision, together with the provisions for its design and improvement, is consistent with the general plan and all applicable provisions of the Subdivision Ordinance.

- 2. The proposed project does not exceed the total density under the base zoning district or the general plan land use designation.
- 3. The proposed project will not be substantially detrimental to adjacent property and will not materially impair the purposes of the Zoning Ordinance or the public interest.
- 4. As proposed and conditioned herein, the site design of the project is consistent with the new residential development standards in the Zoning Ordinance.
- 5. The proposed project is consistent with the objectives of the General Plan and complies with applicable zoning regulations, and improvement standards adopted by the City.
- 6. The proposed project will not create conflicts with vehicular, bicycle, or pedestrian transportation modes of circulation.
- The project's lot sizes are consistent with densities in the General Plan and are appropriate for this site.
- 8. The Mitigated Negative Declaration previously prepared for General Plan Amendment No. 2016-02 and Zone Change No. 2016-02 is appropriate to be utilized for evaluation of environmental impacts of this project, in accordance with CEQA, as the Mitigated Negative Declaration considered environmental impacts that could result from a project of approximately 134 residential units, which is larger than the amount of units in the project being considered.

**BE IT FURTHER RESOLVED** that the Planning Commission of the City of Lemoore approves Tentative Subdivision Map No. 2018-01 (Tract 793) and Major Site Plan Review No. 2018-03 subject to the following conditions:

- 1. The site shall be developed consistent with the approved tentative map and its conditions, the Site Plan No. 2018-03 comments, and applicable development standards found in the Zoning Ordinance and City Municipal Code.
- 2. All mitigation measures in the Mitigated Negative Declaration approved with General Plan Amendment 2016-02 and Zone Change 2016-02 shall be complied with.
- 3. The minimum lot size of 70x100 feet shall be achieved prior to recordation of the final map or an application for a minor deviation shall be made.
- The project shall be developed and maintained in substantial compliance with the tentative map, except for any modifications that may be needed to meet these conditions of approval.
- 5. The final subdivision map shall be submitted in accordance with City ordinances and standards.
- 6. Plans for all public and private improvements, including but not limited to, water, sewer, storm drainage, road pavement, curb and gutter, sidewalk, street lights, landscaping, and fire hydrants shall be approved by the City Engineer, and these improvements shall be completed in accordance with the approved plans to the satisfaction of the Public Works Department.

- 7. Park land in-lieu fees shall be paid to the City for 0.63 acres (or as adjusted based on the final map) in accordance with the procedures in Section 8-7N-4 of the City Municipal Code. Fees shall be paid prior to approval of the final map.
- 8. A public facilities maintenance district (PFMD) shall be formed in conjunction with the final map acceptance to provide the maintenance costs for common landscaping and other improvements, in accordance with existing City policy. Annexation into an existing PFMD is acceptable.
- 9. The project shall be subject to the applicable development impact fees adopted by resolution of the City Council.
- 10. A noise and odor easement shall be recorded on the property, in a form acceptable to the City Attorney, to acknowledge the presence of nearby industry and railroad, and the right of the industry and railroad to continue to emit such noise and odors as are otherwise allowable by law and to ensure that industry in these areas is not unreasonable hindered by residential users and owners that move nearby at a later date.
- 11. The developer shall comply with the standards, provisions, and requirements of the San Joaquin Valley Air Pollution Control District that relate to the project.
- 12. A block wall shall be constructed along the rear lot line of Lots 1 through 6 and Lots 27 through 42. The block wall will separate the single-family lots from Highway 198 and the multi-family lots from Highway 41.
- 13. Fire hydrant types and locations shall be approved by the Lemoore Volunteer Fire Department.
- 14. Concrete pads for installation of mailboxes shall be provided in accordance with determinations made by the Lemoore Postmaster.
- 15. Street trees from the city approved street tree list shall be planted with root barriers as per Public Works Standards and Specifications.
- 16. Street lights shall be provided within the project as per City local street lighting standards.
- 17. Any existing roadway, sidewalk, or curb and gutter that is damaged during construction shall be repaired or replaced to the satisfaction of the Public Works Department.
- 18. All signs shall require a sign permit separate from the building permit.
- 19. The project and all subsequent uses must meet the requirements found in Section 9-5B-2 of the Zoning Ordinance related to noise, odor, and vibration, and maintenance.
- 20. This tentative subdivision map approval shall expire within two years, unless a final map is filed or an extension is granted via legislation or by the City, in accordance with the Subdivision Map Act.

Passed and adopted at a Regular Meeting of the Planning Commission of the City of Lemoore held on August 13, 2018, by the following votes:

AYES: Meade, Koelewyn, Boerkamp, Franklin, Marvin, Etchegoin, Clement

NOES:

**ABSTAINING:** 

ABSENT:

**APPROVED:** 

**Bob Clement, Chairperson** 

ATTEST:

Kristie Baley, Planning Commission Secretary



119 Fox Street • Lemoore, California 93245 • (559) 924-6700 • Fax (559) 924-9003

# **Major Site Plan Review Comments**

To: Lemoore Planning Commission

From: Steve Brandt, Planner

Date: July 27, 2018

Subject: Major Site Plan Review No. 2018-03 (Daley Enterprises)

This site plan is being reviewed under the current Zoning Ordinance requirements for Preapplication Conferences and Major Site Plan Review. These are City staff's recommended comments. The comments will be approved by the Planning Commission.

# SITE PLAN DESCRIPTION

The site is located on the northeast corner of Highway 198 and Highway 41. This project is requesting approval of Tract No. 793 for 42 lots. The project would be constructed in two phases, with the east portion of the site developing first. Lot sizes range from 7,045 square feet to 14,296 square feet with an average size of 8,599 square feet.

# USE

The area of the site planned for single family residential is designated Low Density Residential by the General Plan and zoned RLD (Low Density Residential). The portion of the site planned for multi-family residential is designated Low-Medium Density Residential by the General Plan and zoned RLMD (Low-Medium Density Residential). The portion of the map shown as a remainder parcel is zoned RLD. The proposal tentative map is therefore consistent with the City's land use designation and zoning.

#### **RIGHT OF WAY AND ACCESS**

The single-family lots front a proposed extension of Arcata Avenue. The multi-family lots will front a proposed extension of Sonoma Avenue. Staff has reviewed the estimated trips

generated by the project and concluded that the increase in trips would not trigger the need for additional traffic signals.

# STREET NAMES

Street names shall be adjusted on the final map as follows: Change Somoma Avenue to Sonoma Avenue

Alternatively, if the developer's ultimate intention is to connect the two stub streets, then it would be more appropriate if both streets were named Arcata Drive. This should be discussed with City staff prior to final map submittal.

# AREA, SETBACK, HEIGHT AND COVERAGE STANDARDS

#### 9-5A-4: GENERAL ZONING DISTRICT DEVELOPMENT STANDARDS

The project, as shown, meets the standards in Table 9-5A-4A, except in one aspect. Lots 14 through 19 and lots 25 and 26 do not meet the minimum lot depth of 100 feet. The street right of way shall be adjusted on the final map to meet the minimum standard. Where the standard absolutely cannot be met, the applicant shall obtain a minor deviation permit from the Community Development Director prior to approval of the final map.

For single-family residential subdivisions, the front yard setback of adjacent homes shall have a minimum two-foot (2') stagger between adjacent lots.

#### DRAINAGE

The site is planned to drain to the storm drain system that has been constructed with the neighborhood north of the site.

# PARKS AND OPEN SPACE

There is no new dedicated open space proposed with this tentative map. City Ordinance requires 0.016 acres per single-family lot be dedicated with a new subdivision. The proposed 30 lots would require 0.48 acres of park acreage. The ordinance also requires 0.0127 acres per multi-family unit. The proposed 12 lots would require 0.15 acres of park acreage. The final acreage shall be determined based on the acreage shown on final map. The acreage required shall be provided through an in-lieu fee with the amount based upon an appraisal made by a certified general real estate appraiser in accordance with City Ordinance Section 8-7N-4 or other method acceptable to the Community Development Director.

# **DESIGN STANDARDS**

9-5B-2: NOISE, ODOR, VIBRATION, AND MAINTENANCE PERFORMANCE STANDARDS

The project and all subsequent uses must meet the requirements found in Section 9-5B-2 of the Zoning Ordinance related to noise, odor, and vibration, and maintenance.

A block wall is required along the south and west sides of the site. The wall height shall match the heights of the two adjacent walls where they connect and then gradually adjust from one height to the other.

#### 9-5B-3: PROPERTY AND UTILITY IMPROVEMENTS:

Installation of curbs, gutters, and sidewalks shall be required. All on site utilities shall be installed underground.

#### 9-5B-4: OUTDOOR LIGHTING:

The project shall meet all the applicable requirements for outdoor lighting found in Section 9-5B-4 of the Zoning Ordinance.

#### 9-5B-6: SCREENING:

All exterior roof and ground mounted mechanical equipment, including, but not limited to, heating, air conditioning, refrigeration equipment, plumbing lines, duct work, and transformers, shall be screened from public view from abutting public streets. Screening of mechanical equipment shall be compatible with other on site development in terms of colors, materials, and/or architectural styles.

#### 9-5C-3: DESIGN STANDARDS FOR RESIDENTIAL PROJECTS

See section 9-5C-3 of the Zoning Ordinance for standards pertaining to the residential building design and architecture. Submittal of conceptual elevation and floor plans for the multi-family units and for each single-family master home plan will be required with the tentative map application.

#### 9-5D1-2: LANDSCAPE STANDARDS

Show conceptual locations of trees, shrubs, and groundcover. Identify species of street trees. Drought tolerant species must be used.

- C. Plant Type: Landscape planting shall emphasize drought tolerant and native species (especially along natural, open space areas), shall complement the architectural design of structures on the site, and shall be suitable for the soil and climatic conditions specific to the site. (Ord. 2013-05, 2-6-2014)
- 2. Street And Parking Lot Trees: Street and parking lot trees shall be selected from the city's adopted master list of street trees and parking lot trees.
- 3. Tree Root Barriers: Trees planted within five feet (5') of a street, sidewalk, paved trail, curb, or walkway shall be separated from hardscapes by a root barrier to prevent physical damage to public improvements.
- D. Planting Size, Spacing, And Planter Widths: In order to achieve an immediate effect of a landscape installation and to allow sustained growth of planting materials, minimum plant material sizes, plant spacing, and minimum planter widths (inside measurements) are as follows:

1. Trees: The minimum planting size for trees shall be fifteen (15) gallon, with twenty five percent (25%) of all trees on a project site planted at a minimum twenty four inch (24") box size. For commercial, office, community/civic, and industrial development, tree spacing within perimeter planters along streets and abutting residential property shall be planted no farther apart on center than the mature diameter of the proposed species. Minimum planter widths shall be five feet (5').

Street Trees: Street trees shall be provided a minimum of every thirty feet (30') on center on street adjacent to a side yard, and a minimum one per lot when adjacent to a front yard. Tree species shall be approved by the city as part of the improvement plan review process and shall be selected from a city approved tree list. Trees shall be planted ten feet (10') away from alleys, driveways, fire hydrants, water lines, and sewer lines and five feet (5') from gas, electrical, telephone, cable television, and adjoining property lines. They shall also be planted a minimum of twenty feet (20') from city streetlights. Ultimate planting locations shall be subject to city review and approval based upon field conditions.

Master Landscape Plans for the landscaping of front yards that meet the Model Water Efficient Landscape Ordinance (MWELO) shall be submitted concurrently with Master Home Building Plans.

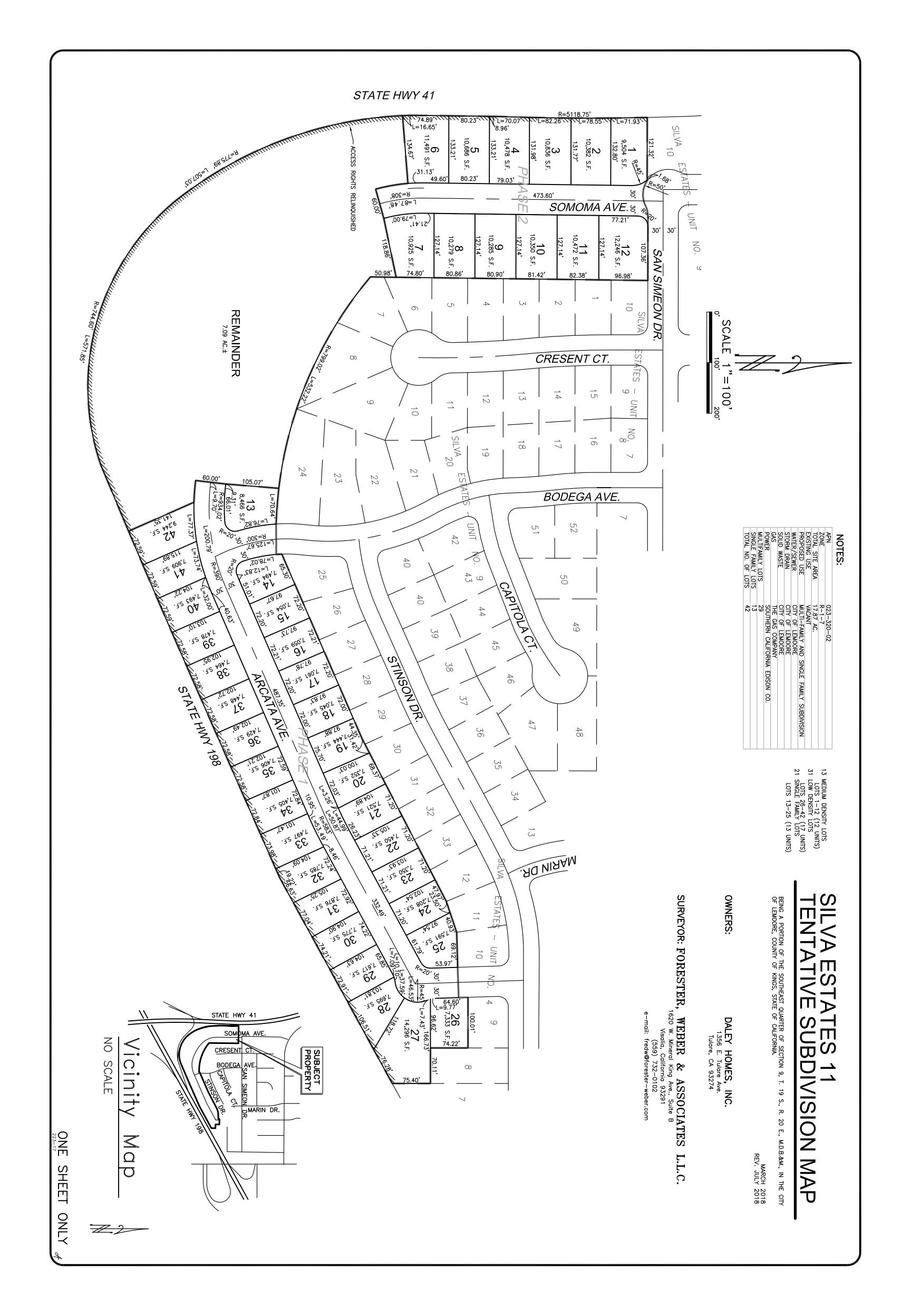
#### **ARCHITECTURAL STANDARDS**

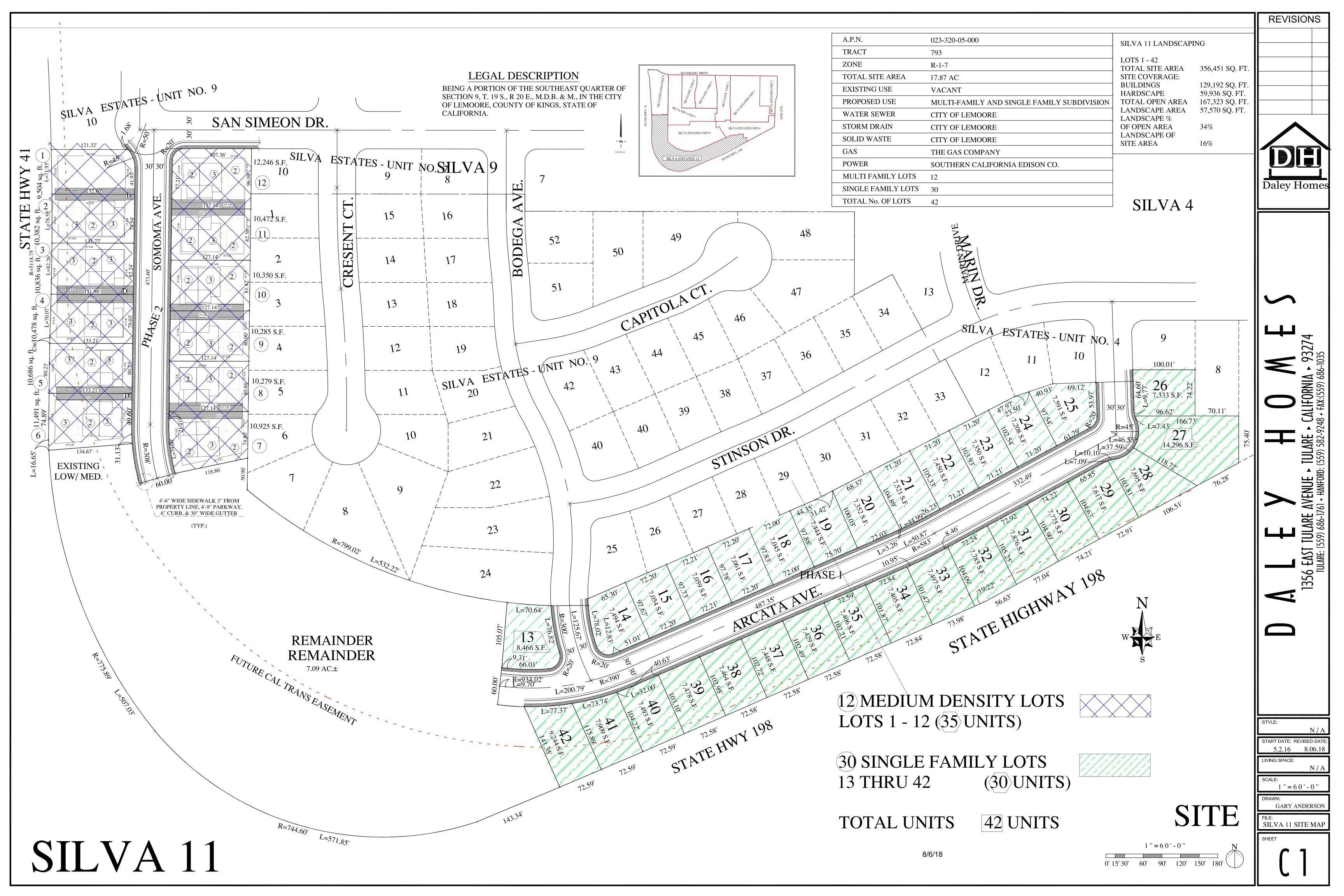
The design of the single-family and multi-family homes shall meet the standards in Section 9-5C-3 DESIGN STANDARDS FOR RESIDENTIAL PROJECTS. This includes variation of multi-family homes, staggered setbacks, garage placement.

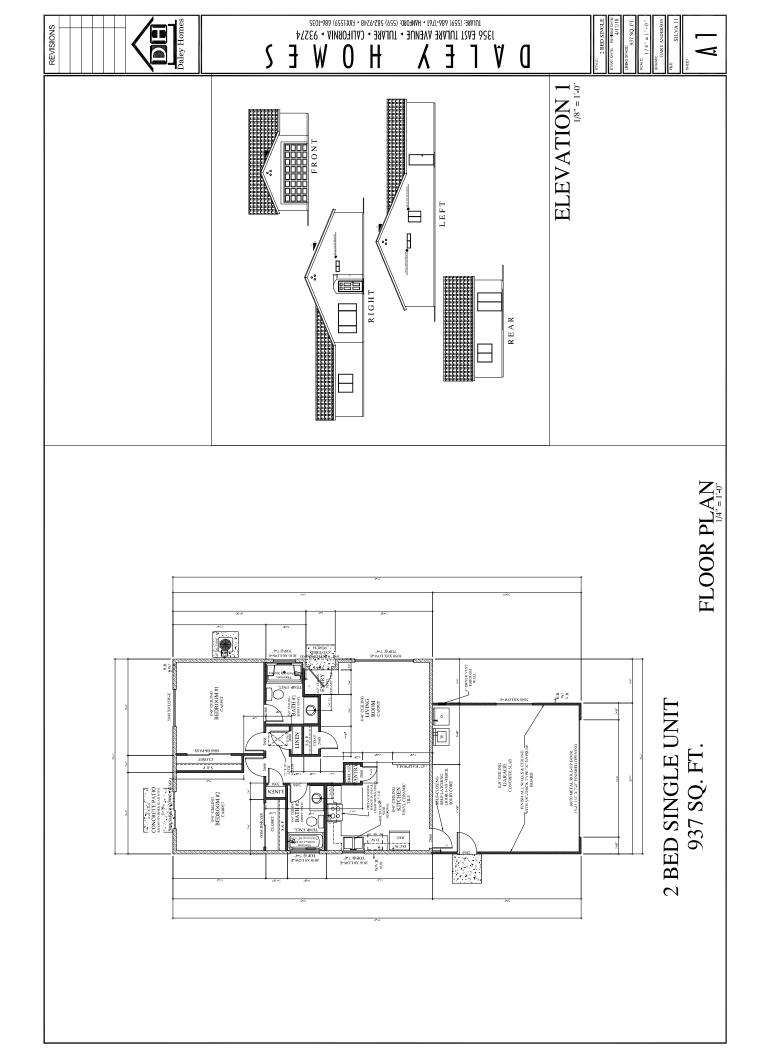
#### **PARKING**

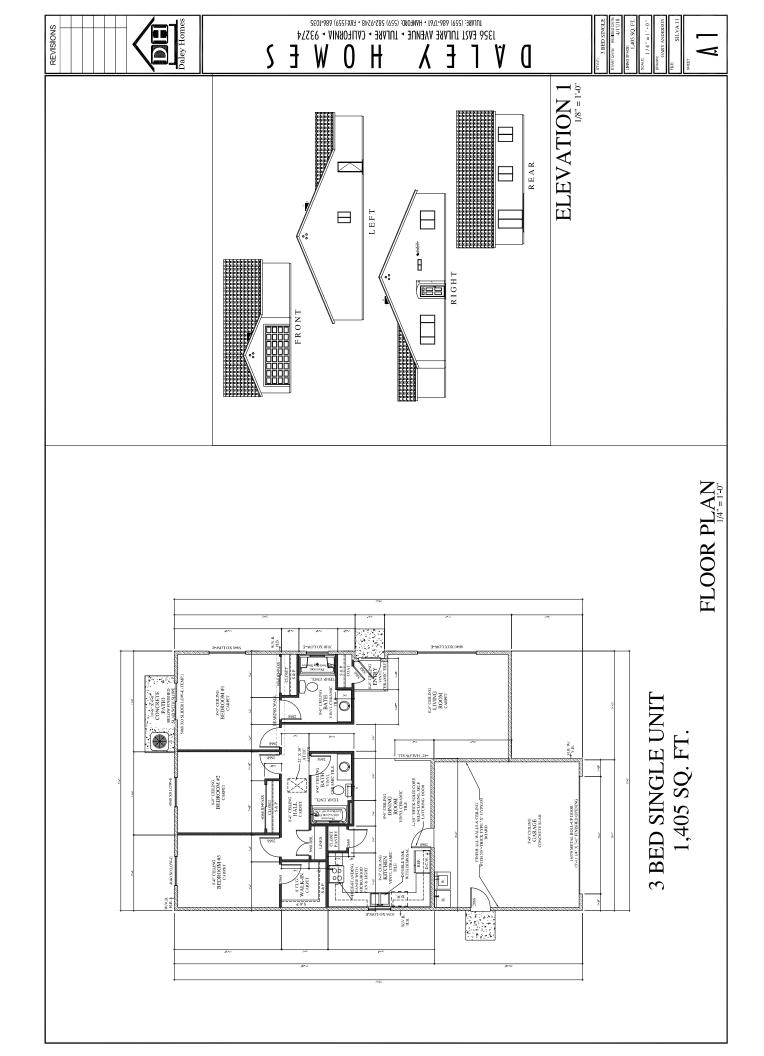
9-5E-3: GENERAL PARKING REGULATIONS:

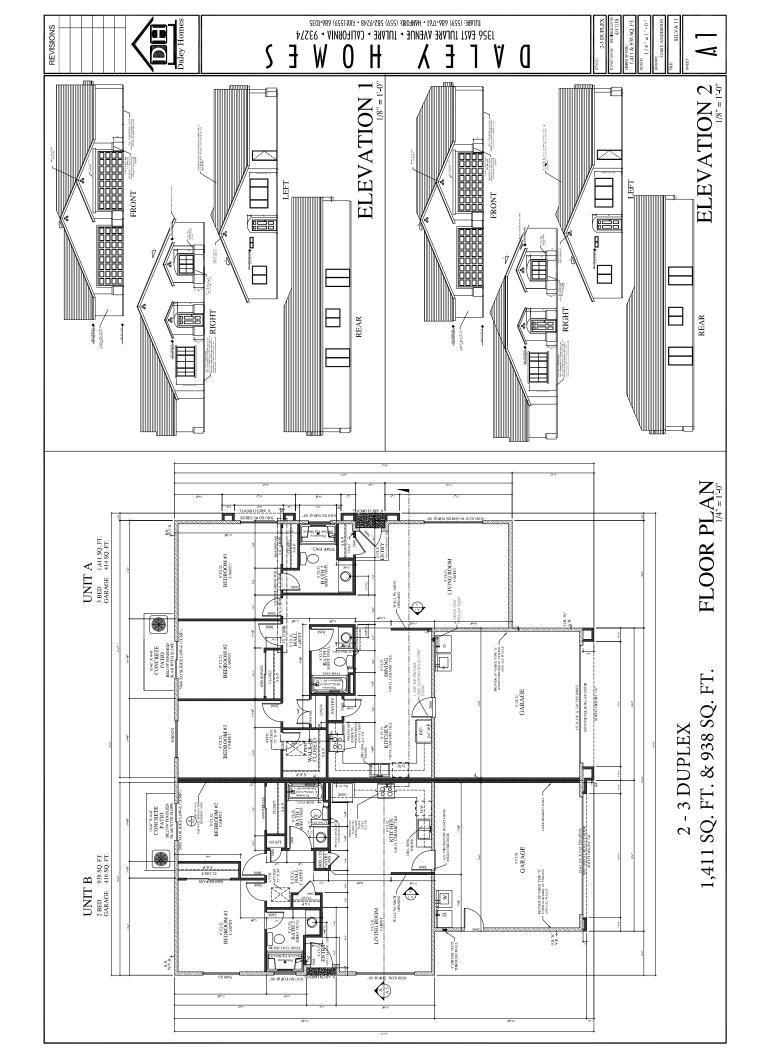
The site plan meets the parking requirements for off-street parking.

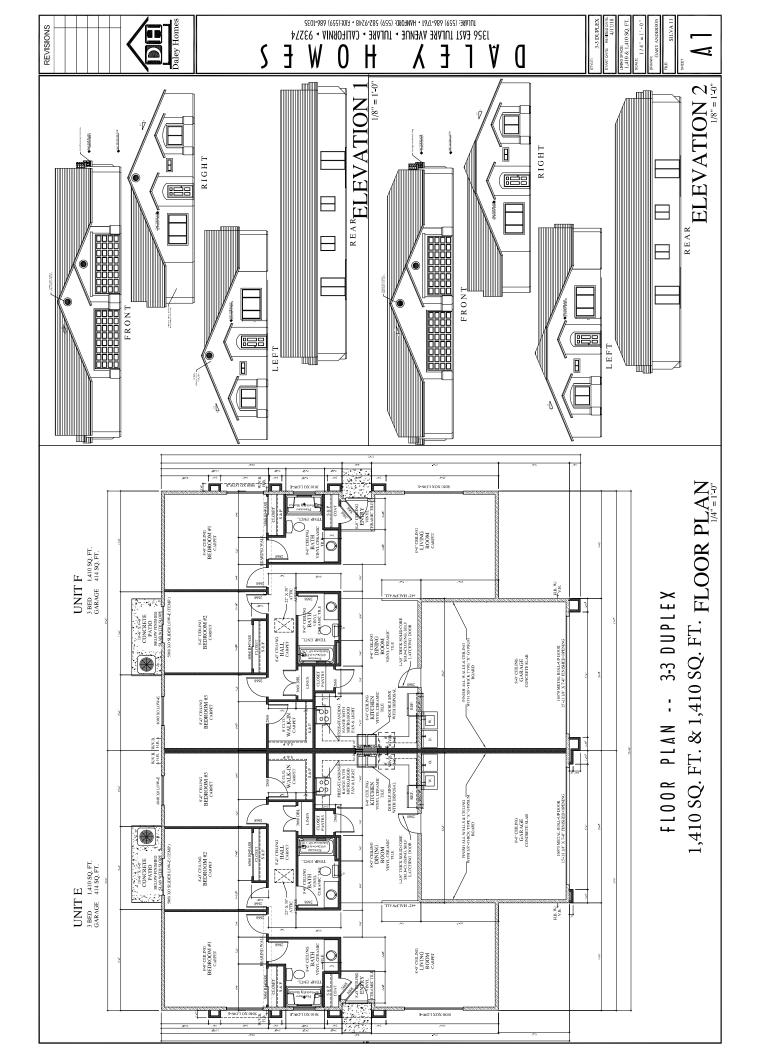












## AUBURN 5B





ELEVATION A



ELEVATION B



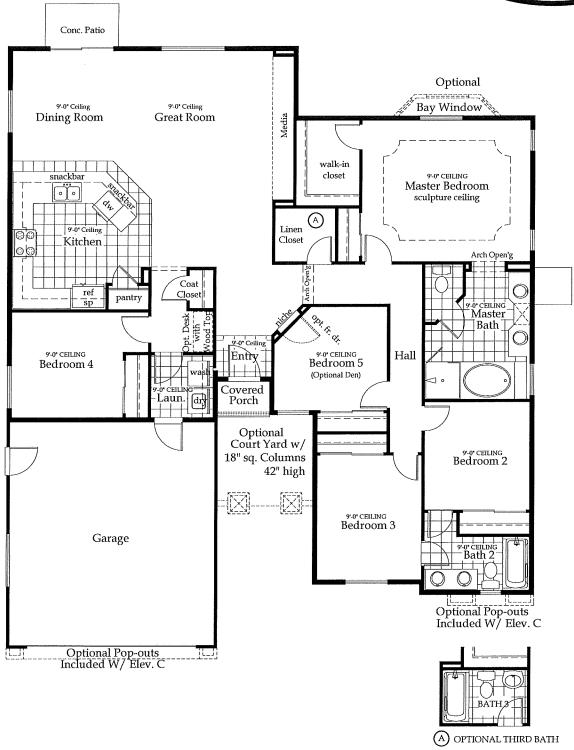
ELEVATION (

Daley Homes reserves the right to modify floor plans, exteriors, home prices, incentives, material and specifications at anytime without notice in the interest of continuing our policy of research and product improvements. Exterior designs, window locations and rooflines may vary depending upon the plan and options selected. Not all homes will fit on all lots with added third car garage. Renderings are an artist's conception. 06.04.12

## AUBURN 5B

5 Bedroom 2 Bath 1,939 sq. ft.





Daley Homes reserves the right to modify floor plans, exteriors, home prices, incentives, material and specifications at anytime without notice in the interest of continuing our policy of research and product improvements. Exterior designs, window locations and rooflines may vary depending upon the plan and options selected. Not all homes will fit on all lots with added third car garage. Renderings are an artist's conception. 01.18.13

# MAJESTIC









**ELEVATION B** 

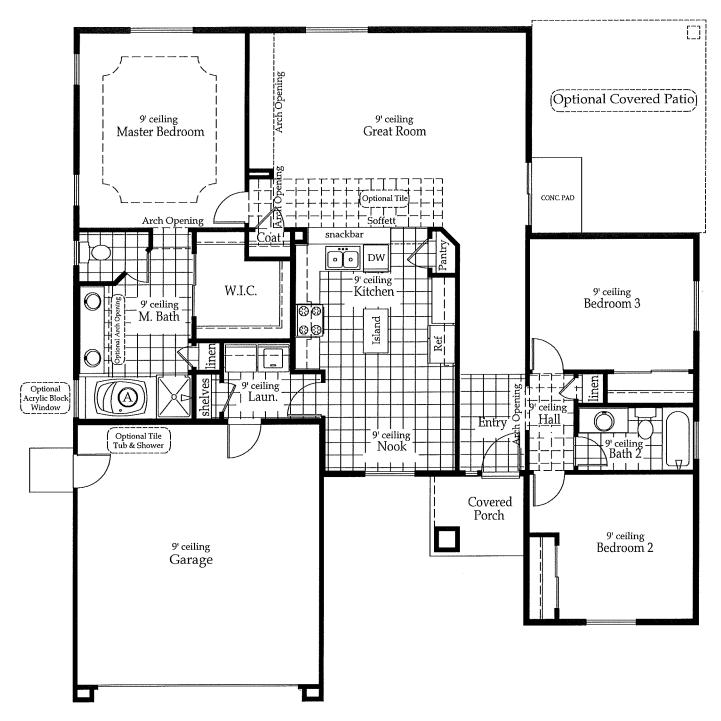


Daley Homes reserves the right to modify floor plans, exteriors, home prices, incentives, material and specifications at anytime without notice in the interest of continuing our policy of research and product improvements. Exterior designs, window locations and rooflines may vary depending upon the plan and options selected. Not all homes will fit on all lots with added third car garage. Renderings are an artist's conception.

# MAJESTIC

3 Bedroom 2 Bath 1,650 sq. ft.





Daley Homes reserves the right to modify floor plans, exteriors, home prices, incentives, material and specifications at anytime without notice in the interest of continuing our policy of research and product improvements. Exterior designs, window locations and rooflines may vary depending upon the plan and options selected. Not all homes will fit on all lots with added third car garage. Renderings are an artist's conception. 01.17.13



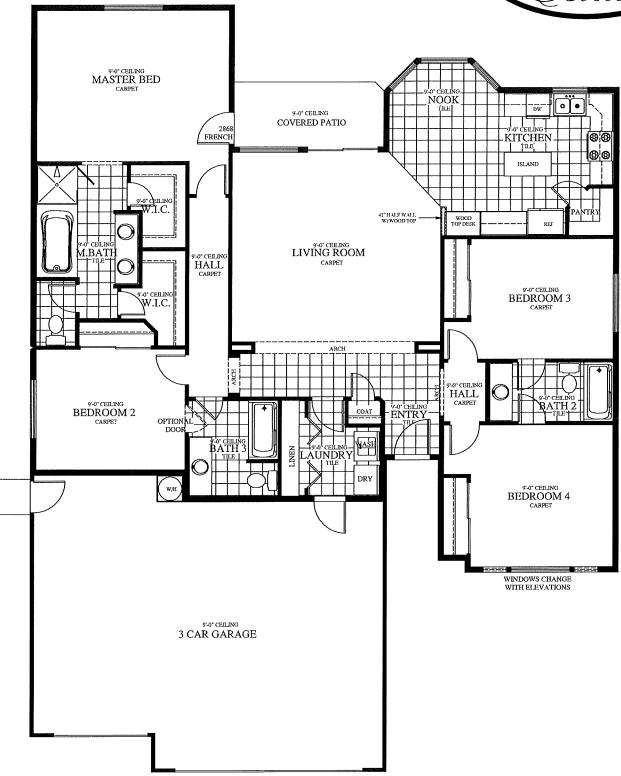
ELEVATION A

Daley Homes reserves the right to modify floor plans, exteriors, home prices, incentives, material and specifications at anytime without notice in the interest of continuing our policy of research and product improvements. Exterior designs, window locations and rooflines may vary depending upon the plan and options selected. Not all homes will fit on all lots with added third car garage. Renderings are an artist's conception.

# **PHOENIX**

4 Bedroom 3 Bath 1,870 sq. ft.





Daley Homes reserves the right to modify floor plans, exteriors, home prices, incentives, material and specifications at anytime without notice in the interest of continuing our policy of research and product improvements. Exterior designs, window locations and rooflines may vary depending upon the plan and options selected. Not all homes will fit on all lots with added third car garage. Renderings are an artist's conception.

# INITIAL STUDY/MITIGATED NEGATIVE DECLARATION CITY OF LEMOORE

### Daley Homes General Plan Amendment and Zone Change

May 2017

#### Contact:

Judy Holwell (559) 924-6740 jholwell@lemoore.com 711 W. Cinnamon Drive Lemoore, CA 93245

Comments must be received by: June 12, 2017 (20 days after notice)



# ADMINISTRATIVE DRAFT INITIAL STUDY/MITIGATED NEGATIVE DECLARATION

## City of Lemoore

#### Prepared for:



City of Lemoore 711 W Cinnamon Drive Lemoore, CA 93245

Contact Person: Judy Holwell, Development Services Director Phone: 559) 924-6740

#### **Consultant:**



901 East Main Street Visalia, CA 93292 Contact: Steve Brandt, City Planner Phone: (559) 733-0440

Fax: (559) 733-7821

June 2017

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#### MITIGATED NEGATIVE DECLARATION

As Lead Agency under the California Environmental Quality Act (CEQA), the City of Lemoore reviewed the Project described below to determine whether it could have a significant effect on the environment because of its development. In accordance with CEQA Guidelines Section 15382, "[s]ignificant effect on the environment" means a substantial, or potentially substantial, adverse change in any of the physical conditions within the area affected by the project, including land, air, water, minerals, flora, fauna, ambient noise, and objects of historic or aesthetic significance.

#### **Project Name**

Daley Homes General Plan Amendment and Zone Change

#### **Project Location**

The Project site is located at the northeast corner of State Route 198 and State Route 41, south of San Simeon Drive and west of Arcata Avenue (APN 023-320-005).

#### **Project Description**

A request by Daley Homes to reestablish the Low Density Residential and Low Medium Density Residential land use designation on the proposed undesignated site, and reestablish the Low-Medium Density Residential (RLMD) and the Low Density Residential (RLD) zones on the proposed unzone site. The reestablishment of the general plan designation and zoning would accommodate up to approximately 134 residential units.

#### Mailing Address and Phone Number of Contact Person

Scott Daley Vice President Daley Homes 1356 E. Tulare Ave. Tulare, CA 93274 (559) 686-1761

#### **Findings**

As Lead Agency, the City of Lemoore finds that the Project will not have a significant effect on the environment. The Environmental Checklist (CEQA Guidelines Appendix G) or Initial Study (IS) (see *Section 3 - Environmental Checklist*) identified one or more potentially significant effects on the environment, but revisions to the Project have been made before the release of this Mitigated Negative Declaration (MND) or mitigation measures would be implemented that reduce all potentially significant impacts less-than-significant levels. The Lead Agency further finds that there is no substantial evidence that this Project would have a significant effect on the environment.

## Mitigation Measures Included in the Project to Avoid Potentially Significant Effects

**MM 3.8.1:** A qualified biologist shall conduct a pre-construction survey on the Project site and within 500 feet of its perimeter within 14 days of and no more than 30 days prior to the start of construction activities.

If any evidence of occupation of the Project site by listed or other special-status species is subsequently observed, a buffer shall be established by a qualified biologist that results in sufficient avoidance to comply with applicable regulations. If sufficient avoidance cannot be established, the United States Fish and Wildlife Service and California Department of Fish and Game shall be contacted for further guidance and consultation on additional measures. The Project proponent shall obtain any required permits from the appropriate wildlife agency. Copies of all permits and evidence of compliance with applicable regulations shall be submitted to the lead agency.

The following buffer distances shall be established prior to construction activities:

- San Joaquin kit fox or American badger potential den: 50 feet;
- San Joaquin kit fox known den: 100 feet;
- San Joaquin kit fox or American badger pupping den: contact the California Department of Fish and Game and United States Fish and Wildlife Service;
- Burrowing owl burrow outside of breeding season: 160 feet;
- Burrowing owl burrow during breeding season: 250 feet;
- Swainson's hawk nest during breeding season: ½ mile;
- Other protected raptor nests during the breeding season: 300 feet;
- Other protected nesting migratory bird nests during the breeding season: 50 feet; and
- Other special-status wildlife species: as recommended by qualified biologist.

**MM 3.8.2:** If initial grading activities are planned during the potential nesting season for migratory birds/raptors that may nest on or near the Project site, the preconstruction survey shall evaluate the sites and accessible lands within an adequate buffer for active nests of migratory birds/raptors. If any nesting birds/raptors are observed, a qualified biologist shall determine buffer distances and/or the timing of Project activities so that the proposed Project does not cause nest abandonment or destruction of eggs or young. This measure shall be implemented so that the proposed Project remains in compliance with the Migratory Bird Treaty Act and applicable state regulations.

If nesting raptors are identified during the surveys, active raptor nests should be avoided by 500 feet and all other migratory bird nests should be avoided by 250 feet. Avoidance buffers may be reduced if a qualified and approved on-site monitor determines that encroachment into the buffer area is not affecting nest building, the rearing of young, or otherwise affect the breeding behaviors of the resident birds. Avoidance buffers can also be reduced through consultation with the CDFW and USFWS. If Swainson's hawks are found to nest within the survey area, active Swainson's hawk nests shall be avoided by 0.5 mile unless this avoidance buffer is reduced through consultation with the CDFW and/or USFWS.

No construction or earth-moving activity shall occur within a non-disturbance buffer until it is determined by a qualified biologist that the young have fledged (that is, left the nest) and have attained sufficient flight skills to avoid Project construction areas. This typically occurs by early July, but September 1st is considered the end of the nesting period unless otherwise determined by a qualified biologist. Once raptors have completed nesting and young have fledged, disturbance buffers will no longer be needed and can be removed, and monitoring can be terminated.

MM 3.8.3: If any burrowing owl burrows are observed during the preconstruction survey, avoidance measures shall be consistent and in accordance with protocols outlined in the Burrowing Owl Survey Protocol and Mitigation Guidelines (Burrowing Owl Consortium 1993) and the Staff Report on Burrowing Owl Mitigation (CDFW 2012). Active burrows shall be avoided, but if avoidance is not possible then compensation shall be provided for the active or passive displacement of western burrowing owls, and habitat acquisition and the creation of artificial dens for any western burrowing owls shall be provided for any owls relocated from construction areas. These measures are outlined as follows:

- 1. A pre-construction survey of construction area, including a 150-meter buffer (500 feet), shall be conducted no less than 14 days and no more than 30 days prior to ground disturbing activities. If more than 30 days lapse between the time of the pre-construction survey and the start of ground-disturbing activities, another pre-construction survey shall be completed. The second survey (or other subsequent surveys if necessary) shall be conducted and timed to occur sometime between 30 days and 24 hours prior to ground disturbance.
- 2. If western burrowing owls are present on the construction site (or within 500 feet of the construction site), exclusion fencing shall be installed between the nest site or active burrow and any earth-moving activity or other disturbance. Exclusion areas shall extend 160 feet around occupied burrows during the non-breeding season (September 1 through January 31) and extend 250 feet around occupied burrows during the breeding season (February 1 through August 31) as described in The California Burrowing Owl Consortium's Survey Protocol and Mitigation Guidelines (California Burrowing Owl Consortium 1993).
- 3. If western burrowing owls are present in the non-breeding season and must be passively relocated from the Project site, passive relocation shall not commence until October 1st and must be completed by February 1st. Passive relocation must only be conducted by a

qualified biologist or ornithologist and with approval by CDFW. After passive relocation, the area where owls occurred and its immediate vicinity shall be monitored by a qualified biologist daily for one week and once per week for an additional two weeks to document that owls are not reoccupying the site.

4. If permanent impacts to nesting, occupied and satellite burrows, or burrowing owl habitat occur, compensation shall be based upon the number of owls or pairs of owls relocated from the construction area. Compensation acreage shall be determined as described in the CDFW's Staff Report on Burrowing Owl Mitigation (CDFW 2012).

#### **MM 3.8.4:** The measures listed below shall be implemented during construction:

1. Pre-construction surveys shall be conducted no fewer than 14 days and no more than 30 days prior to the beginning of ground disturbance and/or construction activities. If any San Joaquin kit fox dens are found during preconstruction surveys, exclusion zones shall be placed in accordance with USFWS Recommendations using the following:

## San Joaquin kit fox USFWS Exclusion Zone Recommendations

Den Type	Recommendation
Potential Den	50-foot radius
Known Den	100-foot radius
Natal/Pupping Den	Contact U.S. Fish and Wildlife
(Occupied and Unoccupied)	Service for guidance
Atypical Den	50-foot radius

- 2. If any den must be removed, it must be appropriately monitored and excavated by a trained wildlife biologist. Destruction of natal dens and other "known" kit fox dens must not occur until authorized by USFWS. Replacement dens will be required if such dens are removed. Potential dens that are removed do not need to be replaced if they are determined to be inactive by using standard monitoring techniques (e.g., applying tracking medium around the den opening and monitoring for San Joaquin kit fox tracks for three consecutive nights).
- 3. Project-related vehicles shall observe a daytime speed limit of 20-mph throughout the site in all Project areas, except on County roads and State and federal highways; this is particularly important at night when kit foxes and badgers are most active. Night-time construction shall be minimized to the extent possible. However, if construction at night does occur, then the speed limit shall be reduced to 10-mph. Off-road traffic outside of designated Project areas shall be prohibited.
- 4. To prevent inadvertent entrapment of kit foxes or other animals during the construction phase of a Project, all excavated, steep-walled holes or trenches more than 2-feet deep should be covered at the close of each working day by plywood or similar materials. If the trenches cannot be closed, one or more escape ramps constructed of earthen-fill or

wooden planks shall be installed. Before such holes or trenches are filled, they shall be thoroughly inspected for trapped animals. If at any time a trapped or injured kit fox is discovered, the USFWS and the CDFW shall be contacted at the addresses provided below.

- 5. Kit foxes are attracted to den-like structures such as pipes and may enter stored pipes and become trapped or injured. All construction pipes, culverts, or similar structures with a diameter of 4-inches or greater that are stored at a construction site for one or more overnight periods shall be thoroughly inspected for kit foxes before the pipe is subsequently buried, capped, or otherwise used or moved in any way. If a kit fox is discovered inside a pipe, that section of pipe shall not be moved until the USFWS has been consulted. If necessary, and under the direct supervision of the biologist, the pipe may be moved only once to remove it from the path of construction activity, until the fox has escaped.
- 6. All food-related trash items such as wrappers, cans, bottles, and food scraps shall be disposed of in securely closed containers and removed at least once a week from a construction or Project sites.
- 7. No pets, such as dogs or cats, shall be permitted on the Project sites to prevent harassment, mortality of kit foxes, or destruction of dens.
- 8. Use of rodenticides and herbicides in Project areas shall be restricted. This is necessary to prevent primary or secondary poisoning of kit foxes and the depletion of prey populations on which they depend. All uses of such compounds shall observe label and other restrictions mandated by the U.S. Environmental Protection Agency, California Department of Food and Agriculture, and other State and Federal legislation, as well as additional Project-related restrictions deemed necessary by the USFWS. If rodent control must be conducted, zinc phosphide shall be used because of a proven lower risk to kit fox.
- 9. A representative shall be appointed by the Project proponent who will be the contact source for any employee or contractor who might inadvertently kill or injure a kit fox or who finds a dead, injured or entrapped kit fox. The representative will be identified during the employee education program and their name and telephone number shall be provided to the USFWS.
- 10. An employee education program shall be conducted. The program shall consist of a brief presentation by persons knowledgeable in San Joaquin kit fox biology and legislative protection to explain endangered species concerns to contractors, their employees, and military and/or agency personnel involved in the Project. The program shall include: a description of the San Joaquin kit fox and its habitat needs; a report of the occurrence of kit fox in the Project area; an explanation of the status of the species and its protection under the Endangered Species Act; and a list of measures being taken to reduce impacts to the species during Project construction and implementation. A fact sheet conveying

this information shall be prepared for distribution to the previously referenced people and anyone else who may enter the Project sites.

- 11. Upon completion of the Project, all areas subject to temporary ground disturbances, including storage and staging areas, temporary roads, pipeline corridors, etc. shall be recontoured if necessary, and revegetated to promote restoration of the area to pre-Project conditions. An area subject to "temporary" disturbance means any area that is disturbed during the Project, but after Project completion will not be subject to further disturbance and has the potential to be revegetated. Appropriate methods and plant species used to revegetate such areas should be determined on a site-specific basis in consultation with the USFWS, CDFW, and revegetation experts.
- 12. In the case of trapped animals, escape ramps or structures should be installed immediately to allow the animal(s) to escape, or the USFWS shall be contacted for guidance.
- 13. Any contractor, employee, or military or agency personnel who are responsible for inadvertently killing or injuring a San Joaquin kit fox shall immediately report the incident to their representative. This representative shall contact the CDFW immediately in the case of a dead, injured or entrapped kit fox. The CDFW contact for immediate assistance is State Dispatch at (916)445-0045. They will contact the local warden or CDFW representative, the wildlife biologist, at (530)934-9309. The USFWS shall be contacted at the numbers below.
- 14. The Sacramento Fish and Wildlife Office of USFWS and CDFW shall be notified in writing within three working days of the accidental death or injury to a San Joaquin kit fox during Project-related activities. Notification must include the date, time, and location of the incident or of the finding of a dead or injured animal and any other pertinent information. The USFWS contact is the Chief of the Division of Endangered Species, at the addresses and telephone numbers below. The CDFW contact can be reached at 1701 Nimbus Road, Suite A, Rancho Cordova, California 95670, (530) 934-9309.
- 15. All sightings of the San Joaquin kit fox shall be reported to the California Natural Diversity Database (CNDDB). A copy of the reporting form and a topographic map clearly marked with the location of where the kit fox was observed shall also be provided to the Service at the address below.

Any Project-related information required by the USFWS or questions concerning the above conditions or their implementation may be directed in writing to the U.S. Fish and Wildlife Service at: Endangered Species Division, 2800 Cottage Way, Suite W 2605, Sacramento, California 95825-1846, phone (916) 414-6620 or (916) 414-6600.

**MM 3.9.1:** If prehistoric or historic-era cultural or archaeological materials are encountered during construction activities, all work within 25 feet of the find shall halt until a qualified professional archaeologist, meeting the Secretary of the Interior's Professional Qualification Standards for prehistoric and historic archaeologist, can evaluate the significance of the find

and make recommendations. Cultural resource materials may include prehistoric resources such as flaked and ground stone tools and debris, shell, bone, ceramics, and fire-affected rock as well as historic resources such as glass, metal, wood, brick, or structural remnants. If the qualified professional archaeologist determines that the discovery represents a potentially significant cultural resource, additional investigations may be required to mitigate adverse impacts from Project implementation. These additional studies may include avoidance, testing, and evaluation or data recovery excavation.

If a potentially-eligible resource is encountered, then the qualified professional archaeologist, the Lead Agency, and the Project proponent shall arrange for either 1) total avoidance of the resource or 2) test excavations to evaluate eligibility and, if eligible, total data recovery. The determination shall be formally documented in writing and submitted to the Lead Agency as verification that the provisions for managing unanticipated discoveries have been met.

**MM 3.9.2:** During any ground disturbance activities, if paleontological resources are encountered, all work within 25 feet of the find shall halt until a qualified paleontologist as defined by the Society of Vertebrate Paleontology Standard Procedures for the Assessment and Mitigation of Adverse Impacts to Paleontological Resources (2010), can evaluate the find and make recommendations regarding treatment. Paleontological resource materials may include resources such as fossils, plant impressions, or animal tracks preserved in rock. The qualified paleontologist shall contact the University of California Museum of Paleontology, or other appropriate facility regarding any discoveries of paleontological resources.

If the qualified paleontologist determines that the discovery represents a potentially significant paleontological resource, additional investigations and fossil recovery may be required to mitigate adverse impacts from Project implementation. If avoidance is not feasible, the paleontological resources shall be evaluated for their significance. If the resources are not significant, avoidance is not necessary. If the resources are significant, they shall be avoided to ensure no adverse effects, or such effects must be mitigated. Construction in that area shall not resume until the resource appropriate measures are recommended or the materials are determined to be less than significant. If the resource is significant and fossil recovery is the identified form of treatment, then the fossil shall be deposited in an accredited and permanent scientific institution. Copies of all correspondence and reports shall be submitted to the Lead Agency.

Construction in that area shall not resume until the resource appropriate measures are recommended or the materials are determined to be less than significant. If the resource is significant and fossil recovery is the identified form of treatment, then the fossil shall be deposited in an accredited and permanent scientific institution. Copies of all correspondence and reports shall be submitted to the Lead Agency.

**MM 3.9.3:** If human remains are discovered during construction or operational activities, further excavation or disturbance shall be prohibited pursuant to Section 7050.5 of the California Health and Safety Code. The protocol, guidelines, and channels of communication outlined by the Native American Heritage Commission, in accordance with Section 7050.5 of

the Health and Safety Code, Section 5097.98 of the Public Resources Code (Chapter 1492, Statutes of 1982, Senate Bill 297), and Senate Bill 447 (Chapter 44, Statutes of 1987), shall be followed. Section 7050.5(c) shall guide any potential Native American involvement, in the event of discovery of human remains, at the direction of the county coroner.

MM 3.12.1: Prior to ground-disturbing activities, the City shall prepare and implement a Stormwater Pollution Prevention Plan (SWPPP) that specifies best management practices (BMP), with the intent of keeping all products of erosion from moving offsite. The SWPPP shall include contain a site map that shows the construction site perimeter, existing and proposed man-made facilities, stormwater collection and discharge points, general topography both before and after construction, and drainage patterns across the Project site. Additionally, the SWPPP shall contain a visual monitoring program and a chemical monitoring program for non-visible pollutants to be implemented (if there is a failure of best management practices). The requirements of the SWPPP and BMPs shall be incorporated into design specifications and construction contracts. Recommended best management practices for the construction phase may include the following:

- Stockpiling and disposing of demolition debris, concrete, and soil properly.
- Protecting any existing storm drain inlets and stabilizing disturbed areas.
- Implementing erosion controls.
- Properly managing construction materials.
- Managing waste, aggressively controlling litter, and implementing sediment controls.

Administrative Draft Introduction

#### **SECTION 1 - Introduction**

#### 1.1 - Overview

The Project is the General Plan Amendment/Zone Change of a 20-acre parcel in southern Lemoore to accommodate the development of 134 residential lots.

#### 1.2 - CEQA Requirements

The City of Lemoore is the Lead Agency for this Project pursuant to the CEQA Guidelines (Public Resources Code Section 15000 et seq.). The Environmental Checklist (CEQA Guidelines Appendix G) or Initial Study (IS) (see *Section 3 – Initial Study*) provides analysis that examines the potential environmental effects of the construction and operation of the Project. Section 15063 of the CEQA Guidelines requires the Lead Agency to prepare an IS to determine whether a discretionary project will have a significant effect on the environment. A Mitigated Negative Declaration (MND) is appropriate when an IS has been prepared and a determination can be made that no significant environmental effects will occur because revisions to the Project have been made or mitigation measures will be implemented that reduce all potentially significant impacts to less-than-significant levels. The content of a MND is the same as a Negative Declaration, with the addition of identified mitigation measures and a Mitigation Monitoring and Reporting Program (MMRP) (see *Appendix A – Mitigation Monitoring and Reporting Program*).

Based on the IS, the Lead Agency has determined that the environmental review for the proposed application can be completed with a MND.

#### 1.3 - Impact Terminology

The following terminology is used to describe the level of significance of project environmental impacts.

- A finding of "no impact" is appropriate if the analysis concludes that the project would not affect a topic area in any way.
- An impact is considered "less than significant" if the analysis concludes that it would cause no substantial adverse change to the environment and requires no mitigation.
- An impact is considered "less than significant with mitigation incorporated" if the analysis concludes that it would cause no substantial adverse change to the environment with the inclusion of environmental commitments that have been agreed to by the proponent.
- An impact is considered "potentially significant" if the analysis concludes that it could have a substantial adverse effect on the environment.

Administrative Draft Introduction

#### 1.4 - Document Organization and Contents

The content and format of this IS/MND is designed to meet the requirements of CEQA. The report contains the following sections:

- Section 1 Introduction: This section provides an overview of CEQA requirements, intended uses of the IS/MND, document organization, and a list of regulations that have been incorporated by reference.
- *Section 2 Project Description:* This section describes the Project and provides data on the site's location.
- Section 3 Environmental Checklist: This chapter contains the evaluation of 18 different environmental resource factors contained in Appendix G of the CEQA Guidelines. Each environmental resource factor is analyzed to determine whether the proposed Project would have an impact. One of four findings is made for each factor, which include: no impact, less-than-significant impact, less than significant with mitigation, or significant and unavoidable. If the evaluation results in a finding of significant and unavoidable for any of the 18 environmental resource factors, then an Environmental Impact Report will be required.
- *Section 4 References:* This chapter contains a full list of references that were used in the preparation of this IS/MND.
- Appendix A Mitigation Monitoring and Reporting Program: This appendix contains the Mitigation Monitoring and Reporting Program.
- Appendix B CalEEMod Results: This appendix contains the 2005 (base) and 2019 (estimated opening day) readouts from the California Emissions Estimator Model.

#### **SECTION 2 - PROJECT DESCRIPTION**

#### 2.1 - Introduction

The Project is the General Plan Amendment/Zone Change (GPA/ZC) of a 20-acre parcel in southern Lemoore to accommodate the development of 134 residential lots.

#### 2.2 - Project Location

The site consists of a 20-acre parcel (APN 023-320-005) located at the northeast corner of State Route 198 and State Route 41, south of San Simeon Drive and west of Arcata Avenue in south-central Lemoore. The site is in Section 9, Township 19 South, Range 20 East, Mount Diablo Base and Meridian (MDB&M) within the Lemoore United States Geological Survey (USGS) 7.5-minute topographic quadrangle. Figure 2-2 and Figure 2-3 provide a regional vicinity and location map of the Project site, respectively.

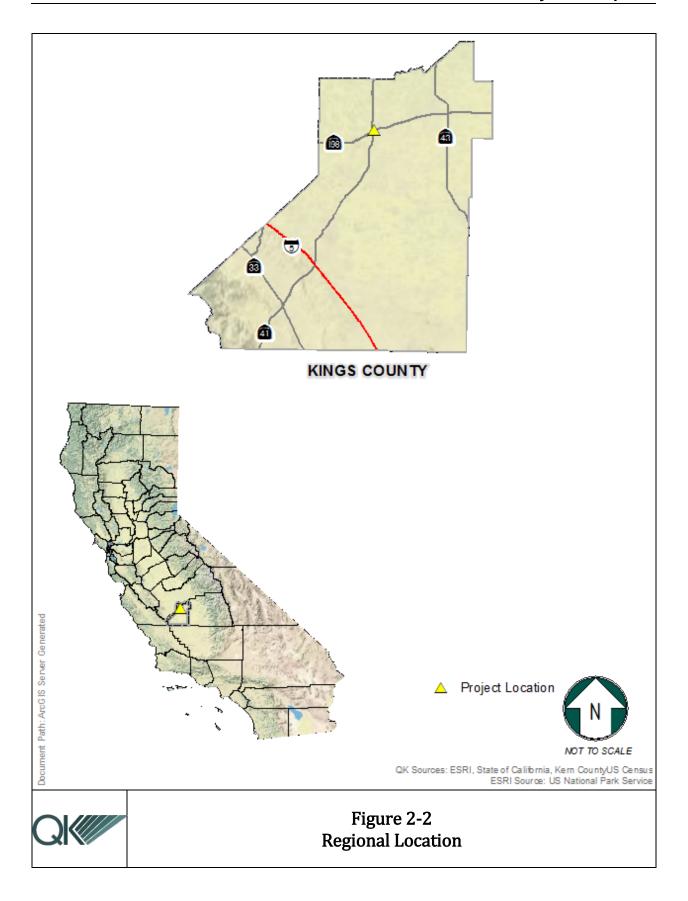
#### 2.3 - Surrounding Land Uses

The area surrounding the proposed site consists of a single-family residential to the north, Highway 41 to the west, Highway 198 to the east and the Highway 41 Interchange to the south. Beyond Highway 41 to the west are more single-family residences and beyond Highway 198 to the southeast are a variety of uses including Bennett Farm Supply, Motel 6, Valero gas station, McCann and Sons Truck and Tractor Service and Master Storage. Land uses and development surrounding the subdivision site are depicted on Figure 2-4.

#### 2.4 - Proposed Project

The proposed Project is the development of up to 134 residential lots on a 20-acre parcel in southern Lemoore (Figure 2-1). The City actions required to permit the Project include a GPA/ZC, major site plan review, and a tentative subdivision map. Currently, the site, is a vacant undeveloped lot. The request by Daley Homes is to reestablish the Low Density Residential and Low Medium Density Residential land use designation on the proposed undesignated site, and reestablish the Low-Medium Density Residential (RLMD) and the Low Density Residential (RLD) zones on the proposed unzoned site.





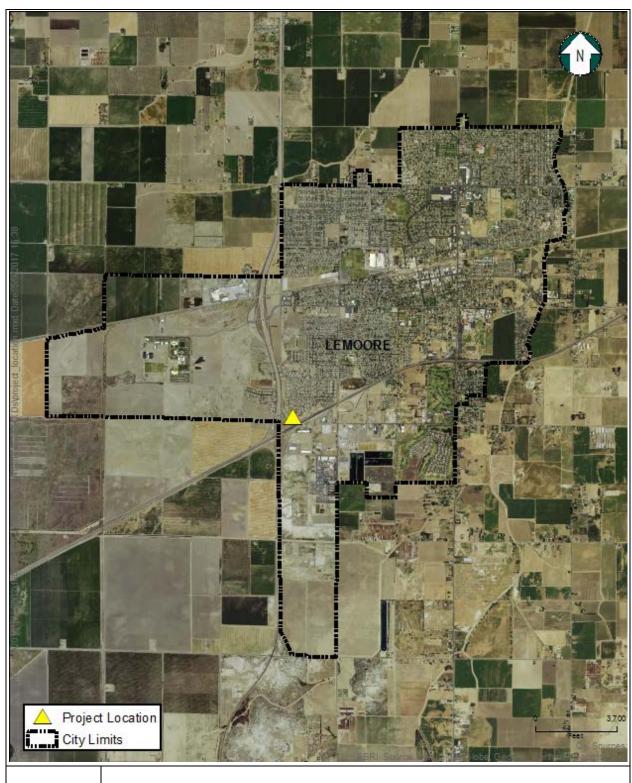




Figure 2-3
Project Location in City



#### **SECTION 3 - EVALUATION OF ENVIRONMENTAL IMPACTS**

#### 3.1 - Environmental Checklist and Discussion

#### 1. Project Title:

Daley Homes General Plan Amendment and Zone Change

#### 2. Lead Agency Name and Address:

City of Lemoore 119 Fox Street Lemoore, CA 93245

#### 3. Contact Person and Phone Number:

Judy Holwell, Development Services Director (559) 924-6740

#### 4. Project Location:

The site consists of a 20-acre parcel (APN 023-320-005) located at the northeast corner of State Route 198 and State Route 41, south of San Simeon Drive and west of Arcata Avenue in south-central Lemoore.

#### 5. Project Sponsor's Name and Address:

Scott Daley, Vice President Daley Homes 1356 E. Tulare Ave. Tulare, CA 93274 (559) 686-1761

#### 6. General Plan Designation:

Low Density Residential and Low Medium Density Residential

#### 7. Zoning:

Low-Medium Density Residential (RLMD) and Low Density Residential (RLD)

#### 8. Description of Project:

See Section 2.4 - Proposed Project.

#### 9. Surrounding Land Uses and Setting:

See *Section 2.3 – Surrounding Land Uses* and Figures 2-4.

10. Other Public Agencies Approval Required:

None.

11. Have California Native American tribes traditionally and culturally affiliated with the project area requested consultation pursuant to Public Resources Code section 21080.3.1? If so, has consultation begun?

Yes, the Santa Rosa Rancheria Tachi Tribe has requested consultation with the City of Lemoore. Letters were sent to the tribe on May 9, 2017, informing them of the Project.

NOTE: Conducting consultation early in the CEQA process allows tribal governments, lead agencies, and project proponents to discuss the level of environmental review, identify and address potential adverse impacts to tribal cultural resources, and reduce the potential for delay and conflict in the environmental review process. (See Public Resources Code section 21083.3.2.) Information may also be available from the California Native American Heritage Commission's Sacred Lands File per Public Resources Code section 5097.96 and the California Historical Resources Information System administered by the California Office of Historic Preservation. Please also note that Public Resources Code section 21082.3(c) contains provisions specific to confidentiality.

#### 3.2 - Environmental Factors Potentially Affected:

involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages. Aesthetics Agriculture and Forest Air Quality Resources **Biological Resources** Cultural Resources Geology /Soils Greenhouse Gas Hazards & Hazardous ☐ Hydrology / Water **Emissions** Materials Quality Land Use/Planning Mineral Resources Noise Population/Housing | Public Services Recreation Transportation/Traffic Utilities / Service Findings of Significance Systems 3.3 - Determination On the basis of this initial evaluation: I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.  $\boxtimes$ I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared. I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required. I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect (a) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and (b) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENT IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed. I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable

The environmental factors checked below would be potentially affected by this project,

standards, and (b) have be-	en avoided or mitigated pursuant to that earlier EIR or
NEGATIVE DECLARATION,	including revisions or mitigation measures that are
imposed upon the proposed	project, nothing further is required.
/ss	May 22, 2017

Judy Holwell, Development Services Director

Date

# 3.4 - Evaluation of Environmental Impacts

- 1. A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
- 2. All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
- 3. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect may be significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
- 4. "Negative Declaration: Less Than Significant With Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less Than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from "Earlier Analyses," as described in (5) below, may be cross-referenced).
- 5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:
  - a. Earlier Analysis Used. Identify and state where they are available for review.
  - b. Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
  - c. Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.

- 6. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.
- 7. Supporting Information Sources: A source list should be attached, and other sources used or individuals contacted should be cited in the discussion.
- 8. This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whatever format is selected.
- 9. The explanation of each issue should identify:
  - a. the significance criteria or threshold, if any, used to evaluate each question; and
  - b. the mitigation measure identified, if any, to reduce the impact to less than significance.

		Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less-than- Significant Impact	No Impact
3.5	- Aesthetics				
Woul	d the project:				
a.	Have a substantial adverse effect on a scenic vista?				
b.	Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?				
C.	Substantially degrade the existing visual character or quality of the site and its surroundings?				
d.	Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?				

Response: a) The Project site is located adjacent to the Highway 41 and 198 interchange to the south and single-family residential developments to the north. The City of Lemoore 2030 General Plan states there are currently no buildings or structures listed in the National Register of Historic Places or as California Historic Landmarks. However, there are 37 sites listed as having local historic significance located within the downtown district (City of Lemoore, 2008). There are no local historic resources within the vicinity of the Project site. The Project is not located in an area that would result in substantial adverse effects on any scenic vistas and no impact would occur.

**Mitigation Measures:** No mitigation is required.

**Conclusion:** There would be *no impact.* 

**Response:** b), c) There are no listed State scenic highways within Kings County; therefore, the site would not damage scenic resources within a state scenic highway (Caltrans, 2017). As discussed, the proposed development is consistent with the existing character and uses of the surrounding area. There would be no substantial degrade to the existing visual character or quality of the site and its surroundings.

**Mitigation Measures:** No mitigation is required.

**Response:** d) The proposed development would comply with all lighting standards established in the City's Zoning Ordinance (Title 9, Chapter 5, Article B, Section 4). There would be no impact.

**Mitigation Measures:** No mitigation is required.

Less than
Significant
Potentially with Less-thanSignificant Mitigation Significant No
Impact Incorporated Impact Impact

# 3.6 - Agriculture and Forestry Resources

In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment Project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. Would the project:

a.	Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to nonagricultural use?		
b.	Conflict with existing zoning for agricultural use or a Williamson Act Contract?		
c.	Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?		
d.	Result in the loss of forest land or conversion of forest land to non-forest use?		$\boxtimes$
e.	Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?		

**Response:** a), b), c), d), e) There will not be any conversion of farmland, nor conflict with any existing zoning for agricultural use or forest land, or Williamson Act contracts. The proposed Project site is classified as "urban and built-up land" by the Department of Conservation's Farmland Mapping and Monitoring Program (FMMP). The site is an undeveloped-vacant urban parcel.

**Mitigation Measures:** None are required.

		Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less-than- Significant Impact	No Impact
3.7	' - Air Quality				
	re available, the significance criteria established l rol district may be relied upon to make the follov			-	pollution
a.	Conflict with or obstruct implementation of the applicable air quality plan?			$\boxtimes$	
b.	Violate any air quality standard or contribute substantially to an existing or projected air quality violation?				
c.	Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions				

The proposed Project is located within the San Joaquin Valley Air Basin (SJVAB). The proposed Project consists of the development and operation of 134 residential lot subdivision.

П

## Small Project Analysis Level (SPAL)

ozone precursors)?

pollutant concentrations?

substantial number of people?

d.

e.

which exceed quantitative thresholds for

Expose sensitive receptors to substantial

Create objectionable odors affecting a

The District has established thresholds of significance for criteria pollutant emissions, which are based on District New Source Review (NSR) offset requirements for stationary sources. Using project type and size, the District has pre-qualified emissions and determined a size below which it is reasonable to conclude that a project would not exceed applicable thresholds of significance for criteria pollutants. In the interest of streamlining CEQA requirements, projects that fit the descriptions and project sizes provided below are deemed to have a less than significant impact on air quality and as such are excluded from quantifying criteria pollutant emissions for CEQA purposes. Table 3-1 below shows the SPAL thresholds for single-family projects.

 $\boxtimes$ 

 $\boxtimes$ 

Table 3-1 SPAL Thresholds – Single Family

Vehicle Trip Thresholds	Project Type Thresholds
Single Family – 1,453 trips/day	Single Family – 134 units

**Response:** a) The project includes the division of 20 gross acres to create 134 residential lots. The ultimate build out of these lots would consist of up to 134 residential lots and is projected to generate approximately 1,282 additional daily trips (at a worst-case scenario) within the existing residential area (134 lots X 9.57 average trips per household). Therefore, the project qualifies as a SPAL and is deemed to have a less than significant impact on air quality.

**Mitigation Measures:** None are required.

**Conclusion:** Impacts would be *less than significant*.

**Response:** b) There are two pollutants of concern for this impact: CO and localized PM10. The proposed Project would not result in localized CO hotspots or PM 10 impacts as discussed below. Therefore, the proposed Project would not violate an air quality standard or contribute to a violation of an air quality standard in the Project area.

#### Localized PM10

Localized PM10 would be generated by Project construction activities, which would include earth-disturbing activities. The proposed Project would comply with the SJVAPCD's Regulation VIII dust control requirements during construction. Compliance with this regulation would reduce the potential for significant localized PM10 impacts to less than significant levels.

#### **CO Hotspot**

Localized high levels of CO are associated with traffic congestion and idling or slow-moving vehicles. The SJVAPCD provides screening criteria to determine when to quantify local CO concentrations based on impacts to the level of service (LOS) of roadways in the Project vicinity.

This proposed Project would result in the division of 20 gross acres to create 134 residential lots. Construction of the proposed Project would result in minor-temporary increases in traffic for the surrounding road network during the construction period and an estimated 1,282 daily trips (134 lots x 9.57 average trips per household) during the operation, which is the worst-case scenario. The minor increase in trips would not substantially lower the LOS. Therefore, the Project would not generate, or substantially contribute to, additional traffic that would exceed State or federal CO standards.

**Mitigation Measures:** None are required.

**Conclusion:** There would be *no impact.* 

**Response:** c) The SJVAPCD does not have quantifiable thresholds for analyzing a project's cumulative impacts on air quality. As previously determined, the project will have a less than significant impact on air quality since it qualified as a SPAL. Since a majority of the surrounding land is developed, there are not many opportunities for new development to occur in the future. Therefore, the project plus future projects combined, will not create a cumulatively considerable increase in criteria pollutants.

**Mitigation Measures:** None are required.

**Conclusion:** Impacts would be *less than significant.* 

**Response:** d) The proposed Project is consistent with the surrounding land uses and would not create or expose sensitive receptors to substantial pollutant concentrations or emissions (Figure 2-4).

Mitigation Measures: None are required.

**Conclusion:** There would be *no impact.* 

**Response:** e) According to the 2015 SJVAPCD's GAMAQI, analysis of potential odor impacts should be conducted for the following two situations:

- Generators projects that would potentially generate odorous emissions proposed to locate near existing sensitive receptors or other land uses where people may congregate; and
- Receivers residential or other sensitive receptor projects or other projects built for the intent of attracting people locating near existing odor sources.

The proposed Project does not meet any of these two criteria.

**Mitigation Measures:** None are required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less-than- Significant Impact	No Impact
3.8 - Biological Resources				
Would the project:				
a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special-status species in local or regional plans, policies, or regulations or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?				
b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?				
c. Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?				
d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species, or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?				
e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?				
f. Conflict with the provisions of an adopted habitat conservation plan, natural community conservation plan, or other approved local, regional, or state habitat conservation plan?				

**Methodology:** Database searches were conducted to determine which sensitive biological resources historically occurred on and within 10 miles of the Project site. The California Natural Diversity Database (CNDDB) (CNDDB 2017), California Native Plants Society (CNPS)

database (CNPS 2017), U.S. Fish and Wildlife Service (USFWS) Threatened and Endangered Species List (USFWS 2017a), and USFWS Critical Habitat database (USFWS 2017b) were reviewed to identify State and federal special-status species were searched. The CNDDB provides element-specific spatial information on individual documented occurrences of special-status species and sensitive natural vegetation communities. The CNPS database provides similar information specific to plant species, but at a much lower spatial resolution. The USFWS query generates a list of federally-protected species known to potentially occur within individual USGS quadrangles. Wildlife species designated as "Fully Protected" by California Fish and Game Code Sections 5050 (Fully Protected reptiles and amphibians), 3511 (Fully Protected birds), 5515 (Full Protected Fish), and 4700 (Fully Protected mammals) are added to the list.

Additional databases that were accessed included the USFWS National Wetlands Inventory (NWI) Map (NWI 2017), the USGS topographical maps, National Hydrography Dataset (NHD) (NHD 2017), Federal Emergency Management Agency (FEMA) 100-year floodplain database (FEMA 2017), and the Recovery Plan for Upland Species of the San Joaquin Valley and Essential Connectivity Habitat Areas for wildlife corridors (Spencer 2010).

**Response:** a), b) The CNDDB searches listed historical occurrences of five special-status bird species, three special-status plant species, nine special-status wildlife species and one sensitive natural community within a 10-mile buffer around the Project site (Figure 3-1 through Figure 3-4). However, none of these records occurred on or within the immediate vicinity of the Project site.

No USFWS-designated Critical Habitat units occur on the Project site. Critical Habitat for the Buena Vista Lake ornate Shrew (*Sorex ornatus relictus*) is over five miles southwest of the site (Figure 3-5). Riparian habitats are defined as vegetative communities that are influenced by a river or stream, specifically the land area that encompasses the water channel and its current or potential floodplain. No riparian habitat occurs on or near the Project site. No sensitive natural communities or critical habitats occur on or near the Project site.

The proposed Project site is frequently disked and surrounded by residential urban uses to the north and bordered by Highway 41 and 198 to the west, east and south. The potential for special-status species to occur on the site is low; however, a pre-construction survey would need to be completed to ensure there is no evidence of occupation by special-status species on the Project site. General mitigation measures are included to prevent any potential impacts during construction. Therefore, there would be a less-than-significant impact with mitigation incorporated.

### **Mitigation Measures:**

**MM 3.8.1:** A qualified biologist shall conduct a pre-construction survey on the Project site and within 500 feet of its perimeter within 14 days and no more than 30 days prior to the start of construction activities.

If any evidence of occupation of the Project site by listed or other special-status species is subsequently observed, a buffer shall be established by a qualified biologist that results in sufficient avoidance to comply with applicable regulations. If sufficient avoidance cannot be established, the United States Fish and Wildlife Service and California Department of Fish and Game shall be contacted for further guidance and consultation on additional measures. The Project proponent shall obtain any required permits from the appropriate wildlife agency. Copies of all permits and evidence of compliance with applicable regulations shall be submitted to the lead agency.

The following buffer distances shall be established prior to construction activities:

- San Joaquin kit fox or American badger potential den: 50 feet;
- San Joaquin kit fox known den: 100 feet;
- San Joaquin kit fox or American badger pupping den: contact the California Department of Fish and Game and United States Fish and Wildlife Service;
- Burrowing owl burrow outside of breeding season: 160 feet;
- Burrowing owl burrow during breeding season: 250 feet;
- Swainson's hawk nest during breeding season: ½ mile;
- Other protected raptor nests during the breeding season: 300 feet;
- Other protected nesting migratory bird nests during the breeding season: 50 feet; and
- Other special-status wildlife species: as recommended by qualified biologist.

**MM 3.8.2:** If initial grading activities are planned during the potential nesting season for migratory birds/raptors that may nest on or near the Project site, the preconstruction survey shall evaluate the sites and accessible lands within an adequate buffer for active nests of migratory birds/raptors. If any nesting birds/raptors are observed, a qualified biologist shall determine buffer distances and/or the timing of Project activities so that the proposed Project does not cause nest abandonment or destruction of eggs or young. This measure shall be implemented so that the proposed Project remains in compliance with the Migratory Bird Treaty Act and applicable state regulations.

If nesting raptors are identified during the surveys, active raptor nests should be avoided by 500 feet and all other migratory bird nests should be avoided by 250 feet. Avoidance buffers may be reduced if a qualified and approved on-site monitor determines that encroachment into the buffer area is not affecting nest building, the rearing of young, or otherwise affect the breeding behaviors of the resident birds. Avoidance buffers can also be reduced through consultation with the CDFW and USFWS. If Swainson's hawks are found to nest within the survey area, active Swainson's hawk nests shall be avoided by 0.5 mile unless this avoidance buffer is reduced through consultation with the CDFW and/or USFWS.

No construction or earth-moving activity shall occur within a non-disturbance buffer until it is determined by a qualified biologist that the young have fledged (that is, left the nest) and have attained sufficient flight skills to avoid Project construction areas. This typically occurs by early July, but September 1st is considered the end of the nesting period unless otherwise determined by a qualified biologist. Once raptors have completed nesting and young have fledged, disturbance buffers will no longer be needed and can be removed, and monitoring can be terminated.

**MM 3.8.3:** If any burrowing owl burrows are observed during the preconstruction survey, avoidance measures shall be consistent and in accordance with protocols outlined in the Burrowing Owl Survey Protocol and Mitigation Guidelines (Burrowing Owl Consortium 1993) and the Staff Report on Burrowing Owl Mitigation (CDFW 2012). Active burrows shall be avoided, but if avoidance is not possible then compensation shall be provided for the active or passive displacement of western burrowing owls, and habitat acquisition and the creation of artificial dens for any western burrowing owls shall be provided for any owls relocated from construction areas. These measures are outlined as follows:

- 1. A pre-construction survey of construction area, including a 150-meter buffer (500 feet), shall be conducted no less than 14 days and no more than 30 days prior to ground disturbing activities. If more than 30 days lapse between the time of the pre-construction survey and the start of ground-disturbing activities, another pre-construction survey shall be completed. The second survey (or other subsequent surveys if necessary) shall be conducted and timed to occur sometime between 30 days and 24 hours prior to ground disturbance.
- 2. If western burrowing owls are present on the construction site (or within 500 feet of the construction site), exclusion fencing shall be installed between the nest site or active burrow and any earth-moving activity or other disturbance. Exclusion areas shall extend 160 feet around occupied burrows during the non-breeding season (September 1 through January 31) and extend 250 feet around occupied burrows during the breeding season (February 1 through August 31) as described in The California Burrowing Owl Consortium's Survey Protocol and Mitigation Guidelines (California Burrowing Owl Consortium 1993).
- 3. If western burrowing owls are present in the non-breeding season and must be passively relocated from the Project site, passive relocation shall not commence until October 1st and must be completed by February 1st. Passive relocation must only be conducted by a qualified biologist or ornithologist and with approval by CDFW. After passive relocation, the area where owls occurred and its immediate vicinity shall be monitored by a qualified biologist daily for one week and once per week for an additional two weeks to document that owls are not reoccupying the site.
- 4. If permanent impacts to nesting, occupied and satellite burrows, or burrowing owl habitat occur, compensation shall be based upon the number of owls or pairs of owls relocated from the construction area. Compensation acreage shall be determined as described in the CDFW's Staff Report on Burrowing Owl Mitigation (CDFW 2012).

## **MM 3.8.4:** The measures listed below shall be implemented during construction:

1. Pre-construction surveys shall be conducted no fewer than 14 days and no more than 30 days prior to the beginning of ground disturbance and/or construction activities. If any San Joaquin kit fox dens are found during preconstruction surveys, exclusion zones shall be placed in accordance with USFWS Recommendations using the following:

## San Joaquin kit fox USFWS Exclusion Zone Recommendations

Den Type	Recommendation
Potential Den	50-foot radius
Known Den	100-foot radius
Natal/Pupping Den	Contact U.S. Fish and Wildlife
(Occupied and Unoccupied)	Service for guidance
Atypical Den	50-foot radius

- 2. If any den must be removed, it must be appropriately monitored and excavated by a trained wildlife biologist. Destruction of natal dens and other "known" kit fox dens must not occur until authorized by USFWS. Replacement dens will be required if such dens are removed. Potential dens that are removed do not need to be replaced if they are determined to be inactive by using standard monitoring techniques (e.g., applying tracking medium around the den opening and monitoring for San Joaquin kit fox tracks for three consecutive nights).
- 3. Project-related vehicles shall observe a daytime speed limit of 20-mph throughout the site in all Project areas, except on County roads and State and federal highways; this is particularly important at night when kit foxes and badgers are most active. Night-time construction shall be minimized to the extent possible. However, if construction at night does occur, then the speed limit shall be reduced to 10-mph. Off-road traffic outside of designated Project areas shall be prohibited.
- 4. To prevent inadvertent entrapment of kit foxes or other animals during the construction phase of a Project, all excavated, steep-walled holes or trenches more than 2-feet deep should be covered at the close of each working day by plywood or similar materials. If the trenches cannot be closed, one or more escape ramps constructed of earthen-fill or wooden planks shall be installed. Before such holes or trenches are filled, they shall be thoroughly inspected for trapped animals. If at any time a trapped or injured kit fox is discovered, the USFWS and the CDFW shall be contacted at the addresses provided below.
- 5. Kit foxes are attracted to den-like structures such as pipes and may enter stored pipes and become trapped or injured. All construction pipes, culverts, or similar structures with a diameter of 4-inches or greater that are stored at a construction site for one or more overnight periods shall be thoroughly inspected for kit foxes before the pipe is subsequently buried, capped, or otherwise used or moved in any way. If a kit fox is

discovered inside a pipe, that section of pipe shall not be moved until the USFWS has been consulted. If necessary, and under the direct supervision of the biologist, the pipe may be moved only once to remove it from the path of construction activity, until the fox has escaped.

- 6. All food-related trash items such as wrappers, cans, bottles, and food scraps shall be disposed of in securely closed containers and removed at least once a week from a construction or Project sites.
- 7. No pets, such as dogs or cats, shall be permitted on the Project sites to prevent harassment, mortality of kit foxes, or destruction of dens.
- 8. Use of rodenticides and herbicides in Project areas shall be restricted. This is necessary to prevent primary or secondary poisoning of kit foxes and the depletion of prey populations on which they depend. All uses of such compounds shall observe label and other restrictions mandated by the U.S. Environmental Protection Agency, California Department of Food and Agriculture, and other State and Federal legislation, as well as additional Project-related restrictions deemed necessary by the USFWS. If rodent control must be conducted, zinc phosphide shall be used because of a proven lower risk to kit fox.
- 9. A representative shall be appointed by the Project proponent who will be the contact source for any employee or contractor who might inadvertently kill or injure a kit fox or who finds a dead, injured or entrapped kit fox. The representative will be identified during the employee education program and their name and telephone number shall be provided to the USFWS.
- 10. An employee education program shall be conducted. The program shall consist of a brief presentation by persons knowledgeable in San Joaquin kit fox biology and legislative protection to explain endangered species concerns to contractors, their employees, and military and/or agency personnel involved in the Project. The program shall include: a description of the San Joaquin kit fox and its habitat needs; a report of the occurrence of kit fox in the Project area; an explanation of the status of the species and its protection under the Endangered Species Act; and a list of measures being taken to reduce impacts to the species during Project construction and implementation. A fact sheet conveying this information shall be prepared for distribution to the previously referenced people and anyone else who may enter the Project sites.
- 11. Upon completion of the Project, all areas subject to temporary ground disturbances, including storage and staging areas, temporary roads, pipeline corridors, etc. shall be recontoured if necessary, and revegetated to promote restoration of the area to pre-Project conditions. An area subject to "temporary" disturbance means any area that is disturbed during the Project, but after Project completion will not be subject to further disturbance and has the potential to be revegetated. Appropriate methods and plant species used to revegetate such areas should be determined on a site-specific basis in consultation with the USFWS, CDFW, and revegetation experts.

- 12. In the case of trapped animals, escape ramps or structures should be installed immediately to allow the animal(s) to escape, or the USFWS shall be contacted for guidance.
- 13. Any contractor, employee, or military or agency personnel who are responsible for inadvertently killing or injuring a San Joaquin kit fox shall immediately report the incident to their representative. This representative shall contact the CDFW immediately in the case of a dead, injured or entrapped kit fox. The CDFW contact for immediate assistance is State Dispatch at (916)445-0045. They will contact the local warden or CDFW representative, the wildlife biologist, at (530)934-9309. The USFWS shall be contacted at the numbers below.
- 14. The Sacramento Fish and Wildlife Office of USFWS and CDFW shall be notified in writing within three working days of the accidental death or injury to a San Joaquin kit fox during Project-related activities. Notification must include the date, time, and location of the incident or of the finding of a dead or injured animal and any other pertinent information. The USFWS contact is the Chief of the Division of Endangered Species, at the addresses and telephone numbers below. The CDFW contact can be reached at 1701 Nimbus Road, Suite A, Rancho Cordova, California 95670, (530) 934-9309.
- 15. All sightings of the San Joaquin kit fox shall be reported to the California Natural Diversity Database (CNDDB). A copy of the reporting form and a topographic map clearly marked with the location of where the kit fox was observed shall also be provided to the Service at the address below.

Any Project-related information required by the USFWS or questions concerning the above conditions or their implementation may be directed in writing to the U.S. Fish and Wildlife Service at: Endangered Species Division, 2800 Cottage Way, Suite W 2605, Sacramento, California 95825-1846, phone (916) 414-6620 or (916) 414-6600.

**Conclusion:** Impacts would be *less than significant with mitigation incorporated.* 

**Response:** c) No National Wetlands Inventory (NWI) features or blue-line drainages (as found on USGS topographic maps and in the National Hydrography Dataset) occurred on the Project site (Figure 3-6).

**Mitigation Measures:** None are required.

**Conclusion:** There would be *no impact.* 

Response: d) The proposed Project site does not occur within a known migration route, significant wildlife corridor, or linkage area as identified in the Recovery Plan for Upland Species in the San Joaquin Valley (USFWS 1998). The site is located within areas of residential development and highways. Wildlife movement corridors are routes that provide shelter and sufficient food supplies to support regular movements of wildlife species. A movement corridor is a continuous geographic extent of habitat that either spatially or functionally links ecosystems across fragmented, or otherwise inhospitable, landscapes.

Faunal movement may include seasonal or migration movement, life cycle links, species dispersal, re-colonization of an area, and movement in response to external pressures. Movement corridors typically include riparian habitats, ridgelines, and ravines, as well as other contiguous expanses of natural habitats. Movement corridors may be functional on regional, sub-regional, or local scales.

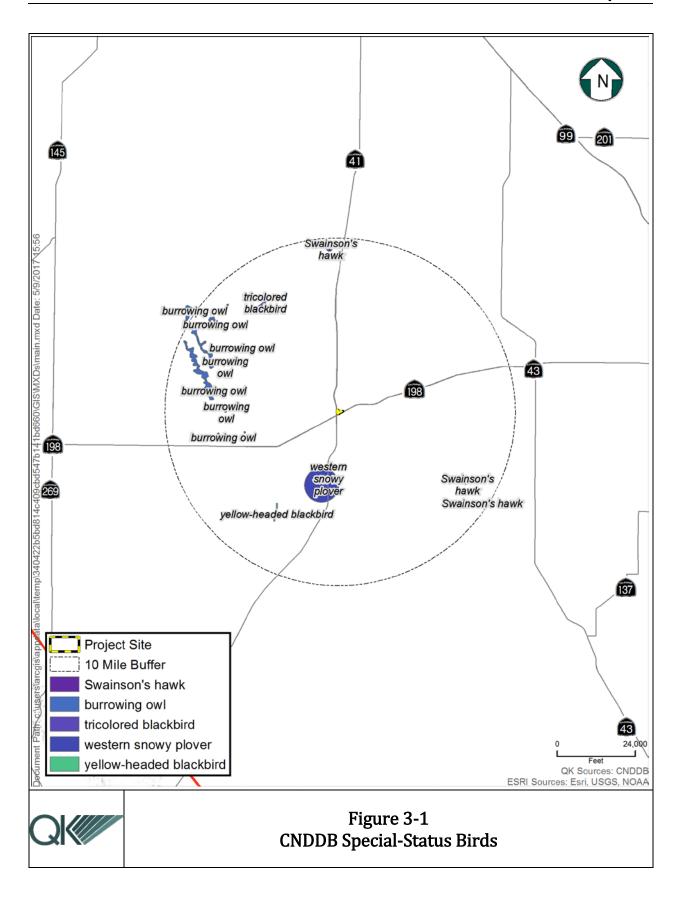
No significant wildlife movement corridors, core areas, or Essential Habitat Connectivity areas occur on or near the Project site. The Project would not substantially affect migrating birds or other wildlife. The Project will not restrict, eliminate, or significantly alter wildlife movement corridors, core areas, or Essential Habitat Connectivity areas either during construction or after the Project has been constructed. Project construction will not substantially interfere with wildlife movements or reduce breeding opportunities.

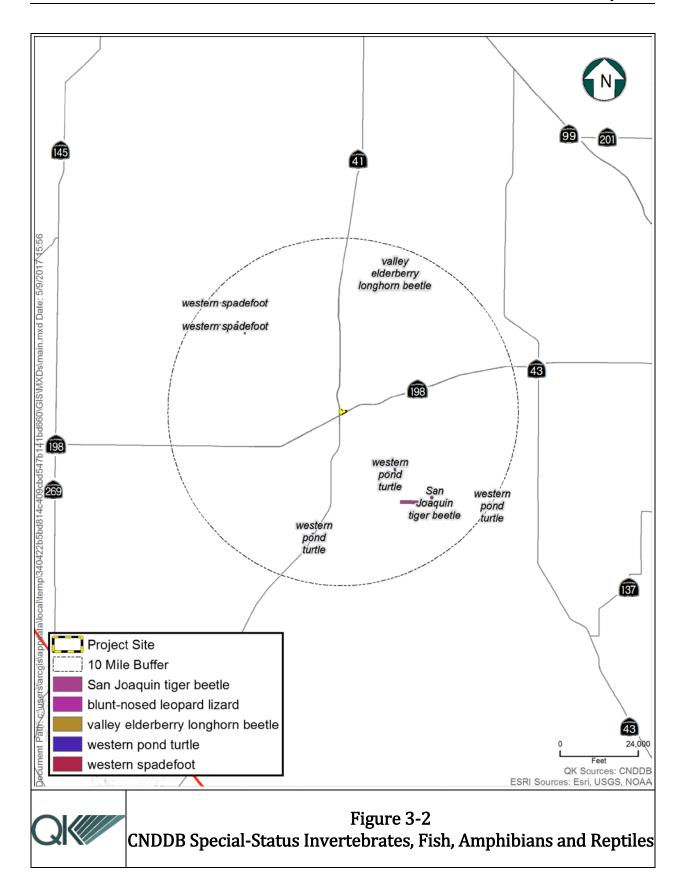
Mitigation Measures: None are required.

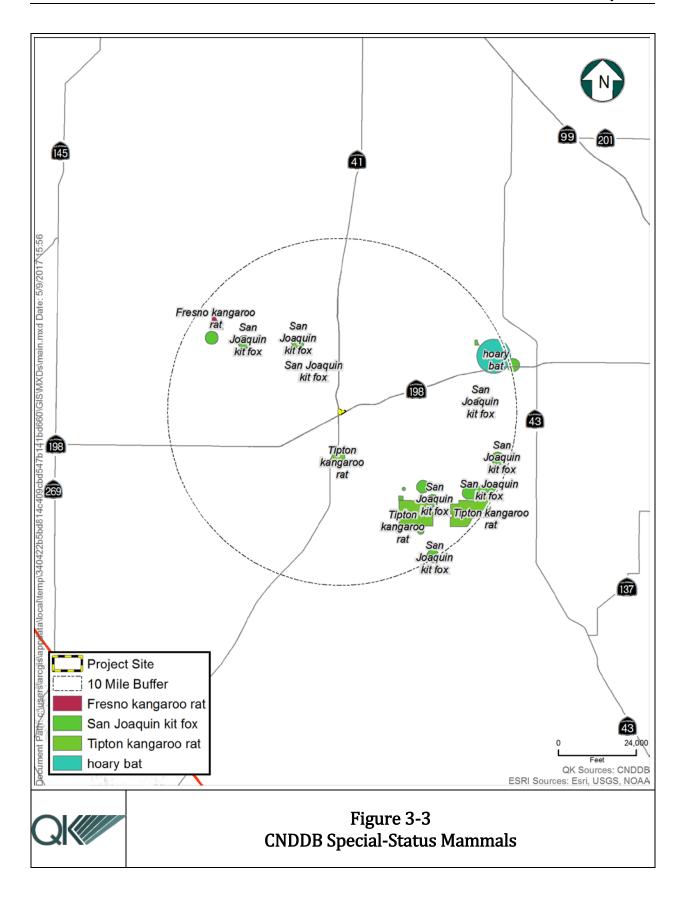
**Conclusion:** There would be *no impact.* 

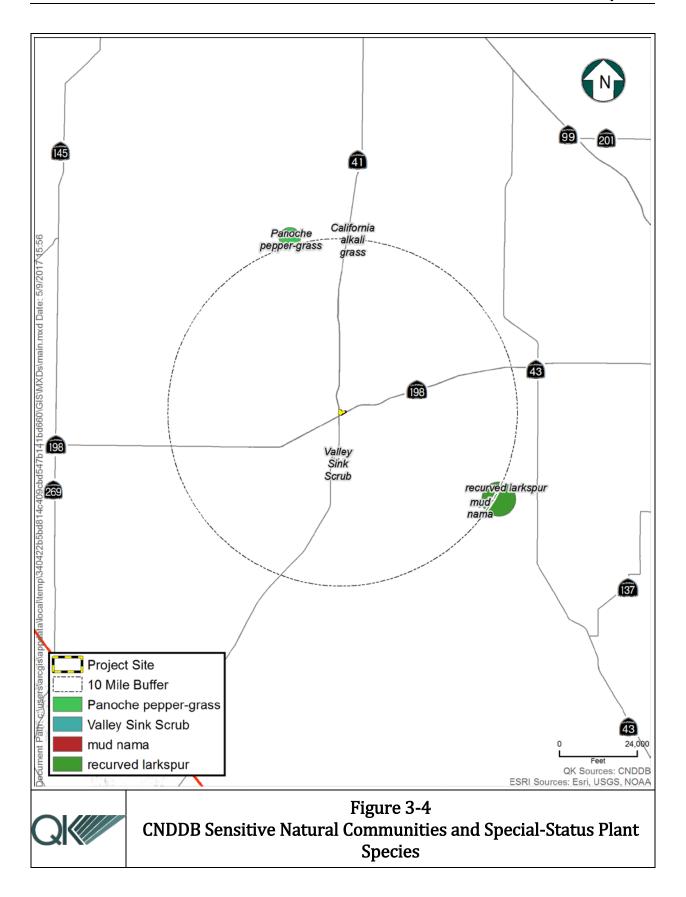
**Response:** e), f) The City of Lemoore does not have any local policies or ordinances protecting biological resources nor an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan. Therefore, there would be no impact.

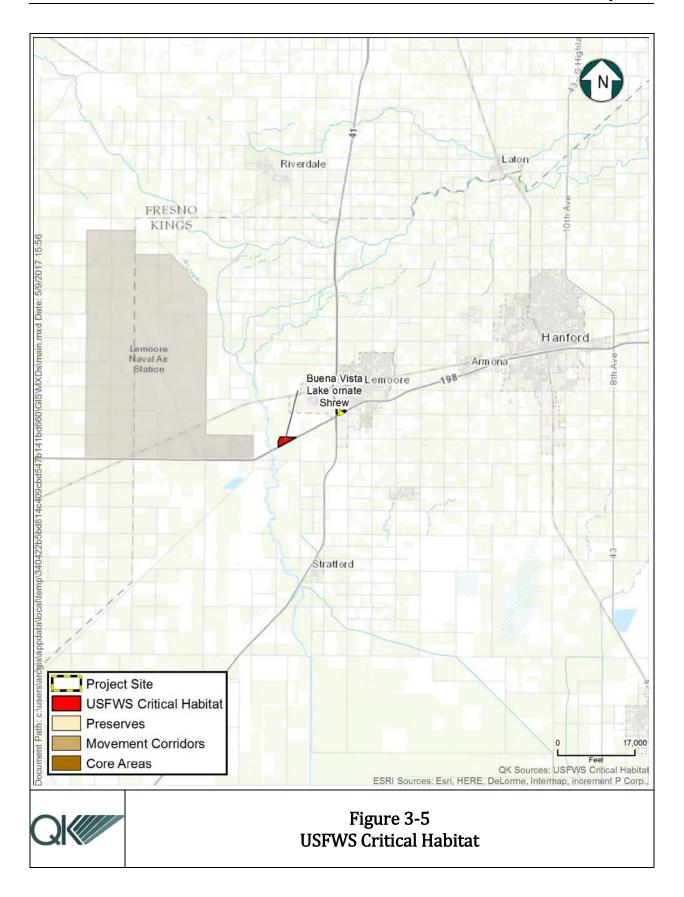
**Mitigation Measures:** None are required.













<i>3</i> .9	9 - Cultural Resources	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less-than- Significant Impact	No Impact
Wo	uld the project:				
a.	Cause a substantial adverse change in the significance of a historical resource as defined in CEQA Guidelines Section 15064.5?				
b.	Cause a substantial adverse change in the significance of an archaeological resource pursuant to CEQA Guidelines Section 15064.5?		$\boxtimes$		
C.	Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?				
d.	Disturb any human remains, including those interred outside of formal cemeteries?		$\boxtimes$		

**Response:** a), b) As discussed in *Section 3.5 – Aesthetics*, there are no identified historical resources within the vicinity of the Project site. There is a low potential for ground-disturbing activities to expose and affect previously unknown significant cultural resources, including historical or prehistorical resources at the Project site. However, there is still a possibility that historical materials may be exposed during construction. Grading and trenching, as well as other ground-disturbing actions, have the potential to damage or destroy these previously unidentified and potentially significant cultural resources within the Project area, including historical resources. Disturbance of any deposits that have the potential to provide significant cultural data would be considered a significant impact under CEQA.

Although considered unlikely since there is no indication of any archaeological resources on or in the vicinity of the Project site, subsurface construction activities associated with the proposed Project could potentially damage or destroy previously undiscovered archaeological resources.

#### **Mitigation Measures:**

**MM 3.9.1:** If prehistoric or historic-era cultural or archaeological materials are encountered during construction activities, all work within 25 feet of the find shall halt until a qualified professional archaeologist, meeting the Secretary of the Interior's Professional Qualification Standards for prehistoric and historic archaeologist, can evaluate the significance of the find and make recommendations. Cultural resource

materials may include prehistoric resources such as flaked and ground stone tools and debris, shell, bone, ceramics, and fire-affected rock as well as historic resources such as glass, metal, wood, brick, or structural remnants. If the qualified professional archaeologist determines that the discovery represents a potentially significant cultural resource, additional investigations may be required to mitigate adverse impacts from Project implementation. These additional studies may include avoidance, testing, and evaluation or data recovery excavation.

If a potentially-eligible resource is encountered, then the qualified professional archaeologist, the Lead Agency, and the Project proponent shall arrange for either 1) total avoidance of the resource or 2) test excavations to evaluate eligibility and, if eligible, total data recovery. The determination shall be formally documented in writing and submitted to the Lead Agency as verification that the provisions for managing unanticipated discoveries have been met.

**Conclusion:** Impacts would be *less than significant with mitigation incorporated.* 

**Response:** c) There are no unique geological features or known fossil-bearing sediments in the vicinity of the Project site. However, there remains the possibility for previously unknown, buried paleontological resources or unique geological sites to be uncovered during subsurface construction activities.

## **Mitigation Measures:**

MM 3.9.2: During any ground disturbance activities, if paleontological resources are encountered, all work within 25 feet of the find shall halt until a qualified paleontologist as defined by the Society of Vertebrate Paleontology Standard Procedures for the Assessment and Mitigation of Adverse Impacts to Paleontological Resources (2010), can evaluate the find and make recommendations regarding treatment. Paleontological resource materials may include resources such as fossils, plant impressions, or animal tracks preserved in rock. The qualified paleontologist shall contact the University of California Museum of Paleontology, or other appropriate facility regarding any discoveries of paleontological resources.

If the qualified paleontologist determines that the discovery represents a potentially significant paleontological resource, additional investigations and fossil recovery may be required to mitigate adverse impacts from Project implementation. If avoidance is not feasible, the paleontological resources shall be evaluated for their significance. If the resources are not significant, avoidance is not necessary. If the resources are significant, they shall be avoided to ensure no adverse effects, or such effects must be mitigated. Construction in that area shall not resume until the resource appropriate measures are recommended or the materials are determined to be less than significant. If the resource is significant and fossil recovery is the identified form of treatment, then the fossil shall be deposited in an accredited and permanent scientific institution. Copies of all correspondence and reports shall be submitted to the Lead Agency.

Construction in that area shall not resume until the resource appropriate measures are recommended or the materials are determined to be less than significant. If the resource is significant and fossil recovery is the identified form of treatment, then the fossil shall be deposited in an accredited and permanent scientific institution. Copies of all correspondence and reports shall be submitted to the Lead Agency.

**Conclusion:** Impacts would be *less than significant with mitigation incorporated.* 

**Response:** d) Human remains including known cemeteries are not known to exist within the Project area. However, construction would involve earth-disturbing activities, and it is still possible that human remains may be discovered, possibly in association with archaeological sites.

## **Mitigation Measures:**

MM 3.9.3: If human remains are discovered during construction or operational activities, further excavation or disturbance shall be prohibited pursuant to Section 7050.5 of the California Health and Safety Code. The protocol, guidelines, and channels of communication outlined by the Native American Heritage Commission, in accordance with Section 7050.5 of the Health and Safety Code, Section 5097.98 of the Public Resources Code (Chapter 1492, Statutes of 1982, Senate Bill 297), and Senate Bill 447 (Chapter 44, Statutes of 1987), shall be followed. Section 7050.5(c) shall guide any potential Native American involvement, in the event of discovery of human remains, at the direction of the county coroner.

**Conclusion:** Impacts would be *less than significant with mitigation incorporated.* 

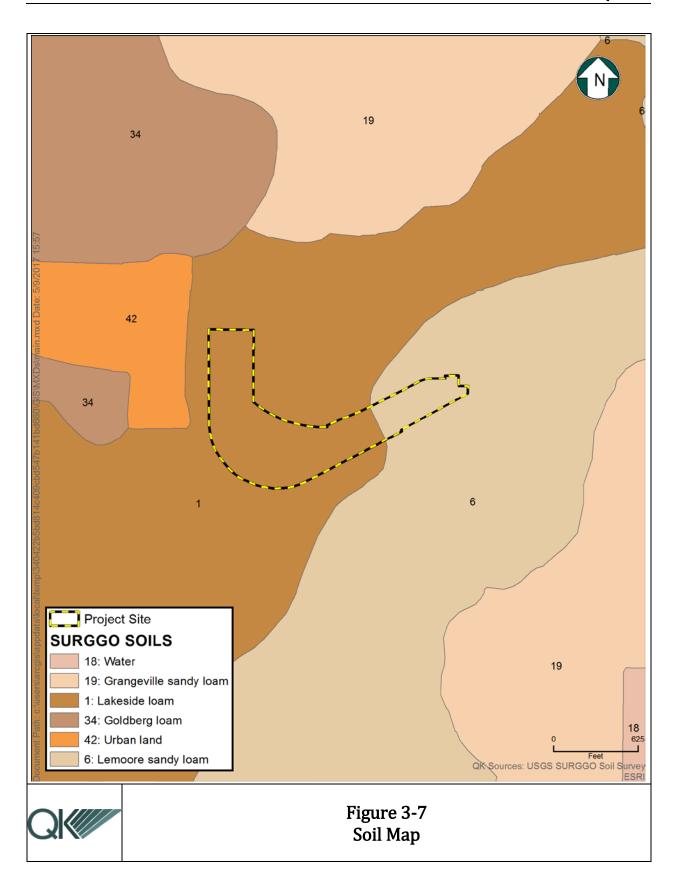
			Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less-than- Significant Impact	No Impact
3.	10 - G	eology and Soils				
Wo	uld the p	project:				
a.	substa	re people or structures to potential antial adverse effects, including the risk s, injury, or death involving:				
	i.	Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.				
	ii.	Strong seismic ground shaking?			$\boxtimes$	
	iii.	Seismic-related ground failure, including liquefaction?				
	iv.	Landslides?				$\boxtimes$
b.	Result topsoi	t in substantial soil erosion or the loss of il?			$\boxtimes$	
c.	unstal result on- o	cated on a geologic unit or soil that is ble, or that would become unstable as a of the project, and potentially result in r offsite landslide, lateral spreading, dence, liquefaction, or collapse?				
d.	Table	cated on expansive soil, as defined in 18-1-B of the Uniform Building Code c), creating substantial risks to life or rty?				
e.	the u waste sewer	soils incapable of adequately supporting use of septic tanks or alternative water disposal systems in areas where is are not available for the disposal of water?				

Response: a), b), c), d), e) There are no known active seismic faults in Kings County or within its immediate vicinity. The principle earthquake hazard affecting the area is ground shaking as opposed to surface rupture or ground failure (City of Lemoore , 2008). Per the Department of Conservation Landslide Map, the City of Lemoore does not contain any areas that are prone to landslides (Department of Conservation, 2017). As shown in Figure 3-1, the site contains Lakeside loam, partially drained and Lemoore sandy loam, partially drained soil. Both soils are very deep, somewhat poorly drained, saline-alkali soils that are mainly used for irrigated crops and urban development (United States Department of Agriculture, 1986). Impacts from soil erosion would be minimal as it most likely occurs on sloped areas and the project site is relatively flat and the site soils contain zero to one percent slopes. Per Table 15 of the Kings County Soil Survey, the site soils have a low shrink-swell potential; therefore, the site does not contain expansive soils (United States Department of Agriculture, 1986). The proposed residential developments would be required to comply with City building code requirements and Lemoore's General Plan policies, and their cited regulations, that mitigate seismic hazards and soils-related structural concerns for permitted development.

The Project site is not located on an unstable geologic unit or soil nor on expansive soil. The proposed Project does not include the development of septic tanks or alternative wastewater disposal systems as the Project would hook up to the City's existing sewer system.

**Mitigation Measures:** None are required.

**Conclusion:** There would be *no impacts and less than significant impacts.* 



		Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less-than- Significant Impact	No Impact
3.2	11 - Greenhouse Gas Emissions				
Wo	uld the project:				
a.	Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?				
b.	Conflict with any applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of greenhouse gases?				

Response: a), b) Greenhouse gas (GHG) significance thresholds are based on the 2014 Kings County Regional Climate Action Plan (CAP). According to the CAP, the AB 32 Scoping Plan encourages local governments to establish a GHG reduction target that "parallels the State's commitment to reduce GHG emissions by approximately 15 percent from current levels by 2020." Therefore, this CAP establishes a reduction target to achieve emissions levels 15 percent below 2005 baseline levels by 2020 consistent with the AB 32 Scoping Plan. Proposed development projects that are consistent with the emission reduction and adaptation measures included in the CAP and the programs that are developed as a result of the CAP, would be considered to have a less than significant cumulative impact on climate change. Therefore, the 15 percent reduction will be used as the significance threshold for GHG emissions for this analysis.

The Project Emissions were calculated using CalEEMod, the SJVAPCD's approved modeling system for quantifying emissions. The results are shown in the Table 3-5 below*

Table 3-5
Project GHG Emissions

	CO2e (tons/year)
Business as Usual (2005)	3,326
Project (2019)	1,958
% reduction	15%
15% reduction met?	YES

^{*}See Appendix B for calculations

**Mitigation Measures:** None are required.

**Conclusion:** Impacts would be *less than significant.* 

		Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less-than- Significant Impact	No Impact
	12 - Hazards and Hazardous aterials				
Woı	ald the project:				
a.	Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?				
b.	Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?				
c.	Emit hazardous emissions or involve handling hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?				
d.	Be located on a site that is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?				
e.	For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?				
f.	For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?				
g.	Impair implementation of, or physically interfere with, an adopted emergency response plan or emergency evacuation plan?				
h.	Expose people or structures to a significant risk of loss, injury, or death involving wildland fires, including where wildlands are				

Less than
Significant
Potentially with
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with Less-than-Mitigation Significant Incorporated Impact

No Impact

adjacent to urbanized areas or where residences are intermixed with wildlands?

Response: a), b), c) There will not be any hazardous material transported to and from the project site, nor utilized thereon after construction. Project construction activities may involve the use of hazardous materials. These materials might include fuels, oils, mechanical fluids, and other chemicals used during construction. The use of such materials would be considered minimal and would not require these materials to be stored in large quantities. There will not be any hazardous material stored in unapproved quantities at the site. Adherence to regulations and standard protocols during storage, transport, and use of hazardous materials would minimize or avoid potential upset and accident conditions involving the release of such materials into the environment.

P.W. Engvall Elementary School is located approximately a half mile northeast of the proposed Project site. The proposed Project would not emit hazardous emissions or involve handling hazardous or acutely hazardous materials, substances, or waste within ¼-mile of an existing school.

**Mitigation Measures:** None are required.

**Conclusion:** Impacts would be *less than significant.* 

d) Per the Cortese List, there are no hazardous waste and substances sites in the vicinity of the Project site (Cal EPA, 2017). Additionally, the State Water Resources Control Board GeoTracker compiles a list of Leaking Underground Storage Tank (LUST) Sites. There are no LUST Cleanup Sites within the vicinity of the Project site (California Water Resources Board, 2017). The proposed Project site is not located on a site that is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and would therefore not create a significant hazard to the public or the environment.

**Mitigation Measures:** None are required.

**Conclusion:** There would be *no impact.* 

e), f) There are two private airstrips and no public airports within the Lemoore area including Reeves Field at the Naval Air Station and Stone Airstrip. There is no adopted airport land use plan for the City of Lemoore. Both are located outside of the City's limits and would not impact the proposed Project.

**Mitigation Measures:** None are required.

g) The City of Lemoore published an Emergency Operations Plan in 2005, which provides guidance to City staff in the event of extraordinary emergency situation associated with natural disaster and technological incidents (City of Lemoore , 2008). The proposed Project would not interfere with the City's adopted emergency response plan; therefore, there would be no impact.

**Mitigation Measures:** None are required.

**Conclusion:** There would be *no impact.* 

h) The proposed Project site is in an unzoned area of the Kings County Fire Hazard Severity Zone Map Local Responsibility Area (LRA). However, Cal Fire has determined that portions of the City of Lemoore are categorized as a Moderate Fire Hazard Severity Zone in LRA. The Project site is not within a wildland area nor is there within the vicinity of the Project site. The Project would not expose people or structures to a significant risk of loss, injury, or death involving wildland fires. Therefore, there would be no impact.

**Mitigation Measures:** None are required.

		Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less-than- Significant Impact	No Impact
3.13 - Hydrology and Water Quality					
Wou	ld the project:				
a.	Violate any water quality standards or waste discharge requirements?				
b.	Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level that would not support existing land uses or planned uses for which permits have been granted)?				
C.	Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner that would result in substantial erosion or siltation on site or off site?				
d.	Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner that would result in flooding on site or off site?				
e.	Create or contribute runoff water that would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?				
f.	Otherwise substantially degrade water quality?				
g.	Place housing within a 100-year flood hazard area as mapped on a federal flood hazard boundary or flood insurance rate map or other flood hazard delineation map?				

Evaluation of Environmental Impacts

 $\boxtimes$ 

**Response:** a), f) Project construction would cause ground disturbance that could result in soil erosion or siltation and subsequent water quality degradation offsite, which is a potentially significant impact. Construction-related activities would also involve the use of materials such as vehicle fuels, lubricating fluids, solvents, and other materials that could result in polluted runoff, which is also a potentially significant impact. However, the potential consequences of any spill or release of these types of materials are generally small due to the localized, short-term nature of such releases because of construction. The volume of any spills would likely be relatively small because the volume in any single vehicle or container would generally be anticipated to be less than 50 gallons.

As required by the State Water Resources Control Board's (SWRCB) National Pollutant Discharge Elimination System (NPDES) General Permit (No. 2012-0006-DWQ) for stormwater discharges associated with construction and land disturbance activities, the City must develop and implement a SWPPP that specifies BMPs to prevent construction pollutants from contacting stormwater, with the intent of keeping all products of erosion from moving offsite. The City is required to comply with the Construction General Permit because Project-related construction activities result in soil disturbances of least 1 one acre of total land area. Mitigation Measure MM HYD-1 below requires the preparation and implementation of a SWPPP to comply with the Construction General Permit requirements.

With implementation of Mitigation Measures MM HYD-1, the Project would not violate any water quality standards or waste discharge requirements (WDRs) during the construction period, and impacts would be less than significant.

Project operation would not violate any water quality standards or WDRs because it: 1) does not result in point-source pollution (e.g., outfall pipe) discharges into surface waters that require WDRs and 2) would be developed in compliance with the General Permit for the Discharge of Stormwater from Small MS4s (No. 2013-0001-DWQ) in which the City is one of the permittees. Operators of MS4s¹, like the City, serve urbanized areas with populations fewer than 100,000. To comply with the MS4 General Permit, the Project would have to comply with City design standards to maximize the reduction of pollutant loading in runoff to the maximum extent practicable. The City Building Department would review grading and

-

Administrative Draft

or mudflow?

j.

failure of a levee or dam?

Contribute to inundation by seiche, tsunami,

¹ MS4s are defined as a conveyance or system of conveyances (including roads with drainage systems, municipal streets, catch basins, curbs, gutters, ditches, man-made channels or storm drains): 1) designed or used for collecting and/or conveying storm water; 2) which is not a combined sewer; and 3) which is not part or a Publicly Owned Treatment Works.

site plans to ensure compliance before approving such plans. The site plan review process ensures that operations of the Project would not violate water quality standards outlined in the MS4 General Permit, and operational impacts would be less than significant.

## **Mitigation Measures:**

MM 3.12.1: Prior to ground-disturbing activities, the City shall prepare and implement a Stormwater Pollution Prevention Plan (SWPPP) that specifies best management practices (BMP), with the intent of keeping all products of erosion from moving offsite. The SWPPP shall include contain a site map that shows the construction site perimeter, existing and proposed man-made facilities, stormwater collection and discharge points, general topography both before and after construction, and drainage patterns across the Project site. Additionally, the SWPPP shall contain a visual monitoring program and a chemical monitoring program for non-visible pollutants to be implemented (if there is a failure of best management practices). The requirements of the SWPPP and BMPs shall be incorporated into design specifications and construction contracts. Recommended best management practices for the construction phase may include the following:

- Stockpiling and disposing of demolition debris, concrete, and soil properly.
- Protecting any existing storm drain inlets and stabilizing disturbed areas.
- Implementing erosion controls.
- Properly managing construction materials.
- Managing waste, aggressively controlling litter, and implementing sediment controls.

#### Conclusion:

Impacts would be *less than significant with mitigation incorporated*.

Response: b) The City of Lemoore currently utilizes local groundwater as its sole source of supply from underground aquifers via ten active groundwater wells. The groundwater basin underlying the City is the Tulare Lake Basin and the City of Lemoore is immediately adjacent to the south boundary of the Kings subbasin. Water for construction and operation would come from the City of Lemoore's existing water system. Per the City's Urban Water Management Plan, the City's existing system has a total supply capacity of 21,674,000 gallons per day with an average day demand of 8,769,000 gallons (City of Lemoore, 2013). The proposed Project would have temporary construction water usage and operation is estimated to demand approximately 40,870 gallons per day requiring 0.19% of the total supply capacity. Since the proposed Project would have minimal impacts on the City's water supply, impacts would be less than significant.

**Mitigation Measures:** None are required.

**Conclusion:** Impacts would be *less than significant*.

**Response:** c), d), e) The Project site is relatively flat and Project grading would be minimal and consist of mostly grubbing the site to remove vegetation. The topography of the site would not appreciably change because of grading activities. The site does not contain any blue-line water features, including streams or rivers. Construction-related erosion and sedimentation impacts as a result of soil disturbance would be less than significant after implementation of a SWPPP (MM 3.12.1). The Project would not substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner that would result in substantial erosion or siltation on- or offsite. Impacts would be less than significant.

Mitigation Measures: None are required.

Conclusion: Impacts would be less than significant.

**Response:** g), h) As shown in Figure 3-8, the Project is not located within a FEMA 100-year floodplain. The Project would not place housing within a 100-year flood hazard area as mapped on a federal flood hazard boundary or flood insurance rate map or other flood hazard delineation map. The Project would not place, within a 100-year flood hazard areas, structures that would impede or redirect flood flows. There would be no impact.

**Mitigation Measures:** None are required.

**Conclusion:** There would be *no impact.* 

Response: i) The City of Lemoore is located within the Pine Flat Dam inundation area. Pine Flat Dam is located east of the valley floor in the Sierra Nevada Mountains. If Pine Flat Dam failed while at full capacity, its floodwaters would arrive in Kings County within approximately five hours (Kings County, 2010). Dam failure has been adequately planned for through the Kings County Multi-Hazard Mitigation Plan, which identifies a dam failure hazard to be of medium significance and unlikely to occur in the City of Lemoore (Kings County, 2007). With the implementation of the Kings County Multi-Hazard Mitigation Plan, impacts related to dam failure would be less than significant.

**Mitigation Measures:** None are required.

**Conclusion:** Impacts would be *less than significant.* 

**Response:** j) The Project site is not located near the ocean, body of water or a steep topographic feature (i.e., mountain, hill, bluff, etc.). Therefore, there is no potential for the site to be inundated by seiche, tsunami or mudflow. There would be no impact.

**Mitigation Measures:** None are required.



		Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less-than- Significant Impact	No Impact
3.1	4 - Land Use and Planning				
Woul	d the project:				
a.	Physically divide an established community?				
b.	Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to, the general plan, specific plan, local coastal Program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?				
C.	Conflict with any applicable habitat conservation plan or natural community conservation plan?				

**Response:** a) The Project would not physically divide an established community (see Figure 2-1). The proposed residential development would connect to the surrounding uses and City road network.

**Mitigation Measures:** None are required.

**Conclusion:** There would be *no impact.* 

b) If approved, the new general plan and zoning designations would be consistent with the Project as proposed and therefore no impacts will be created.

**Mitigation Measures:** None are required.

**Conclusion:** There would be *no impact.* 

c) The Project site is not within the boundaries of an adopted habitat or natural community conservation plan. Therefore, there would be no impact.

**Mitigation Measures:** None are required.

		Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less-than- Significant Impact	No Impact
3.1	5 - Mineral Resources				
Wou	ld the project:				
a.	Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?				
b.	Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan, or other land use plan?				

**Response:** a), b) The City of Lemoore and the surrounding area are designated as Mineral Resources Zone 1 (MRZ-1) by the State Mining and Geology Board (SMGB). MRZ-1 areas are described as those for which adequate information indicates that no significant mineral deposits are present or where it is judged that little likelihood exists for their presence. Additionally, per the California Division of Oil, Gas, and Geothermal Resources (DOGGR), there are no active, inactive, or capped oil wells located within the Project site, and it is not within a DOGGR-recognized oilfield. Therefore, there would be no impact.

**Mitigation Measures:** None are required.

		Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less-than- Significant Impact	No Impact
3.1	6 - Noise				
Woul	ld the project result in:				
a.	Exposure of persons to, or generate, noise levels in excess of standards established in a local general plan or noise ordinance or applicable standards of other agencies?				
b.	Exposure of persons to or generate excessive groundborne vibration or groundborne noise levels?				
C.	A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?				
d.	A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?				
e.	For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?				
f.	For a project located within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?				

Response: a) Project construction would generate temporary increases in noise levels. Title 5, Chapter 6 of the City's Municipal Code establishes regulations and enforcement procedures for noise generated in the city. The regulations do not apply to the operation on days other than Sunday of construction equipment or of a construction vehicle, or the performance on days other than Sunday of construction work, between the hours of 7:00 A.M. and 8:00 P.M., provided that all required permits for the operation of such construction equipment or construction vehicle or the performance of such construction work have been obtained from the appropriate city department (Lemoore Municipal Code 5-6-1-C.4). The City of Lemoore 2030 General Plan (City of Lemoore , 2008) has objectives to minimize residential development noise levels. The proposed Project would comply with all regulations, standards and policies within the City's General Plan and Municipal Code.

Therefore, the Project would not result in the exposure of persons to, or generate, noise levels more than standards established in a local general plan or noise ordinance or applicable standards of other agencies. Impacts would be less than significant.

**Mitigation Measures:** None are required.

**Conclusion:** Impacts would be *less than significant.* 

Response: b), c), d) The Project involves the construction and operation of 134-residential units. As shown in Figure 2-4, the Project would be consistent with the surrounding land uses and would not cause out of the ordinary noise levels than what is currently established in the area. Construction of the Project would generate temporary ground borne vibrations. However, like construction noise, such vibrations would be attenuated over distance to the point where they would not be felt by the nearest receptors. Additionally, construction would be done during the daylight hours and would be temporary so the surrounding land uses would not be affected by construction of the new development. The Project would not expose persons to or generate excessive groundborne vibration or noise levels and would not result in substantial permanent, temporary or periodic increase in ambient noise levels above the existing environment.

**Mitigation Measures:** None are required.

**Conclusion:** Impacts would be *less than significant.* 

**Response:** e), f) There are no airports within two miles of the Project site, nor is it in the vicinity of a private airstrip. The Lemoore Zoning Ordinance has adopted a military influence area that identifies areas that may be subject to noise impacts from the Naval Air Station Lemoore, which is approximately seven miles west of the project site. The project site is outside of the established Naval Air Station Lemoore Overlay Zone. Therefore, there would be no impact.

Mitigation Measures: None are required.

**Conclusion:** Impacts would be *less than significant.* 

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less- than Significant Impact	No Impact
3.17 - Population and Housing				
Would the project:				
a. Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?				
b. Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?				
c. Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?				

Response: a) The proposed Project would accommodate, but not induce, population growth. Table 2-34 of the Kings County and Cities of Avenal, Corcoran, Hanford and Lemoore 2016-2024 Housing Element (2016-2024 Housing Element) shows the City of Lemoore's housing needs allocations for the 2014-2024 period. The Regional Housing Needs Allocation (RHNA) Plan determines the number and affordability of housing units that jurisdictions need to plan for through land use policies, regulations, infrastructure plans, and other housing assistance programs (Kings County, 2016). Construction and development of the proposed 134 single-family units would assist in meeting the RHNA Plan, which allocates for 2,773 units of different income category. Therefore, there would be no impact.

Mitigation Measures: None are required.

**Conclusion:** There would be *no impact.* 

**Response:** b), c) The Project site is currently undeveloped. Therefore, the Project would not displace substantial numbers of existing housing or people. There would be no impact.

**Mitigation Measures:** None are required.

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#### 3.18 - Public Services

Would the project:

a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times, or to other performance objectives for any of the public services:

i.	Fire protection?		$\boxtimes$	
ii.	Police protection?		$\boxtimes$	
iii.	Schools?		$\boxtimes$	
iv.	Parks?			$\boxtimes$
v.	Other public facilities?			$\boxtimes$

Response: a) In general, impacts to public services from implementation of a Project are due to its ability to induce population growth and, in turn, result in a greater need for fire and police protection, etc. to serve the increased population. The proposed Project includes the construction and operation of 134 single-family residential units, which would accommodate the City's future population growth and require amenities provided by public services. Additionally, the Project would not physically affect any existing government facilities as the proposed site is currently undeveloped. As part of the City's project approval processes, the applicant will be required to construct the infrastructure needed to serve the Project site and pay the appropriate impact fees to cover the subdivision's impacts to public services.

i. Fire suppression support is provided by the City of Lemoore Volunteer Fire Department (LVFD). The LVFD has three stations and the closest station to the Project site is located near the intersection of Fox Street and C Street approximately a mile northeast of the Project site. The proposed Project would result in the construction and operation of 134 residential units in south-central Lemoore. Construction activities would be in accordance with local and State fire codes. Fire services are

adequately planned for within the City's General Plan through policies to ensure the City maintains Fire Department performance and response standards by allocating the appropriate resources. As stated, the Daley Homes Project applicant is responsible for constructing any infrastructure needed to serve the subdivision and pay the appropriate impact fees, which would reduce impacts to less than significant.

Mitigation Measures: None are required.

**Conclusion:** Impacts would be *less than significant*.

ii. Law enforcement and public protection are provided by the City of Lemoore Police Department. The City's police station is located at 657 Fox Street on the northwest corner of Fox Street and Cinnamon Drive. The station is approximately a mile northeast of the Project site. As discussed, the proposed Project would not induce but accommodate population growth, and therefore would not increase demands for public safety protection. As stated, the Daley Homes Project applicant is responsible for constructing any infrastructure needed to serve the subdivision and pay the appropriate impact fees. Impacts on police protection services related to population growth would therefore be considered less than significant.

Mitigation Measures: None are required.

**Conclusion:** Impacts would be *less than significant.* 

iii. The schools that would be accommodating the proposed subdivision are P.W. Engvall Elementary School, Liberty Middle School, and Lemoore Union High School. Per the Parks, Schools, and Community Facilities Element of the 2030 General Plan, both the elementary and middle schools are running under capacity. Additionally, the City has identified several sites for a future high school to accommodate population growth as the current high school is running 17% over capacity. Since the proposed Project would be accommodating population growth, the impact to schools would be considered less than significant. The developer will be required to pay established school impact fees upon construction of the homes.

**Mitigation Measures:** None are required.

**Conclusion:** Impacts would be *less than significant*.

iv. The proposed Project includes the development of 134 residential lots. The City is currently maintaining a 5-acre to 1,000 residents park ratio, which exceeds current City Park Standards and Quimby Act requirements (City of Lemoore, 2008). The Project would have no impact to the City park system.

**Mitigation Measures:** None are required.

v. The proposed Project does not include any other impacts to public facilities.

Mitigation Measures: None are required.

		Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less–than- Significant Impact	No Impact
3	19 - Recreation				
Wo	uld the project:				
a.	Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?				
b.	Include recreational facilities or require the construction or expansion of recreational facilities that might have an adverse physical effect on the environment?				

**Response:** a), b) As discussed, the population growth accommodated by the Project (134 homes x 3.05 persons per home) is approximately 409 people. The City's General Plan indicates that the City is continuing to maintain its parkland dedication standard of 5 acres of park land per 1,000 residents. There would be no impact.

**Mitigation Measures:** None are required.

		Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less-than- Significant Impact	No Impact
3.2	0 - Transportation and Traffic				
Woul	d the project:				
a.	Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?				
b.	Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?				
C.	Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?				
d.	Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?				
e.	Result in inadequate emergency access?				$\boxtimes$
f.	Conflict with adopted policies, plans, or Programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?				

**Response:** a) The City's transportation policies and requirements are incorporated in its General Plan. The only such policy which is affected by this Project is that requiring that no Level of Service violations be engendered by a Project. Per the City's Circulation Element of

the City of Lemoore 2030 General Plan Update (City of Lemoore , 2008), the "City of Lemoore does not currently have any adopted level of service (LOS) standard. However, recent traffic studies have used level of service D as the standard for evaluating project impacts at intersections." A LOS of D is characterized by congestion with average vehicle speeds decreasing below the user's desired level for two and four land roads. The Level of Service for 19th Avenue is C; the daily traffic of the Project site is, 1,282 cars per day (9.57 trips per day per residence; see *Section 3.3 - Air Quality*). As discussed in the Population and Housing Section, the Project will be accommodating future population growth. The calculated trips per day is considered the worst-case scenario. It is assumed that the LOS of the surrounding streets would remain the same. Additionally, trips to bring materials for construction to the site would be temporary. Therefore, the Project would not conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system. Impacts would be less than significant.

**Mitigation Measures:** None are required.

**Conclusion:** Impacts would be *less than significant.* 

**Response:** b) Neither the City of Lemoore or Kings County has an adopted congestion management program. Therefore, there would be no impact.

**Mitigation Measures:** None are required.

**Conclusion:** There would be *no impact.* 

**Response:** c) As discussed, there are no public airports or private airstrips within the vicinity of the Project site and the Project does not include the construction of any structures that would interfere with air traffic patterns. Therefore, there would be no impact.

**Mitigation Measures:** None are required.

**Conclusion:** There would be *no impact.* 

**Response:** d), e) The Project would not involve design features that would increase hazards or involve the development of incompatible uses. It would also not result in inadequate emergency access. Therefore, there would be no impact.

**Mitigation Measures:** None are required.

**Conclusion:** There would be *no impact.* 

**Response:** f) The Project would not affect existing pedestrian and bicycle facilities within the surrounding area. There is no conflict with the Kings County's 2005 Regional Bicycle Plan; therefore, there would be no impact.

**Mitigation Measures:** None are required.

			Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less–than- Significant Impact	No Impact
3.2	21 - Ti	ribal Cultural Resources				
Wo	uld the p	project:				
a.	change resour section cultura define landsc cultura	I the project cause a substantial adverse e in the significance of a tribal cultural rce, defined in Public Resources Code in 21074 as either a site, feature, place, all landscape that is geographically d in terms of the size and scope of the cape, sacred place, or object with all value to a California Native American and that is:				
	i.	Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k), or				
	ii.	A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resource Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.				

**Response:** a) The Project is not located within an area with known tribal cultural resources. As discussed in the *Section 3.9 - Cultural Resources*, there are no historical resources located on or within the vicinity of the Project site. Additionally, consultation has been requested from the local tribes; however, no responses have been received. Therefore, the proposed Project would have no impact to tribal cultural resources.

**Mitigation Measures:** None are required.

		Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less-than- Significant Impact	No Impact
3.2	22 - Utilities and Service Systems				
Woı	uld the project:				
a.	Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?				
b.	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?				
C.	Require or result in the construction of new stormwater drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?				
d.	Have sufficient water supplies available to serve the project from existing entitlements and resources, or would new or expanded entitlements be needed?				
e.	Result in a determination by the wastewater treatment provider that serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?				
f.	Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?				
g.	Comply with federal, state, and local statutes and regulations related to solid waste?			$\boxtimes$	

**Response:** a), b), c), d), e), f), g) Like public services, the Project applicant is required to either extend the needed utility infrastructure or pay impact fees to accommodate the subdivision's impact to local utility and infrastructure systems. The City's wastewater facilities, water system, storm drainage system, and solid waste disposal programs have capacity for, or are planned to maintain capacity for, community growth in accord with the adopted General Plan.

**Mitigation Measures:** None are required.

**Conclusion:** Impacts would be *less than significant.* 

		Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less than Significant Impact	No Impact
	3 - Mandatory Findings of nificance				
a.	Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory?				
b.	Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are significant when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.)				
C.	Does the project have environmental effects that would cause substantial adverse effects on human beings, either directly or indirectly?				

Response: a) As evaluated in this IS/MND, the proposed Project would not substantially degrade the quality of the environment; substantially reduce the habitat of a fish or wildlife species; cause a fish or wildlife population to drop below self-sustaining levels; threaten to eliminate a plant or animal community; reduce the number or restrict the range of an endangered, rare, or threatened species; or eliminate important examples of the major periods of California history or prehistory. Mitigation measures have been included to lessen the significance of potential impacts. Similar mitigation measures would be expected of other projects in the surrounding area, most of which share a similar cultural paleontological and biological resources. Consequently, the incremental effects of the proposed project, after mitigation, would not contribute to an adverse cumulative impact on these resources. Therefore, the Project would have a less-than-significant impact with mitigation incorporated.

#### Mitigation Measures:

Implement Mitigation Measures MM 3.8.1 through MM 3.8.4, MM CUL 3.9.1 through MM 3.9.3 and MM 3.12.1.

#### Conclusion:

Impacts would be *less than significant with mitigation incorporated.* 

Response: b) As described in the impact analyses in Sections 3.5 through 3.22 of this IS/MND, any potentially significant impacts of the proposed Project would be reduced to a less-than significant level following incorporation of the mitigation measures listed in *Appendix A – Mitigation Monitoring and Reporting Program*. All planned projects in the vicinity of the proposed Project would be subject to review in separate environmental documents and required to conform to the City of Lemoore General Plan, zoning, mitigate for project-specific impacts, and provide appropriate engineering to ensure the development meets applicable federal, State and local regulations and codes. As currently designed, and with compliance of the recommended mitigation measures, the proposed Project would not contribute to a cumulative impact. Thus, the cumulative impacts of past, present, and reasonably foreseeable future projects would be less than cumulatively considerable.

#### **Mitigation Measures:**

Implement Mitigation Measures MM 3.8.1 through MM 3.8.4, MM CUL 3.9.1 through MM 3.9.3 and MM 3.12.1.

#### **Conclusion:**

Impacts would be *less than significant with mitigation incorporated.* 

**Response:** c) All of the Project's impacts, both direct and indirect, that are attributable to the Project were identified and mitigated to a less than significant level. As shown in *Appendix A - Mitigation Monitoring and Reporting Program*, the Project proponent has agreed to implement mitigation substantially reducing or eliminating impacts of the Project. All planned projects in the vicinity of the proposed Project would be subject to review in separate environmental documents and required to conform to the City of Lemoore General Plan, zoning, mitigate for project-specific impacts, and provide appropriate engineering to ensure the development meets are applicable federal, State and local regulations and codes. Thus, the cumulative impacts of past, present, and reasonably foreseeable future projects would be less than cumulatively considerable. Therefore, the proposed Project would not either directly or indirectly cause substantial adverse effects on human beings because all potentially adverse direct impacts of the proposed Project are identified as having no impact, less than significant impact, or less than significant impact with mitigation incorporated.

#### **Mitigation Measures:**

Implement Mitigation Measures MM 3.8.1 through MM 3.8.4, MM CUL 3.9.1 through MM 3.9.3 and MM 3.12.1.

Conclusion	•

Impacts would be *less than significant with mitigation incorporated.* 

Administrative Draft References

## **SECTION 4 - REFERENCES**

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# APPENDIX A MITIGATION MONITORING AND REPORTING PROGRAM

# **MITIGATION MONITORING AND REPORTING PROGRAM**

## Daley Homes General Plan Amendment and Zone Change

Mitigation Measure	Timeframe	Responsible Monitoring Agency	Date	Initial
MM 3.8.1: A qualified biologist shall conduct a pre-construction survey on the Project site and within 500 feet of its perimeter within 14 days of and no more than 30 days prior to the start of construction activities.  If any evidence of occupation of the Project site by listed or other special-status species is subsequently observed, a buffer shall be established by a qualified biologist that results in sufficient avoidance to comply with applicable regulations. If sufficient avoidance cannot be established, the United States Fish and Wildlife Service and California Department of Fish and Game shall be contacted for further guidance and consultation on additional measures. The Project proponent shall obtain any required permits from the appropriate wildlife agency. Copies of all permits and	Prior to construction	Lead Agency		
evidence of compliance with applicable regulations shall be submitted to the lead agency.  The following buffer distances shall be established prior to construction activities:				
San Joaquin kit fox or American badger potential den: 50 feet;				
San Joaquin kit fox known den: 100 feet;				
<ul> <li>San Joaquin kit fox or American badger pupping den: contact the California Department of Fish and Game and United States Fish and Wildlife Service;</li> </ul>				
Burrowing owl burrow outside of breeding season: 160 feet;				
Burrowing owl burrow during breeding season: 250 feet;				
• Swainson's hawk nest during breeding season: ½ mile;				

<ul> <li>Other protected raptor nests during the breeding season: 300 feet;</li> <li>Other protected nesting migratory bird nests during the breeding season: 50 feet; and</li> <li>Other special-status wildlife species: as recommended by qualified biologist.</li> </ul>			
MM 3.8.2: If initial grading activities are planned during the potential nesting season for migratory birds/raptors that may nest on or near the Project site, the preconstruction survey shall evaluate the sites and accessible lands within an adequate buffer for active nests of migratory birds/raptors. If any nesting birds/raptors are observed, a qualified biologist shall determine buffer distances and/or the timing of Project activities so that the proposed Project does not cause nest abandonment or destruction of eggs or young. This measure shall be implemented so that the proposed Project remains in compliance with the Migratory Bird Treaty Act and applicable state regulations.  If nesting raptors are identified during the surveys, active raptor nests should be avoided by 500 feet and all other migratory bird nests should be avoided by 250 feet. Avoidance buffers may be reduced if a qualified and approved on-site monitor determines that encroachment into the buffer area is not affecting nest building, the rearing of young, or otherwise affect the breeding behaviors of the resident birds. Avoidance buffers can also be reduced through consultation with the CDFW and USFWS. If Swainson's hawk nests shall be avoided by 0.5 mile unless this avoidance buffer is reduced through consultation with the CDFW and/or USFWS.  No construction or earth-moving activity shall occur within a non-disturbance buffer until it is determined by a qualified biologist that the young have fledged (that is, left the nest) and have attained sufficient flight skills to avoid Project construction areas. This typically occurs by early July, but September 1st is considered the end of the nesting period unless otherwise determined by a qualified biologist. Once raptors have completed nesting and young have fledged, disturbance buffers will no longer be needed and can be removed, and monitoring can be terminated.	During construction	Lead Agency	

_				
	AM 3.8.3: If any burrowing owl burrows are observed during the preconstruction survey, avoidance measures shall be consistent and in accordance with protocols outlined in the Burrowing Owl Survey Protocol and Mitigation Guidelines (Burrowing Owl Consortium 1993) and the Staff Report on Burrowing Owl Mitigation (CDFW 2012). Active burrows shall be avoided, but if avoidance is not possible then compensation shall be provided for the active or passive displacement of western burrowing owls, and habitat acquisition and the creation of artificial dens for any western burrowing owls shall be provided for any owls relocated from construction areas. These measures are outlined as follows:	During construction	Lead Agency	
	L. A pre-construction survey of construction area, including a 150-meter buffer (500 feet), shall be conducted no less than 14 days and no more than 30 days prior to ground disturbing activities. If more than 30 days lapse between the time of the pre-construction survey and the start of ground-disturbing activities, another pre-construction survey shall be completed. The second survey (or other subsequent surveys if necessary) shall be conducted and timed to occur sometime between 30 days and 24 hours prior to ground disturbance.			
2	2. If western burrowing owls are present on the construction site (or within 500 feet of the construction site), exclusion fencing shall be installed between the nest site or active burrow and any earth-moving activity or other disturbance. Exclusion areas shall extend 160 feet around occupied burrows during the non-breeding season (September 1 through January 31) and extend 250 feet around occupied burrows during the breeding season (February 1 through August 31) as described in The California Burrowing Owl Consortium's Survey Protocol and Mitigation Guidelines (California Burrowing Owl Consortium 1993).			
	3. If western burrowing owls are present in the non-breeding season and must be passively relocated from the Project site, passive relocation shall not commence until October 1st and must be completed by February 1st. Passive relocation must only be conducted by a qualified biologist or ornithologist and with approval by CDFW. After passive relocation, the area where owls occurred and its immediate vicinity shall be monitored by a qualified biologist daily for one week and once			

	per week for an additional to reoccupying the site.	wo weeks to document that owls are not			
4.	burrowing owl habitat occur number of owls or pairs of ov	sting, occupied and satellite burrows, or compensation shall be based upon the wls relocated from the construction area. The determined as described in the CDFW's wl Mitigation (CDFW 2012).			
	<b>3.8.4:</b> The measures listed struction:	l below shall be implemented during	During construction	Lead Agency	
1.	no more than 30 days prior and/or construction activities during preconstruction surv	Il be conducted no fewer than 14 days and to the beginning of ground disturbance s. If any San Joaquin kit fox dens are found theys, exclusion zones shall be placed in the immendations using the following:			
	San Joaquin kit fox USFWS	Exclusion Zone Recommendations			
	, <u>-</u>	Exclusion Zone Recommendations  Recommendation			
	San Joaquin kit fox USFWS  Den Type  Potential Den				
	<b>Den Type</b> Potential Den Known Den	Recommendation 50-foot radius 100-foot radius			
Na	Den Type Potential Den Known Den atal/Pupping Den (Occupied	Recommendation 50-foot radius 100-foot radius Contact U.S. Fish and Wildlife Service			
Na	Den Type Potential Den Known Den atal/Pupping Den (Occupied and Unoccupied)	Recommendation 50-foot radius 100-foot radius Contact U.S. Fish and Wildlife Service for guidance			
Na	Den Type Potential Den Known Den atal/Pupping Den (Occupied	Recommendation 50-foot radius 100-foot radius Contact U.S. Fish and Wildlife Service			
2.	Den Type Potential Den Known Den Atal/Pupping Den (Occupied and Unoccupied) Atypical Den  If any den must be removed, excavated by a trained wildlif other "known" kit fox dens m Replacement dens will be req dens that are removed do determined to be inactive by (e.g., applying tracking me monitoring for San Joaquin kit	Recommendation 50-foot radius 100-foot radius Contact U.S. Fish and Wildlife Service for guidance 50-foot radius  it must be appropriately monitored and fe biologist. Destruction of natal dens and ust not occur until authorized by USFWS. Juired if such dens are removed. Potential not need to be replaced if they are y using standard monitoring techniques edium around the den opening and t fox tracks for three consecutive nights).			
2.	Den Type Potential Den Known Den Atal/Pupping Den (Occupied and Unoccupied) Atypical Den  If any den must be removed, excavated by a trained wildlif other "known" kit fox dens m Replacement dens will be req dens that are removed do determined to be inactive by (e.g., applying tracking me monitoring for San Joaquin ki	Recommendation 50-foot radius 100-foot radius Contact U.S. Fish and Wildlife Service for guidance 50-foot radius  it must be appropriately monitored and fe biologist. Destruction of natal dens and ust not occur until authorized by USFWS. uired if such dens are removed. Potential not need to be replaced if they are y using standard monitoring techniques edium around the den opening and			

State and federal highways; this is particularly important at night when kit foxes and badgers are most active. Night-time construction shall be minimized to the extent possible. However, if construction at night does occur, then the speed limit shall be reduced to 10-mph. Off-road traffic outside of designated Project areas shall be prohibited. 4. To prevent inadvertent entrapment of kit foxes or other animals during the construction phase of a Project, all excavated, steep-walled holes or trenches more than 2-feet deep should be covered at the close of each working day by plywood or similar materials. If the trenches cannot be closed, one or more escape ramps constructed of earthen-fill or wooden planks shall be installed. Before such holes or trenches are filled, they shall be thoroughly inspected for trapped animals. If at any time a trapped or injured kit fox is discovered, the USFWS and the CDFW shall be contacted at the addresses provided below. 5. Kit foxes are attracted to den-like structures such as pipes and may enter stored pipes and become trapped or injured. All construction pipes, culverts, or similar structures with a diameter of 4-inches or greater that are stored at a construction site for one or more overnight periods shall be thoroughly inspected for kit foxes before the pipe is subsequently buried, capped, or otherwise used or moved in any way. If a kit fox is discovered inside a pipe, that section of pipe shall not be

remove it from the path of construction activity, until the fox has escaped.6. All food-related trash items such as wrappers, cans, bottles, and food scraps shall be disposed of in securely closed containers and removed at least once a week from a construction or Project sites.

moved until the USFWS has been consulted. If necessary, and under the direct supervision of the biologist, the pipe may be moved only once to

- 7. No pets, such as dogs or cats, shall be permitted on the Project sites to prevent harassment, mortality of kit foxes, or destruction of dens.
- 8. Use of rodenticides and herbicides in Project areas shall be restricted. This is necessary to prevent primary or secondary poisoning of kit foxes and the depletion of prey populations on which they depend. All uses of such compounds shall observe label and other restrictions

	mandated by the U.S. Environmental Protection Agency, California Department of Food and Agriculture, and other State and Federal legislation, as well as additional Project-related restrictions deemed necessary by the USFWS. If rodent control must be conducted, zinc phosphide shall be used because of a proven lower risk to kit fox.		
9.	A representative shall be appointed by the Project proponent who will be the contact source for any employee or contractor who might inadvertently kill or injure a kit fox or who finds a dead, injured or entrapped kit fox. The representative will be identified during the employee education program and their name and telephone number shall be provided to the USFWS.		
10.	An employee education program shall be conducted. The program shall consist of a brief presentation by persons knowledgeable in San Joaquin kit fox biology and legislative protection to explain endangered species concerns to contractors, their employees, and military and/or agency personnel involved in the Project. The program shall include: a description of the San Joaquin kit fox and its habitat needs; a report of the occurrence of kit fox in the Project area; an explanation of the status of the species and its protection under the Endangered Species Act; and a list of measures being taken to reduce impacts to the species during Project construction and implementation. A fact sheet conveying this information shall be prepared for distribution to the previously referenced people and anyone else who may enter the Project sites.		
11.	Upon completion of the Project, all areas subject to temporary ground disturbances, including storage and staging areas, temporary roads, pipeline corridors, etc. shall be re-contoured if necessary, and revegetated to promote restoration of the area to pre-Project conditions. An area subject to "temporary" disturbance means any area that is disturbed during the Project, but after Project completion will not be subject to further disturbance and has the potential to be revegetated. Appropriate methods and plant species used to revegetate such areas should be determined on a site-specific basis in consultation with the USFWS, CDFW, and revegetation experts.		

12. In the case of trapped animals, escape ramps or structures should be installed immediately to allow the animal(s) to escape, or the USFWS shall be contacted for guidance.			
13. Any contractor, employee, or military or agency personnel who are responsible for inadvertently killing or injuring a San Joaquin kit fox shall immediately report the incident to their representative. This representative shall contact the CDFW immediately in the case of a dead, injured or entrapped kit fox. The CDFW contact for immediate assistance is State Dispatch at (916)445-0045. They will contact the local warden or CDFW representative, the wildlife biologist, at (530)934-9309. The USFWS shall be contacted at the numbers below.			
14. The Sacramento Fish and Wildlife Office of USFWS and CDFW shall be notified in writing within three working days of the accidental death or injury to a San Joaquin kit fox during Project-related activities. Notification must include the date, time, and location of the incident or of the finding of a dead or injured animal and any other pertinent information. The USFWS contact is the Chief of the Division of Endangered Species, at the addresses and telephone numbers below. The CDFW contact can be reached at 1701 Nimbus Road, Suite A, Rancho Cordova, California 95670, (530) 934-9309.			
15. All sightings of the San Joaquin kit fox shall be reported to the California Natural Diversity Database (CNDDB). A copy of the reporting form and a topographic map clearly marked with the location of where the kit fox was observed shall also be provided to the Service at the address below.			
Any Project-related information required by the USFWS or questions concerning the above conditions or their implementation may be directed in writing to the U.S. Fish and Wildlife Service at: Endangered Species Division, 2800 Cottage Way, Suite W 2605, Sacramento, California 95825-1846, phone (916) 414-6620 or (916) 414-6600.			
<b>MM 3.9.1:</b> If prehistoric or historic-era cultural or archaeological materials are encountered during construction activities, all work within 25 feet of the find shall halt until a qualified professional archaeologist, meeting the Secretary of the Interior's Professional Qualification Standards for	During construction	Lead Agency	

prehistoric and historic archaeologist, can evaluate the significance of the find and make recommendations. Cultural resource materials may include prehistoric resources such as flaked and ground stone tools and debris, shell, bone, ceramics, and fire-affected rock as well as historic resources such as glass, metal, wood, brick, or structural remnants. If the qualified professional archaeologist determines that the discovery represents a potentially significant cultural resource, additional investigations may be required to mitigate adverse impacts from Project implementation. These additional studies may include avoidance, testing, and evaluation or data recovery excavation.			
If a potentially-eligible resource is encountered, then the qualified professional archaeologist, the Lead Agency, and the Project proponent shall arrange for either 1) total avoidance of the resource or 2) test excavations to evaluate eligibility and, if eligible, total data recovery. The determination shall be formally documented in writing and submitted to the Lead Agency as verification that the provisions for managing unanticipated discoveries have been met.			
MM 3.9.2: During any ground disturbance activities, if paleontological resources are encountered, all work within 25 feet of the find shall halt until a qualified paleontologist as defined by the Society of Vertebrate Paleontology Standard Procedures for the Assessment and Mitigation of Adverse Impacts to Paleontological Resources (2010), can evaluate the find and make recommendations regarding treatment. Paleontological resource materials may include resources such as fossils, plant impressions, or animal tracks preserved in rock. The qualified paleontologist shall contact the University of California Museum of Paleontology, or other appropriate facility regarding any discoveries of paleontological resources.	During construction	Lead Agency	
If the qualified paleontologist determines that the discovery represents a potentially significant paleontological resource, additional investigations and fossil recovery may be required to mitigate adverse impacts from Project implementation. If avoidance is not feasible, the paleontological resources shall be evaluated for their significance. If the resources are not significant, avoidance is not necessary. If the resources are significant, they shall be avoided to ensure no adverse effects, or such effects must be mitigated. Construction in that area shall not resume until the resource appropriate measures are recommended or the materials are determined			

to be less than significant. If the resource is significant and fossil recovery is the identified form of treatment, then the fossil shall be deposited in an accredited and permanent scientific institution. Copies of all correspondence and reports shall be submitted to the Lead Agency.  Construction in that area shall not resume until the resource appropriate measures are recommended or the materials are determined to be less than significant. If the resource is significant and fossil recovery is the identified form of treatment, then the fossil shall be deposited in an accredited and permanent scientific institution. Copies of all correspondence and reports shall be submitted to the Lead Agency.			
<b>MM 3.9.3:</b> If human remains are discovered during construction or operational activities, further excavation or disturbance shall be prohibited pursuant to Section 7050.5 of the California Health and Safety Code. The protocol, guidelines, and channels of communication outlined by the Native American Heritage Commission, in accordance with Section 7050.5 of the Health and Safety Code, Section 5097.98 of the Public Resources Code (Chapter 1492, Statutes of 1982, Senate Bill 297), and Senate Bill 447 (Chapter 44, Statutes of 1987), shall be followed. Section 7050.5(c) shall guide any potential Native American involvement, in the event of discovery of human remains, at the direction of the county coroner.	During construction	Lead Agency	
MM 3.12.1: Prior to ground-disturbing activities, the City shall prepare and implement a Stormwater Pollution Prevention Plan (SWPPP) that specifies best management practices (BMP), with the intent of keeping all products of erosion from moving offsite. The SWPPP shall include contain a site map that shows the construction site perimeter, existing and proposed manmade facilities, stormwater collection and discharge points, general topography both before and after construction, and drainage patterns across the Project site. Additionally, the SWPPP shall contain a visual monitoring program and a chemical monitoring program for non-visible pollutants to be implemented (if there is a failure of best management practices). The requirements of the SWPPP and BMPs shall be incorporated into design specifications and construction contracts. Recommended best management practices for the construction phase may include the following:	Prior to construction	Lead Agency	

•	Stockpiling and disposing of demolition debris, concrete, and soil properly.		
•	Protecting any existing storm drain inlets and stabilizing disturbed areas.		
•	Implementing erosion controls.		
•	Properly managing construction materials.		
•	Managing waste, aggressively controlling litter, and implementing sediment controls.		

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#### Daily Homes Subdivision - Kings County, Annual

## Daily Homes Subdivision Kings County, Annual

## 1.0 Project Characteristics

## 1.1 Land Usage

Land Uses	Size	Metric	Lot Acreage	Floor Surface Area	Population	
Single Family Housing	134.00	Dwelling Unit	20.00	241,200.00	383	

## 1.2 Other Project Characteristics

Urbanization Urban Wind Speed (m/s) 2.2 Precipitation Freq (Days) 37

Climate Zone 3 Operational Year 2018

Utility Company Pacific Gas & Electric Company

 CO2 Intensity
 641.35
 CH4 Intensity
 0.029
 N20 Intensity
 0.006

 (lb/MWhr)
 (lb/MWhr)
 (lb/MWhr)
 (lb/MWhr)

#### 1.3 User Entered Comments & Non-Default Data

Project Characteristics -

Land Use - Per site plan

Table Name	Column Name	Default Value	New Value		
tblLandUse	LotAcreage	43.51	20.00		
tblWoodstoves	NumberCatalytic	20.00	0.00		
tblWoodstoves	NumberNoncatalytic	20.00	0.00		

## 2.0 Emissions Summary

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## Daily Homes Subdivision - Kings County, Annual

## 2.1 Overall Construction

## **Unmitigated Construction**

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Year	tons/yr												MT	-/yr		
2004					1 1 1											0.0000
Maximum																0.0000

## **Mitigated Construction**

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Year	tons/yr												МТ	-/yr		
2004	ii ii			1 1 1												0.0000
Maximum																0.0000

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio-CO2	Total CO2	CH4	N20	CO2e
Percent Reduction	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

## Daily Homes Subdivision - Kings County, Annual

Quarter	Start Date	End Date	Maximum Unmitigated ROG + NOX (tons/quarter)	Maximum Mitigated ROG + NOX (tons/quarter)					
		Highest							

## 2.2 Overall Operational

**Unmitigated Operational** 

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					MT/yr											
Area					! !			! !								60.0603
Energy	,		,	,	,	,		,	,				<del></del>	<del></del>	,	599.4823
Mobile	,		,	,	,	,		,	,				<del></del>	<del></del>	,	2,566.342 5
Waste	,		,	,	,	,		,	,				<del></del>	<del></del>	,	69.3401
Water			y	y <del></del> ! ! !	,	,		y <del></del> ! ! !	,			,		<del></del>     	,	31.3069
Total																3,326.532 1

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## Daily Homes Subdivision - Kings County, Annual

## 2.2 Overall Operational

## **Mitigated Operational**

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					MT/yr											
Area															i i	60.0603
Energy							,       								1	599.4823
Mobile							,       								1	2,566.342 5
Waste							,       								1	69.3401
Water															1 1 1 1	31.3069
Total																3,326.532 1

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio-CO2	Total CO2	CH4	N20	CO2e
Percent Reduction	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

## 3.0 Construction Detail

## **Construction Phase**

#### Daily Homes Subdivision - Kings County, Annual

Phase Number	Phase Name	Phase Type	Start Date	End Date	Num Days Week	Num Days	Phase Description
1	Architectural Coating	Architectural Coating	1/5/2004	1/4/2004	5	20	
2	Building Construction	Building Construction	1/5/2004	1/4/2004	5	300	
3	Demolition	Demolition	1/5/2004	1/4/2004	5	20	
4	Grading	Grading	1/5/2004	1/4/2004	5	30	
5	Paving	Paving	1/5/2004	1/4/2004	5	20	
6	Site Preparation	Site Preparation	1/5/2004	1/4/2004	5	10	

Acres of Grading (Site Preparation Phase): 0

Acres of Grading (Grading Phase): 75

Acres of Paving: 0

Residential Indoor: 488,430; Residential Outdoor: 162,810; Non-Residential Indoor: 0; Non-Residential Outdoor: 0; Striped Parking Area: 0 (Architectural Coating – sqft)

OffRoad Equipment

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Phase Name	Offroad Equipment Type	Amount	Usage Hours	Horse Power	Load Factor
Architectural Coating	Air Compressors	1	6.00	78	0.48
Demolition	Excavators	3	8.00	158	0.38
Demolition	Concrete/Industrial Saws	1	8.00	81	0.73
Grading	Excavators	2	8.00	158	0.38
Building Construction	Cranes	1	7.00	231	0.29
Building Construction	Forklifts	3	8.00	89	0.20
Building Construction	Generator Sets	1	8.00	84	0.74
Paving	Pavers	2	8.00	130	0.42
Paving	Rollers	2	8.00	80	0.38
Demolition	Rubber Tired Dozers	2	8.00	247	0.40
Grading	Rubber Tired Dozers	1	8.00	247	0.40
Building Construction	Tractors/Loaders/Backhoes	3	7.00	97	0.37
Grading	Graders	1	8.00	187	0.41
Grading	Tractors/Loaders/Backhoes	2	8.00	97	0.37
Paving	Paving Equipment	2	8.00	132	0.36
Site Preparation	Tractors/Loaders/Backhoes	4	8.00	97	0.37
Site Preparation	Rubber Tired Dozers	3	8.00	247	0.40
Grading	Scrapers	2	8.00	367	0.48
Building Construction	Welders	1	8.00	46	0.45

**Trips and VMT** 

#### Daily Homes Subdivision - Kings County, Annual

Phase Name	Offroad Equipment Count	Worker Trip Number	Vendor Trip Number	Hauling Trip Number	Worker Trip Length	Vendor Trip Length	Hauling Trip Length	Worker Vehicle Class	Vendor Vehicle Class	Hauling Vehicle Class
Architectural Coating	1	10.00	0.00	0.00	10.80	7.30	20.00	LD_Mix	HDT_Mix	HHDT
Building Construction	9	48.00	14.00	0.00	10.80	7.30	20.00	LD_Mix	HDT_Mix	HHDT
Demolition	6	15.00	0.00	0.00	10.80	7.30	20.00	LD_Mix	HDT_Mix	HHDT
Grading	8	20.00	0.00	0.00	10.80	7.30	20.00	LD_Mix	HDT_Mix	HHDT
Paving	6	15.00	0.00	0.00	10.80	7.30	20.00	LD_Mix	HDT_Mix	HHDT
Site Preparation	7	18.00	0.00	0.00	10.80	7.30	20.00	LD_Mix	HDT_Mix	HHDT

#### **3.1 Mitigation Measures Construction**

## 3.2 Architectural Coating - 2004

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Archit. Coating	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

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## 3.2 Architectural Coating - 2004 Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Archit. Coating	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

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3.2 Architectural Coating - 2004 Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

## 3.3 Building Construction - 2004

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Off-Road	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

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## 3.3 Building Construction - 2004 <u>Unmitigated Construction Off-Site</u>

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Off-Road	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

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3.3 Building Construction - 2004 Mitigated Construction Off-Site

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

#### 3.4 Demolition - 2004

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
0	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

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3.4 Demolition - 2004

<u>Unmitigated Construction Off-Site</u>

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Off-Road	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

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3.4 Demolition - 2004

<u>Mitigated Construction Off-Site</u>

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

#### 3.5 Grading - 2004

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Fugitive Dust	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

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3.5 Grading - 2004

<u>Unmitigated Construction Off-Site</u>

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Fugitive Dust	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

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3.5 Grading - 2004

Mitigated Construction Off-Site

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

# 3.6 Paving - 2004 <u>Unmitigated Construction On-Site</u>

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Off-Road	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Paving	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

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3.6 Paving - 2004
<u>Unmitigated Construction Off-Site</u>

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Off-Road	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Paving	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

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3.6 Paving - 2004

<u>Mitigated Construction Off-Site</u>

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

#### 3.7 Site Preparation - 2004

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Fugitive Dust	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

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#### Daily Homes Subdivision - Kings County, Annual

3.7 Site Preparation - 2004

<u>Unmitigated Construction Off-Site</u>

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Fugitive Dust	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

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#### Daily Homes Subdivision - Kings County, Annual

3.7 Site Preparation - 2004

<u>Mitigated Construction Off-Site</u>

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

#### 4.0 Operational Detail - Mobile

### **4.1 Mitigation Measures Mobile**

#### Daily Homes Subdivision - Kings County, Annual

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
lgatea																2,566.342 5
Unmitigated	,,															2,566.342 5

#### **4.2 Trip Summary Information**

	Avei	rage Daily Trip Ra	ate	Unmitigated	Mitigated
Land Use	Weekday	Saturday	Sunday	Annual VMT	Annual VMT
Single Family Housing	1,275.68	1,327.94	1155.08	3,623,370	3,623,370
Total	1,275.68	1,327.94	1,155.08	3,623,370	3,623,370

#### **4.3 Trip Type Information**

		Miles			Trip %			Trip Purpos	e %
Land Use	H-W or C-W	H-S or C-C	H-O or C-NW	H-W or C-W	H-S or C-C	H-O or C-NW	Primary	Diverted	Pass-by
Single Family Housing	10.80	7.30	7.50	42.30	19.60	38.10	86	11	3

#### 4.4 Fleet Mix

Land Use	LDA	LDT1	LDT2	MDV	LHD1	LHD2	MHD	HHD	OBUS	UBUS	MCY	SBUS	MH
Single Family Housing	0.472912	0.030922	0.145205	0.141616	0.025886	0.005289	0.012264	0.153809	0.001816	0.002088	0.006227	0.001059	0.000908

## 5.0 Energy Detail

Historical Energy Use: N

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#### Daily Homes Subdivision - Kings County, Annual

#### **5.1 Mitigation Measures Energy**

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	-/yr		
2.0000.1								i i i								368.5390
Electricity Unmitigated		<del></del>	,		,	,	<del></del>	,				,	,		,	368.5390
NaturalGas Mitigated	1 1 1 1	<del></del>	,		,	,		,				,	,		,	230.9433
NaturalGas Unmitigated	1 1		, , , , , , , , , , , , , , , , , , ,		 : : :	 : : :		 ! ! !	 : : :	· · · · · · · · · · · · · · · · · · ·			,		 : : :	230.9433

## 5.2 Energy by Land Use - NaturalGas

**Unmitigated** 

	NaturalGa s Use	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Land Use	kBTU/yr					ton	s/yr							MT	/yr		
Single Family Housing	4.30215e +006																230.9433
Total																	230.9433

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#### Daily Homes Subdivision - Kings County, Annual

# **5.2 Energy by Land Use - NaturalGas Mitigated**

	NaturalGa s Use	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Land Use	kBTU/yr					ton	s/yr							MT	/yr		
Single Family Housing	4.30215e +006								1 1 1 1								230.9433
Total																	230.9433

## 5.3 Energy by Land Use - Electricity Unmitigated

	Electricity Use	Total CO2	CH4	N2O	CO2e
Land Use	kWh/yr		МТ	/yr	
Single Family Housing	+006				368.5390
Total					368.5390

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#### Daily Homes Subdivision - Kings County, Annual

## 5.3 Energy by Land Use - Electricity Mitigated

	Electricity Use	Total CO2	CH4	N2O	CO2e
Land Use	kWh/yr		МТ	-/yr	
Single Family Housing	1.2619e +006				368.5390
Total					368.5390

#### 6.0 Area Detail

#### **6.1 Mitigation Measures Area**

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Mitigated	ii															60.0603
Unmitigated	11 11 11 11						 		i i					i i		60.0603

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#### Daily Homes Subdivision - Kings County, Annual

## 6.2 Area by SubCategory Unmitigated

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
SubCategory					ton	s/yr							MT	/уг		
Coating																0.0000
Consumer Products			,       		       	       	       		,       						,	0.0000
Hearth			,       		       	       	       		,       						,	58.3948
Landscaping			,       		       	       	       		       			;				1.6655
Total																60.0603

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#### Daily Homes Subdivision - Kings County, Annual

## 6.2 Area by SubCategory Mitigated

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
SubCategory					ton	s/yr							МТ	7/yr		
Architectural Coating																0.0000
Consumer Products			i i	   	       		     	i i	       						i i	0.0000
Hearth	 			 	 		     	i i	       						i i	58.3948
Landscaping	) 			 	 		     	i i	 			i i			i i	1.6655
Total																60.0603

#### 7.0 Water Detail

#### 7.1 Mitigation Measures Water

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	Total CO2	CH4	N2O	CO2e
Category		MT	√yr	
Willigatou				31.3069
Ommigated				31.3069

## 7.2 Water by Land Use Unmitigated

	Indoor/Out door Use	Total CO2	CH4	N2O	CO2e
Land Use	Mgal		МТ	-/yr	
Single Family Housing	8.73064 / 5.5041				31.3069
Total					31.3069

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#### Daily Homes Subdivision - Kings County, Annual

7.2 Water by Land Use

#### **Mitigated**

	Indoor/Out door Use	Total CO2	CH4	N2O	CO2e
Land Use	Mgal		МТ	√yr	
Single Family Housing	8.73064 / 5.5041				31.3069
Total					31.3069

#### 8.0 Waste Detail

#### 8.1 Mitigation Measures Waste

#### Category/Year

	Total CO2	CH4	N2O	CO2e
		МТ	√yr	
Willigatou				69.3401
Unmitigated				69.3401

#### Daily Homes Subdivision - Kings County, Annual

8.2 Waste by Land Use Unmitigated

	Waste Disposed	Total CO2	CH4	N2O	CO2e
Land Use	tons		МТ	-/yr	
Single Family Housing	137.88				69.3401
Total			-		69.3401

#### **Mitigated**

	Waste Disposed	Total CO2	CH4	N2O	CO2e
Land Use	tons		МТ	-/yr	
Single Family Housing	137.88				69.3401
Total					69.3401

## 9.0 Operational Offroad

Equipment Type	Number	Hours/Day	Days/Year	Horse Power	Load Factor	Fuel Type

#### Daily Homes Subdivision - Kings County, Annual

## 10.0 Stationary Equipment

#### **Fire Pumps and Emergency Generators**

	Equipment Type	Number	Hours/Day	Hours/Year	Horse Power	Load Factor	Fuel Type
--	----------------	--------	-----------	------------	-------------	-------------	-----------

#### **Boilers**

Equipment Type	Number	Heat Input/Day	Heat Input/Year	Boiler Rating	Fuel Type

#### **User Defined Equipment**

Equipment Type	Number

#### 11.0 Vegetation

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#### Daily Homes Subdivision - Kings County, Annual

## Daily Homes Subdivision Kings County, Annual

#### 1.0 Project Characteristics

#### 1.1 Land Usage

Land Uses	Size	Metric	Lot Acreage	Floor Surface Area	Population
Single Family Housing	134.00	Dwelling Unit	20.00	241,200.00	383

#### 1.2 Other Project Characteristics

Urbanization	Urban	Wind Speed (m/s)	2.2	Precipitation Freq (Days)	37
Climate Zone	3			Operational Year	2019
Utility Company	Pacific Gas & Electric C	ompany			
CO2 Intensity (lb/MWhr)	641.35	CH4 Intensity (lb/MWhr)	0.029	N2O Intensity (lb/MWhr)	0.006

#### 1.3 User Entered Comments & Non-Default Data

Project Characteristics -

Land Use - Per site plan

Mobile Land Use Mitigation -

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Table Name	Column Name	Default Value	New Value
tblArchitecturalCoating	EF_Nonresidential_Exterior	150.00	250.00
tblArchitecturalCoating	EF_Nonresidential_Interior	150.00	250.00
tblArchitecturalCoating	EF_Residential_Exterior	150.00	250.00
tblArchitecturalCoating	EF_Residential_Interior	150.00	250.00
tblLandUse	LotAcreage	43.51	20.00
tblProjectCharacteristics	OperationalYear	2018	2019
tblWoodstoves	NumberCatalytic	20.00	0.00
tblWoodstoves	NumberNoncatalytic	20.00	0.00

## 2.0 Emissions Summary

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#### Daily Homes Subdivision - Kings County, Annual

# 2.1 Overall Construction <a href="Unmitigated Construction">Unmitigated Construction</a>

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Year					ton	s/yr							MT	7/yr		
2017			i i i													255.7931
2018			 													383.8350
2019			 							 						1.4681
Maximum																383.8350

#### **Mitigated Construction**

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Year					ton	s/yr							МТ	/yr		
2017																255.7929
2018	11 11 11			 				 								383.8346
2019	1 1 1 1			1 1 1				 								1.4681
Maximum					-											383.8346

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#### Daily Homes Subdivision - Kings County, Annual

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio-CO2	Total CO2	CH4	N20	CO2e
Percent Reduction	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

	Quarter	Start Date	End Date	Maximum Unmitigated ROG + NOX (tons/quarter)	Maximum Mitigated ROG + NOX (tons/quarter)
Ī			Highest		

#### 2.2 Overall Operational

#### **Unmitigated Operational**

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
Area			: : :		! !			! !								60.0599
Energy	#;  -  -  -  -		<b></b> ! !	,	,	;		,				,			,	599.4823
Mobile	6;	<del></del>		,	,			<b>,</b> : : : :				,				2,533.221 4
Waste	6;	<del></del>		,	,			<b>,</b> : : : :				,				69.3401
Water	#,	<del></del>       		,	,	<del></del>  -  -  -	<del></del>	,		, , , , , , , , , , , , , , , , , , ,						31.3069
Total																3,293.410 6

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#### Daily Homes Subdivision - Kings County, Annual

## 2.2 Overall Operational

#### **Mitigated Operational**

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
Area															i i	60.0599
Energy		       					,       		 						1	599.4823
Mobile		       					,       		 						1	1,198.235 3
Waste		       					,       		 						1	69.3401
Water							,								1 1 1 1	31.3069
Total																1,958.424 5

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio-CO2	Total CO2	CH4	N20	CO2e
Percent Reduction	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	40.54

#### 3.0 Construction Detail

#### **Construction Phase**

#### Daily Homes Subdivision - Kings County, Annual

Phase Number	Phase Name	Phase Type	Start Date	End Date	Num Days Week	Num Days	Phase Description
1	Architectural Coating	Architectural Coating	12/15/2018	1/11/2019	5	20	
2	Building Construction	Building Construction	9/23/2017	11/16/2018	5	300	
3	Demolition	Demolition	7/3/2017	7/28/2017	5	20	
4	Grading	Grading	8/12/2017	9/22/2017	5	30	
5	Paving	Paving	11/17/2018	12/14/2018	5	20	
6	Site Preparation	Site Preparation	7/29/2017	8/11/2017	5	10	

Acres of Grading (Site Preparation Phase): 0

Acres of Grading (Grading Phase): 75

Acres of Paving: 0

Residential Indoor: 488,430; Residential Outdoor: 162,810; Non-Residential Indoor: 0; Non-Residential Outdoor: 0; Striped Parking Area: 0 (Architectural Coating – sqft)

OffRoad Equipment

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Phase Name	Offroad Equipment Type	Amount	Usage Hours	Horse Power	Load Factor
Architectural Coating	Air Compressors	1	6.00	78	0.48
Demolition	Excavators	3	8.00	158	0.38
Demolition	Concrete/Industrial Saws	1	8.00	81	0.73
Grading	Excavators	2	8.00	158	0.38
Building Construction	Cranes	1	7.00	231	0.29
Building Construction	Forklifts	3	8.00	89	0.20
Building Construction	Generator Sets	1	8.00	84	0.74
Paving	Pavers	2	8.00	130	0.42
Paving	Rollers	2	8.00	80	0.38
Demolition	Rubber Tired Dozers	2	8.00	247	0.40
Grading	Rubber Tired Dozers	1	8.00	247	0.40
Building Construction	Tractors/Loaders/Backhoes	3	7.00	97	0.37
Grading	Graders	1	8.00	187	0.41
Grading	Tractors/Loaders/Backhoes	2	8.00	97	0.37
Paving	Paving Equipment	2	8.00	132	0.36
Site Preparation	Tractors/Loaders/Backhoes	4	8.00	97	0.37
Site Preparation	Rubber Tired Dozers	3	8.00	247	0.40
Grading	Scrapers	2	8.00	367	0.48
Building Construction	Welders	1	8.00	46	0.45

**Trips and VMT** 

#### Daily Homes Subdivision - Kings County, Annual

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Phase Name	Offroad Equipment Count	Worker Trip Number	Vendor Trip Number	Hauling Trip Number	Worker Trip Length	Vendor Trip Length	Hauling Trip Length	Worker Vehicle Class	Vendor Vehicle Class	Hauling Vehicle Class
Architectural Coating	1	10.00	0.00	0.00	10.80	7.30	20.00	LD_Mix	HDT_Mix	HHDT
Building Construction	9	48.00	14.00	0.00	10.80	7.30	20.00	LD_Mix	HDT_Mix	HHDT
Demolition	6	15.00	0.00	0.00	10.80	7.30	20.00	LD_Mix	HDT_Mix	HHDT
Grading	8	20.00	0.00	0.00	10.80	7.30	20.00	LD_Mix	HDT_Mix	HHDT
Paving	6	15.00	0.00	0.00	10.80	7.30	20.00	LD_Mix	HDT_Mix	HHDT
Site Preparation	7	18.00	0.00	0.00	10.80	7.30	20.00	LD_Mix	HDT_Mix	HHDT

## **3.1 Mitigation Measures Construction**

## 3.2 Architectural Coating - 2018

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Archit. Coating																0.0000
Off-Road		 	 													1.4076
Total																1.4076

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#### Daily Homes Subdivision - Kings County, Annual

## 3.2 Architectural Coating - 2018 <u>Unmitigated Construction Off-Site</u>

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
1			! !													0.0000
Vendor	r,	,       	,       	       	, ! ! !	1   	,       	, ! ! !	 							0.0000
Worker		,	,		,	1 1 1 1 1	,	,	 							0.3991
Total																0.3991

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	-/yr		
Archit. Coating																0.0000
Off-Road					<del></del>    - 	<del></del> -     										1.4076
Total																1.4076

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#### Daily Homes Subdivision - Kings County, Annual

3.2 Architectural Coating - 2018

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling																0.0000
Tondo:																0.0000
Worker			]			<del></del>   		<del></del> -				,				0.3991
Total																0.3991

## 3.2 Architectural Coating - 2019

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	-/yr		
Archit. Coating																0.0000
Off-Road					       				; ! !						i i	1.1514
Total																1.1514

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#### Daily Homes Subdivision - Kings County, Annual

## 3.2 Architectural Coating - 2019 <u>Unmitigated Construction Off-Site</u>

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
1																0.0000
Vendor	r,		, , , ,	       	, ! ! !	1   		, ! ! !	       							0.0000
Worker	  	<del></del>     	,		, : : :	y <del></del> : : :		,	       			1		<del></del>   		0.3167
Total																0.3167

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
Archit. Coating	ii ii															0.0000
Off-Road																1.1514
Total																1.1514

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## 3.2 Architectural Coating - 2019 Mitigated Construction Off-Site

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
1			: : :													0.0000
Vendor	r,	,       		       	, ! ! !	,		, ! ! !	 							0.0000
Worker	r,	,			,	1       		,	 							0.3167
Total																0.3167

## 3.3 Building Construction - 2017

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
																84.6909
Total																84.6909

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## 3.3 Building Construction - 2017 Unmitigated Construction Off-Site

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
Hauling																0.0000
Vendor	r,	,       	, , , ,	       	, ! ! !	1   		, ! ! !	 			, ! !				13.6948
Worker	  	,	,		,	1 1 1 1 1		,	 			,				12.6832
Total																26.3780

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
			 	i ! !						! ! !						84.6908
Total																84.6908

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## 3.3 Building Construction - 2017 Mitigated Construction Off-Site

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling																0.0000
Vendor	r,	,       	, , , ,	       	, ! ! !			, ! ! !	 							13.6948
Worker	r,	,	,	       	, ! ! !			,								12.6832
Total																26.3780

## 3.3 Building Construction - 2018

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
0																275.1071
Total																275.1071

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## 3.3 Building Construction - 2018 <u>Unmitigated Construction Off-Site</u>

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
1			: : :													0.0000
Vendor	r,	,       		       	, ! ! !			, ! ! !	       							44.8057
Worker	r,	,			,			,								40.0535
Total																84.8592

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
J																275.1068
Total																275.1068

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## 3.3 Building Construction - 2018 <u>Mitigated Construction Off-Site</u>

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling																0.0000
Vendor																44.8057
Worker	N															40.0535
Total													_			84.8592

## 3.4 Demolition - 2017

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
																35.8438
Total																35.8438

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3.4 Demolition - 2017

<u>Unmitigated Construction Off-Site</u>

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling																0.0000
Vendor																0.0000
Worker								<del></del> -	<del></del>     			, — — — — — — — — — — — — — — — — — — —				1.1324
Total																1.1324

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
0																35.8438
Total																35.8438

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3.4 Demolition - 2017

<u>Mitigated Construction Off-Site</u>

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
Hauling																0.0000
Vendor			,													0.0000
Worker			,													1.1324
Total																1.1324

## 3.5 Grading - 2017

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
l agains 2 doi																0.0000
Off-Road		 	 		       	 	 									87.0011
Total													-			87.0011

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## Daily Homes Subdivision - Kings County, Annual

3.5 Grading - 2017

<u>Unmitigated Construction Off-Site</u>

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/уг		
Hauling																0.0000
Vendor			 		       				       							0.0000
Worker			 		       				       					 		2.2649
Total																2.2649

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
l agains 2 act																0.0000
	F)         															87.0010
Total																87.0010

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## Daily Homes Subdivision - Kings County, Annual

3.5 Grading - 2017

<u>Mitigated Construction Off-Site</u>

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/уг		
Hauling																0.0000
Vendor			,						       							0.0000
Worker		<del></del>   	,													2.2649
Total																2.2649

# 3.6 Paving - 2018 <u>Unmitigated Construction On-Site</u>

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
																20.9736
Paving					       			1	       	 					       	0.0000
Total																20.9736

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## Daily Homes Subdivision - Kings County, Annual

3.6 Paving - 2018
<u>Unmitigated Construction Off-Site</u>

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
Hauling																0.0000
Vendor	n		,													0.0000
Worker			,													1.0884
Total												_	_			1.0884

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
Oii Nodu	 															20.9736
Paving	, — — — — — — — — — — — — — — — — — — —	,				1   										0.0000
Total																20.9736

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## Daily Homes Subdivision - Kings County, Annual

3.6 Paving - 2018

Mitigated Construction Off-Site

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
1			! !													0.0000
Vendor	r,	,       	,       	       	, ! ! !			, ! ! !	       							0.0000
Worker	r,	,	,		,			,								1.0884
Total																1.0884

## 3.7 Site Preparation - 2017

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
l agiliro Daet																0.0000
Off-Road								       							       	17.8025
Total																17.8025

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## Daily Homes Subdivision - Kings County, Annual

3.7 Site Preparation - 2017
<u>Unmitigated Construction Off-Site</u>

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
Hauling																0.0000
Vendor	r,	,       	, , , ,	       	, ! ! !	1   	,       	, ! ! !	 			, ! !				0.0000
Worker	  	,	,		,	1       	,	,	 			,				0.6795
Total																0.6795

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	/yr		
l agiliro Daet																0.0000
Off-Road																17.8025
Total																17.8025

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#### Daily Homes Subdivision - Kings County, Annual

## 3.7 Site Preparation - 2017 <u>Mitigated Construction Off-Site</u>

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/yr		
Hauling																0.0000
Vollagi																0.0000
Worker					<del></del>    - 											0.6795
Total																0.6795

## 4.0 Operational Detail - Mobile

## **4.1 Mitigation Measures Mobile**

Increase Density

Improve Walkability Design

Improve Destination Accessibility

Increase Transit Accessibility

Improve Pedestrian Network

## Daily Homes Subdivision - Kings County, Annual

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	/уг		
Mitigated	 		 					 								1,198.235 3
Unmitigated	,, ,, ,,								       							2,533.221 4

## **4.2 Trip Summary Information**

	Avei	rage Daily Trip Ra	ate	Unmitigated	Mitigated
Land Use	Weekday	Saturday	Sunday	Annual VMT	Annual VMT
Single Family Housing	1,275.68	1,327.94	1155.08	3,623,370	1,251,182
Total	1,275.68	1,327.94	1,155.08	3,623,370	1,251,182

## **4.3 Trip Type Information**

		Miles			Trip %			Trip Purpos	e %
Land Use	H-W or C-W	H-S or C-C	H-O or C-NW	H-W or C-W	H-S or C-C	H-O or C-NW	Primary	Diverted	Pass-by
Single Family Housing	10.80	7.30	7.50	42.30	19.60	38.10	86	11	3

#### 4.4 Fleet Mix

Land Use	LDA	LDT1	LDT2	MDV	LHD1	LHD2	MHD	HHD	OBUS	UBUS	MCY	SBUS	МН
Single Family Housing	0.480541	0.029898	0.145962	0.133853	0.023791	0.005025	0.012238	0.156969	0.001786	0.002002	0.006069	0.001023	0.000844

## 5.0 Energy Detail

Historical Energy Use: N

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## Daily Homes Subdivision - Kings County, Annual

## **5.1 Mitigation Measures Energy**

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							МТ	√yr		
Electricity Mitigated								! !				1 1 1 1				368.5390
Unmitigated	n	,				<del></del>	,	,				,	,			368.5390
Mitigated	n	,				<del></del>	,	,				,				230.9433
Unmitigated	  	       	,		     		       	y ! ! !	     			 ! !	,	       	     	230.9433

## 5.2 Energy by Land Use - NaturalGas

**Unmitigated** 

	NaturalGa s Use	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Land Use	kBTU/yr					ton	s/yr							MT	/yr		
Single Family Housing	4.30215e +006																230.9433
Total																	230.9433

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## Daily Homes Subdivision - Kings County, Annual

# **5.2 Energy by Land Use - NaturalGas Mitigated**

	NaturalGa s Use	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Land Use	kBTU/yr					ton	s/yr							MT	/yr		
Single Family Housing	4.30215e +006																230.9433
Total																	230.9433

## 5.3 Energy by Land Use - Electricity Unmitigated

	Electricity Use	Total CO2	CH4	N2O	CO2e
Land Use	kWh/yr		МТ	-/yr	
Single Family Housing	1.2619e +006				368.5390
Total					368.5390

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## Daily Homes Subdivision - Kings County, Annual

## 5.3 Energy by Land Use - Electricity Mitigated

	Electricity Use	Total CO2	CH4	N2O	CO2e
Land Use	kWh/yr		МТ	-/yr	
Single Family Housing	1.2619e +006				368.5390
Total					368.5390

## 6.0 Area Detail

## **6.1 Mitigation Measures Area**

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category					ton	s/yr							MT	-/yr		
Mitigated	ii															60.0599
Unmitigated	11 11 11						 		i i					i i		60.0599

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## Daily Homes Subdivision - Kings County, Annual

## 6.2 Area by SubCategory Unmitigated

	ROG	NOx	СО	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
SubCategory					ton	s/yr							MT	/yr		
Architectural Coating								1 1 1								0.0000
Consumer Products							 	1 1 1								0.0000
Hearth			1	 				 						 		58.3948
Landscaping			 	 			     	 					 			1.6651
Total																60.0599

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## Daily Homes Subdivision - Kings County, Annual

## 6.2 Area by SubCategory Mitigated

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
SubCategory					ton	s/yr							МТ	√yr		
Architectural Coating																0.0000
Consumer Products						       	       									0.0000
Hearth												,				58.3948
Landscaping						,       	<del></del>	<del></del> -     						<del></del>     		1.6651
Total																60.0599

## 7.0 Water Detail

## 7.1 Mitigation Measures Water

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	Total CO2	CH4	N2O	CO2e
Category		МТ	√yr	
Magatod	11 11 11			31.3069
Unmitigated				31.3069

## 7.2 Water by Land Use Unmitigated

	Indoor/Out door Use	Total CO2	CH4	N2O	CO2e
Land Use	Mgal		МТ	-/yr	
Single Family Housing	8.73064 / 5.5041	i (			31.3069
Total					31.3069

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## Daily Homes Subdivision - Kings County, Annual

7.2 Water by Land Use

## **Mitigated**

	Indoor/Out door Use	Total CO2	CH4	N2O	CO2e
Land Use	Mgal		MT	-/yr	
Single Family Housing	8.73064 / 5.5041				31.3069
Total					31.3069

#### 8.0 Waste Detail

## 8.1 Mitigation Measures Waste

## Category/Year

	Total CO2	CH4	N2O	CO2e
		МТ	√yr	
Miligatod				69.3401
Unmitigated				69.3401

## Daily Homes Subdivision - Kings County, Annual

8.2 Waste by Land Use <u>Unmitigated</u>

	Waste Disposed	Total CO2	CH4	N2O	CO2e
Land Use	tons		МТ	√yr	
Single Family Housing	137.88				69.3401
Total					69.3401

## **Mitigated**

	Waste Disposed	Total CO2	CH4	N2O	CO2e
Land Use	tons		МТ	-/yr	
Single Family Housing	137.88				69.3401
Total					69.3401

## 9.0 Operational Offroad

Equipment Type	Number	Hours/Day	Days/Year	Horse Power	Load Factor	Fuel Type
		,	•			**

## Daily Homes Subdivision - Kings County, Annual

## 10.0 Stationary Equipment

## **Fire Pumps and Emergency Generators**

	Equipment Type	Number	Hours/Day	Hours/Year	Horse Power	Load Factor	Fuel Type
--	----------------	--------	-----------	------------	-------------	-------------	-----------

#### **Boilers**

Equipment Type	Number	Heat Input/Day	Heat Input/Year	Boiler Rating	Fuel Type

#### **User Defined Equipment**

Equipment Type	Number
----------------	--------

## 11.0 Vegetation